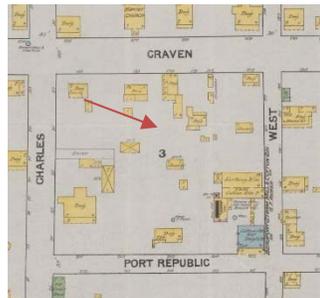


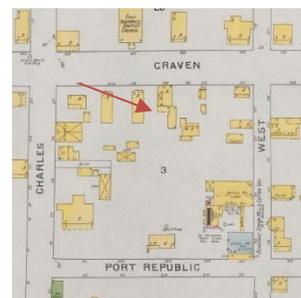
CITY OF BEAUFORT
Historic District Review Board
Full Board
Staff Report
Meeting of August 17, 2016

Case Number: HR16-27
Property Address: 918 Craven Street
Applicant: Structured Parking Solutions
Type of Request: New Construction
Zoning: CC – Core Commercial

Historical: This parcel on Craven Street is located in Core Commercial downtown. It is currently mostly vacant – it contains a contributing structure on the corner of Craven and Charles Streets - but historically housed up to 9 primary structures comprised of 1- and 2-story buildings, and a series of outbuildings including a livery. At maximum build-out there were 11 dwelling units, plus a livery and feed supply store. This pattern was maintained until sometime between 1912-1952, when the corner 2-story structure on the northwest corner was replaced with the existing contributing structure, and all but two dwellings facing Craven Street, and one dwelling facing Charles Street, remained. Since 1952, the only structure to remain is the existing cottage on the corner. It is important to note that the surrounding context, namely the structures at 901, 907, 911 and 915 Craven Street, 315 West Street, 308 and 314 Charles Street, and the c. 1820 Lucius Cuthbert/Scheper house at 915 Port Republic Street, have remained as they were depicted on the 1889 Sanborn Map.



1889 Sanborn Map



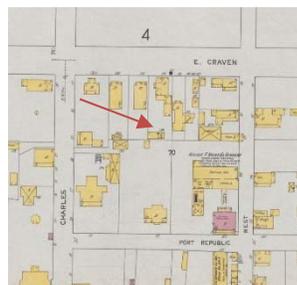
1894 Sanborn Map



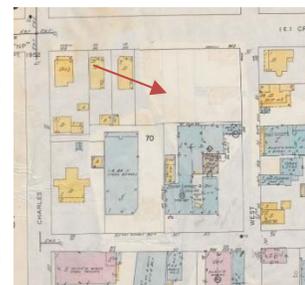
18998 Sanborn Map



1905 Sanborn Map



1912 Sanborn Map



1958 Sanborn Map

Request: The applicant wishes to construct a new 4-level structured parking deck.

Size: The total footprint is approximately 46,000 square feet per floor, with a total square footage of 184,000 square feet. There will be a total of 496 parking spaces. It is shown at 38'-2" to the top of the parapet along Craven Street. The stair towers are shown at 43'-2", and the tower on the west elevation is shown at 47'-2"

Zoning: CC – Core Commercial

- Setbacks: all setbacks in this zone are 0'.
- Maximum Height: 35' above curb at the property line, 50' max. in the interior of the site so long as it does not penetrate the sky exposure plane (for every 1' above 35', the building must be set back 1'). OR
 - If an open area is provided along the full length of the front line, the 35' maximum height is waived. However the maximum building height is still 50' and no building or other structure shall penetrate the alternate sky exposure plane.
 - Exceptions to height limits include: cupolas, spires, elevator shafts, mechanical equipment, and parapets and other devices used to screen rooftop equipment.
 - This nets to an additional 1' setback required for each additional 1' in height, per section 6.5.K.11 of the UDO.
 - At the 6'-9" setback shown along Craven Street, the permitted height would be 41'-9". This is shown at 38'-2"
 - At the 14' setback along Charles Street, the permitted height would be 49'
 - At the 8' setback along West Street, the permitted height would be 43'.
- Demolition: This proposal would require the removal of two structures – 918 Craven Street, a contributing structure as described previously, and 310 West Street, not listed on the survey. Both structures will require a public hearing to move and/or remove if that is the ultimate intent.

Synopsis of Applicable Guidelines:

The issue of parking in this Core Commercial Downtown area has been studied since at least 1972. Please see the attached document, which lists the recommendations for parking from a variety of plans and studies conducted by the city over the past 44 years.

Staff Questions, Comments & Suggestions:

General Comments:

- From a zoning standpoint, a parking garage is permitted. The question to the HRB is whether or not this structure is compatible with the Historic District per applicable guidelines used to review the project.
- The parking structure proposed is larger than the garages that had been proposed or studied in this area previously. Given that the footprint that covers an entire block face, in conjunction with the continuous height, staff does not feel that it is possible to integrate this size, mass and scale of a building into this specific surrounding context, regardless of the architectural detailing.

- With regards to the 496 parking-space number/requirement that is being proposed, it is important that current and future demand be considered, but not be the sole, or even primary, driving element. Design, character and careful integration into the city's most valuable built resource – the historic district – must be concurrently at the forefront of the design effort. This approach may affect the permitted absolute size of the building, as well as the uses, and mixture thereof if applicable.
- It is not realistic to expect that one building will be able to solve all parking problems in the historic district. As with any other development in this area, appropriately-scaled projects often result in incremental development patterns. This is admittedly a costlier solution – to have a smaller scaled parking structure internal to a block – but is the only solution that will not have a detrimental effect on the historic district. This is supported by the planning documents referenced above, as well as the letter from the SC SHPO.

Specific Comments

- **Existing Buildings:** The concept plan showed the entire width of the block being utilized by the parking garage. This includes two existing buildings – 918 Port Republic Street and 310 West Street. What is the plan for these buildings? 918 Craven is a contributing structure, c. 1930. It is not the original structure on this parcel, as the Sanborn Maps through 1924 show a 2 story building on this site. 310 West Street is not contributing; it is not listed on the survey. Both structures will require an HRB application and public hearing to move and/or remove if that is the ultimate intent.
- **Trees:** Given the size of the existing live oaks planted along Craven Street – 17” – 37” caliper – the canopy will be impacted by development within 20’ of the front setback line. There are several other trees within the interior of the site that will be impacted as well. Some of the smaller trees may be able to be transplanted elsewhere onto this or an alternative site. An arborist report will be required for trees 24” caliper and greater that will be impacted by this project. The report should include fertilization and pruning strategies for trees that are proposed to be retained.
- **Design:**
 - It is critical that this building addresses the street appropriately.
 - The core of the large scale parking structure should be set within the interior of the block, as per the past 44 years of plans and studies of this area.
 - The introduction of incremental liner buildings, or the reservation of space to construction them in the future, could help to create an appropriate interaction between the historic built fabric and the modern-day necessity of a parking garage. Liner buildings have been utilized increasingly over the past 15-20 years, particularly in applications where a parking garage is being inserted into blocks where the aesthetics, size, mass and scale are critical to the surrounding blocks and neighborhoods. It is important to note, however, that per the SC SHPO letter, liner buildings would also need to fit in with the size, mass and scale of the surrounding context. Given the size of this parking structure, the size of the liner buildings that would be required may not be able to meet these criteria.
 - As a note, the Neighborhood Commercial zoning district, which abuts this parcel directly to the north, does have requirements that “the entrance to any parking structure shall be on the side or the rear of the building and the first floor of the front of the structure shall contain office and/or commercial space.” The Lady’s Island Village Center, and Boundary Street Design Districts have this requirement as well.

Historically, the intention was to make an amendment to the Core Commercial (CC) zoning district (which is in many respects more significant and important than the other three listed above) to have these requirements for parking structures. However, with the new pending code still in progress, the most recent thought process has been to incorporate similar standards for parking garages, and other large footprint structures (greater than 20,000 square feet footprint) in CC into the new code.

- A physical massing model, studying how a larger structure interfaces and impacts the surrounding historic fabric, will be required. It should depict, at minimum, the block in question, and the surrounding 6 blocks.
- A bicycle rack is required.
- All mechanical equipment, including rooftop equipment, must be screened.
- All dry utilities serving the site must be placed underground. It would be beneficial to discuss this with SCEG soon, as there are existing overhead utilities that run along the Craven Street property line.
- Any dumpsters must be screened on all four sides.
- A lighting plan is required. Full cut-off fixtures are required.
- Signage requires a separate permit.
- **Elevation of First Floor:** This block is a transitional block, as one side contains commercial structures, built at grade, and the other side fronting Craven Street faces residential and civic structures which are elevated off of grade. The concept of an appropriately-scaled liner residential building helps to bridge this gap and reflects the first floor elevations across Craven Street. The structure is also located in the flood plain, so unless they are flood-proofed, all habitable space must be raised to a minimum of 14' above Base Flood Elevation (BFE + 1' freeboard per the IBC).
- **Floor-To-Floor Heights:** New construction should have floor-to-floor heights, particularly on the ground floor, that are consistent with the surround areas. Commercial structures in Beaufort typically have 11'-14' first floor clear heights, while residential structures range from 9'-12' typically. Appropriately-scaled liner buildings facilitate this by providing a natural location for these first floor heights when visible from the public right-of-way. The mid-block parking structure would not need to replicate these heights, as they would not be visible from the street.
- **Bays, Windows & Doors / Proportions:** The proportions of building bays, and openings should be carefully studied and compatible with the surrounding buildings. Particularly for a large-scale structure, the rhythm of the bays will be critical to the new streetscape formed by the building. False fenestration and modulation is not recommended as it detracts from the integrity of the street, block and historic district.
- **Absolute Size:**
 - It is not appropriate to have one, monolithic structure that encompasses the entire block, visible from the streetscape. Appropriately-scaled liner buildings are essential to create real building increments at the street. From a use perspective this alleviates the resultant mono-use block and building as well, something that is not characteristic of Beaufort's historic district. **Currently and historically, there are not and have not been any other blocks in the core commercial downtown that are single-use.** The Core Commercial district is characterized by an intricate pattern of buildings, scales and uses, and this continues to be the

building pattern that is encouraged and desired.

- The two largest buildings in this area, that take up all or the majority of a block by a combination of building and parking area, are the former Bank of America building at 500 Carteret Street, and the Post Office on Charles Street. Neither of these blocks reflect ideal or appropriate building patterns for Beaufort, particularly in the Historic District.
- **Massing:** Solid-to-void ratios, fenestration and porches/exterior projections and overhangs should all be compatible with the current existing fabric.
- **Orientation:** Typically, Beaufort has an east-west building orientation, with the primary façade located on east-west streets. However in the Historic District, particularly adjacent to the primary north-south commercial corridor of Charles Street, it is important that the building be sited and oriented to address all street frontages appropriately. The street hierarchy in this block, is Craven & Charles being the two primary streets, and then West Street being a secondary street. All frontages should all be carefully and specifically considered.
- **Materials:** New materials are appropriate on new buildings, however they should be compatible and sympathetic with regards to color, scale, function and craftsmanship, to the surrounding context and the overall Historic District.

The issue of parking in this Core Commercial Downtown area has been studied since at least 1972. The following lists the recommendations for parking from a variety of plans and studies conducted by the city over the past 44 years:

- 1972 – *Feiss Wright Study* –
 - p. 32-36 – Preservation Goals and Objectives: “...to ensure that existing components of the city’s attractive, viable and authentic environment are retained...and that new elements to improve conditions are introduced without conflict. Objective A: ...The integrity and authenticity of the area must be preserved and recognized as the prime consideration in any future planning efforts. Objective B: Any..proposal for new development...must recognize the above precept...redevelopment and new construction must be channeled and directed to serve a dual purpose, the provision of needed additional facilities, while at the same time being so located as to eliminate blighting uses, sub-standard structures and environmental or aesthetic deficiencies. This precept recognizes the need for new facilities within the historic district, but integrates this new construction activity with the preservation goals and objectives of the community. 6 specific guidelines follow.”
 - Figure 20 (between p. 64-65) – sketch showing internalized parking with smaller increment buildings facing Craven Street.
 - p.69 –“ But perhaps the most important contribution that can be made by the City is the continued cooperation with preservationists and the rigid but fair enforcement of the Historic District Ordinance, Zoning Ordinance and other legal devices available to protect the quality, authenticity and value of one of the most important and impressive preservation districts in the United States, Historic Beaufort. “
- *Milner’s 1979 Preservation Manual*, p. 46-47 discusses “High Density Construction” – “Ideally, the Historic District of Beaufort would be able to avoid the intrusion of large scale building and mid-to-high density construction ad infinitum...While massive construction projects certainly warrant protest on legitimate preservation grounds, the board should be aware of means by which the negative impact of large scale buildings can be minimized. In the event that such construction is deemed a necessity to by the community-at-large, it should, at the very least, conform to the following design and locational parameters:
 - (Parameter 3) – Prior to admitting such construction within the District, the review board and City administration should require that an effort be made to see acceptable alternative sites beyond the boundaries of the District. Assistance should be provided to the owner/developer in locating such sites as will be mutually beneficial to the town and the property owner.
 - (Parameter 4) – No development or large scale construction should be permitted which is predicated upon demolition of historic buildings for implementation)
 - (Parameter 6) – Within the District, locations should be sought which best accommodate the larger scale structures:
 - Areas which previously intruded upon by modern construction
 - Large lots which can be easily screened
 - Areas containing few or no significant historic structures
 - In no case should overscale structures be located...in such a way that they become the dominant visible architectural massing of an area”
 - “Of course, it is highly desirable to avoid large scale construction altogether by limiting the height, volume and/or plan area of new buildings...It should be noted that ‘large scale’ construction, as discussed here, applies equally to one or two story structures of

extensive floor area. Extremely long, low continuous buildings can negatively impact the District to the same degree as mid-to-high rise structures.”

- Robert Marvin’s *1986 Urban Design Plan* begins to address parking as a need for future growth in the “Central Business District” or Core Commercial downtown. P. 22 begins this discussion by listing the following items as part of possible revisions to the Central Business District plan:
 - Preserving the character and scale of downtown Beaufort.
 - Meeting the present and future parking needs.
 - Developing a management plan and organization to adequately control new buildings and renovations (scale, character, parking, etc.)
 - P. 25 specifically discusses the southern block of Craven Street between Charles and Scott Street. It says that “Craven Street is a charming unique street with its own scale and character... This existing character must be respected. The proposed uses along the two blocks fronting on the south side of Craven Street are predominantly residential with townhouses and offices. New construction could fill in between existing structures. Parking to serve these uses could be located southward in the middle of the block.” The next paragraph about Port Republic Street also mentions parking in the center of a block.
 - Finally, this plan references a parking study done prior to this plan. It identifies the need for a consolidated parking plan, utilizing existing lots, strategically acquiring new lots, and also utilizing on-street (a.k.a. curb parking) which should be enhanced.
- *1989 Land Use Plan and Preservation Plan*: p.50-51 discusses Land Use Issues and Policy Recommendations for Protection and Enhancement of the City’s Historic Resources. It describes the Landmark Historic District as “a positive asset that plays a vital role in the economic vitality of the region. Beaufort is recognized nationally for its unique collection of 18th and 19th century residences, churches and commercial and public buildings.” P. 51 describes a concern of high density construction in the core commercial district “which may be in contrast to the massing, scale and height characteristics of the area.” It recommends adherence to the guidance in the Preservation Manual... and continuing to support the residential character of the Landmark Historic District.
- Milner’s *1990 Preservation Manual Supplement*, p. 13-16 discusses new construction. P. 15-16, has essentially the same recommendations regarding large scale construction as were stated in the 1979 Preservation Manual.
- The *2004 Comprehensive Plan* recommends the following:
 - p. V-72-73, Community Facilities Element, discuss the parking studies that had been completed in the historic downtown. It states: “Previous recommendations for parking have included the development of a public parking facilities, relocation of meters, the City leasing property for parking, the development of resident permit system, and improved signage and lighting. One other alternative that has not been explored is the use of remote parking lots and shuttle service to the downtown. This approach has been successful for the cities of Charlotte, NC and Charleston, SC and should be further studied.” This was reiterated in Policy 4.E on p. VI-99.
 - p. VI-97-98, discusses the transportation system in the historic downtown. “Goal 4: The future transportation network for the City of Beaufort and its adjacent surroundings should be structured so as to make the daily activities of its citizen’s flow naturally, conveniently and safely from one point to another while protecting the natural and historic character of the City”... Policy 4.A states: “base future transportation planning

- and decisions on the goal of maximizing the potential of Beaufort's existing network of streets." Strategy 4.A.4 supporting this Goal 4 and Policy 4.A states: "Develop a plan for curb and gutter on-street parking throughout the City of Beaufort." Policy 4.C: "Solve parking problems through innovative parking solutions which enhance the historic character of Downtown and improve accessibility throughout the City."
- The *2008 Historic Preservation Plan Update*, p. 41, acknowledges the changing nature of historic districts and specifically references a proposed parking deck. Note that the deck referenced in this plan was proposed for the southwest corner of Port Republic and Scott Streets. P. 93-95 specifically discuss the parking deck. "There are several important issues related to the planned parking deck for the site at Port Republic and Scott Streets regarding its impact on the visual character and traffic circulation in downtown." A traffic study was recommended.
 - P. 94 discusses details for the parking deck: "The building height and design of the proposed parking deck should minimize the physical impact of the structure on the character of adjacent properties and streetscape. Likewise, it is important that the parking structure have ground-floor retail along Port Republic and Scott Street frontages in order to maintain the pedestrian scale and character of these streets." This is the first reference to liner buildings or mixed-uses incorporated into the parking garages. In this case it refers both to the scale and character implications.
 - P. 95 discusses traffic implications of the parking deck, stating: "The increased number of vehicles attracted to the parking deck has the potential of impeding circulation on the relatively narrow commercial and pedestrian streets between the downtown and Old Commons. Measures should be taken to minimize the potential for circulation issues and traffic accidents, and to maximize pedestrian safety in the area. It is important that business owner and employees commit to using the parking deck on a regular basis in order to retain the surface parking spaces for customers and patrons."
 - The *2009 Comp Plan*, p. 214-216, discusses historic downtown infill strategies. C6 2.5 Recommends : "Construct a public parking structure. As previously noted, the anticipated replacement of existing surface lots with buildings will likely drive the demand for parking structure in the next ten years. Because of the cost of such a structure, a community like Beaufort can typically only afford to construct one such facility in a 10-20 year period. As a result, it is important that due consideration be given to its location, efficiency, and design to ensure that it fits in appropriately with the scale of the area. Ideally, the structure should be designed to have liner shops offices so as not to disrupt the flow of pedestrian throughout the downtown." The accompanying sketches show two possible locations for the garage: The one in question in this submission, and the one on the southwest corner of Scott and Port Republic Street. The one on the Craven Street block is shown with liner buildings wrapping 3 sides. It does not extend all the way to Charles Street, as it maintains the contributing structure at 918 Craven Street. The garage is shown to accommodate 368 cars. It does encompass Tabby Place which has since been restored and repurposed.
 - The *2014 Downtown Revitalization Plan* by Arnett Muldrow & Associates, commissioned by Mains Street Beaufort, discussed a structured parking deck as one of many strategies that together, could help to bolster and revitalize the historic downtown. Each time structured parking was mentioned, it was in conjunction with a mixed-use development, including smaller spaces for incubator businesses, and live-work opportunities.
 - The *2014 Civic Master Plan*, p. 52-53, CMP depict a parking garage located in the interior of the block, lined with buildings on Port Republic, Craven, and West Streets. It states "the parking structure would be concealed with ground floor uses or with mixed-use buildings to

shield the parking area from view... On Craven Street, apartment units would line the parking structure and create an appropriate transition to the residential neighborhoods north of the downtown area. The parking deck would replace the surface parking spaces displaced by new development on the Marina site (see Section 2.2) and provide convenient access to downtown. The structure could accommodate approximately 280 vehicles and promote a vibrant retail environment where visitors park once and then walk between shops. All together, the parking deck would alleviate a parking problem, support businesses, residents and visitors, and extend the Bay Street commercial core to the north without ruining the historic streetscape with concrete walls.” The conceptual imagery shows a garage located behind existing buildings – including 918 Craven Street - on Charles Street, with no portions of the garage fully exposed to any streetscape. Similar to the Arnett Muldrow study, it considers the garage part of a multi-faceted approach to help solve a parking issue, but also create new types of residential and commercial use that currently are not available.

1972
Feiss Wright Study

Chapter Three

A Preservation Plan for Historic Beaufort

The Historic Beaufort Preservation Plan consists of a series of inter-related components, beginning with a statement of Preservation Goals and Objectives, a Review of Planning Proposals for the Historic District, a Review and Recommendations concerning the Historic Beaufort District, Possible Development Parcels and Criteria for their Development, Visual Improvement of the Bay Street Commercial Area, Illustrative Site Plan and Proposed Land Use, and an Action Program.

Beaufort, through the Beaufort County Joint Planning Commission, has contracted with the Community Planning Division of the South Carolina State Planning and Grants Division, Office of the Governor, for planning services to the city. Recent reports supplied the city, which are of interest in the preparation of the Preservation Plan, include: Neighborhood Analysis, Beaufort, South Carolina (January 1970); Urban Beautification Study (August 1970); and Land Development Plan (October 1970). These reports are of high quality and professional competency. The Preservation Plan for Beaufort is meant to supplement these planning studies, and has been developed in concert with the goals and objectives as set forth in the reports. Each of the reports has been reviewed as it may affect preservation efforts in Historic Beaufort, and any minor potential conflicts of objectives are discussed in a later section of this chapter. The preparation of this Preservation Plan is not meant to supersede any of the proposals made in the earlier studies, its main role being that of establishing guidelines for future development of the historic area and integrating preservation objectives with other community goals.

Preservation Goals and Objectives

Preservation Goals and Objectives for Historic Beaufort have been developed to ensure that the existing components of the city's attractive, viable and authentic environment are retained and strengthened where necessary, and that

new elements to improve conditions are introduced without conflict. Goals and objectives also provide basic criteria for use by the Architectural Review Board and help to alert prospective builders and developers to the added checks and controls that must be satisfied if the integrity of Historic Beaufort is to be protected. Each of the goals and objectives of this plan is predicated on two precepts of paramount importance:

- A. The most important consideration of any plan for the preservation of Historic Beaufort is the protection and continuation of the historic district as an active, living historic area of national importance, worthy of its place on the National Register of Historic Districts. The integrity and authenticity of the area must be preserved and recognized as the prime consideration in any future planning efforts.
- B. Any redevelopment or proposals for new construction in the historic area must recognize the above precept. In addition, redevelopment and new construction must be channelled and directed to serve a dual purpose, the provision of needed additional facilities while at the same time being so located as to eliminate blighting uses, sub-standard structures and environmental or esthetic deficiencies. This precept recognizes the need for new facilities within the historic district, but integrates this new construction activity with the preservation goals and objectives of the community. This integration of goals can best be accomplished by using new construction as the catalyst for removing and replacing out-of-scale or sub-standard structures and unsuitable uses with more ap-

appropriately designed buildings of compatible use.

Based on these two precepts, a series of Preservation Goals and Objectives has been developed. To be of maximum benefit to the decision makers within the community, the goals are stated as design and development criteria.

1. Any proposal for new construction, restoration or rehabilitation must reflect the design vocabulary of the particular sub-area of Historic Beaufort for which it is proposed. Basic visual and design characteristics differ from study area to study area, and these variances should be recognized. Historic Beaufort is not a homogeneous area, in use or appearance, and while a design proposal might be acceptable for one location, it might be an intrusion in another. All proposals for new construction, rehabilitation or restoration must be designed for a specific location or building, and must take into account the surrounding properties.
2. All new construction in the historic area should reflect present day building technology and design theory, while relating to its surroundings through the sympathetic use of materials, textures, color, height form and massing. Contemporary design should be promoted to guarantee the integrity of Historic Beaufort and to add to the impressive collection of architectural styles that exist in the area. Period reconstructions or imitations of earlier architectural periods should be discouraged unless such projects are based on historical research and are intended to fulfill an educational role. Reconstruction of structures or building types which never existed in Historic Beaufort should be prohibited to ensure the authenticity of the area.

3. Restoration or rehabilitation work which requires the removal of significant features of a later period should be limited to buildings of major architectural or historic significance. Additions or other changes to many of the buildings in Beaufort are valid expressions of the period in which they were constructed, and should be retained in as many cases as possible. The removal of such elements in restoration, in itself a destructive process, should be limited to cases where enough information exists to permit accurate reconstruction. Also, such restoration should be limited to buildings where it can be demonstrated that the end results will add to the architectural character of Historic Beaufort.
4. All new construction and public improvements planned for the historic district should recognize the importance of retaining the original gridiron street pattern and building set-back lines. The grid pattern, a holdover from the original plan of circa 1717, is the major determinant of design form in Historic Beaufort and should not be violated. Set-back lines, which differ for each of the major land use areas, play an important role in establishing the overall visual character of the various sub-areas. Side yards vary from area to area and should also be taken into consideration in new construction projects. Any new construction in the shopping area of Bay Street, as an example, should be placed at the sidewalk line, with no space, or side yard, between buildings.

If traffic planning considerations suggest that a portion

of any existing street should be closed or realigned, the original right-of-way should be retained and treated to retain the visual quality of the original grid pattern. No new streets should be permitted in the historic district, and any street widening should be submitted to the Architectural Review Board for their comment.

5. Coordinated public expenditures should be promoted to help ensure a healthy climate for private restoration and rehabilitation projects. Such expenditures might include street tree planting, paving and sidewalk repair as well as lighting for parking lots, street furniture and graphics for new projects. All such public improvements stimulate new and continued private investments by protecting property values and safeguarding prior expenditures.
6. Existing buildings of architectural or historic significance should remain in active, viable use, consistent with their original design and location. Where the design use or an earlier use is no longer appropriate or economically feasible, compatible re-uses should be identified and the building adapted to the use, with as little change to the fabric of the original structure as possible. Retaining a structure in active use ensures its future and provides for continued maintenance after the initial outlay for restoration or rehabilitation.

The six Preservation Goals and Objectives outlined above form the basis for planning decisions made as a part of this report.

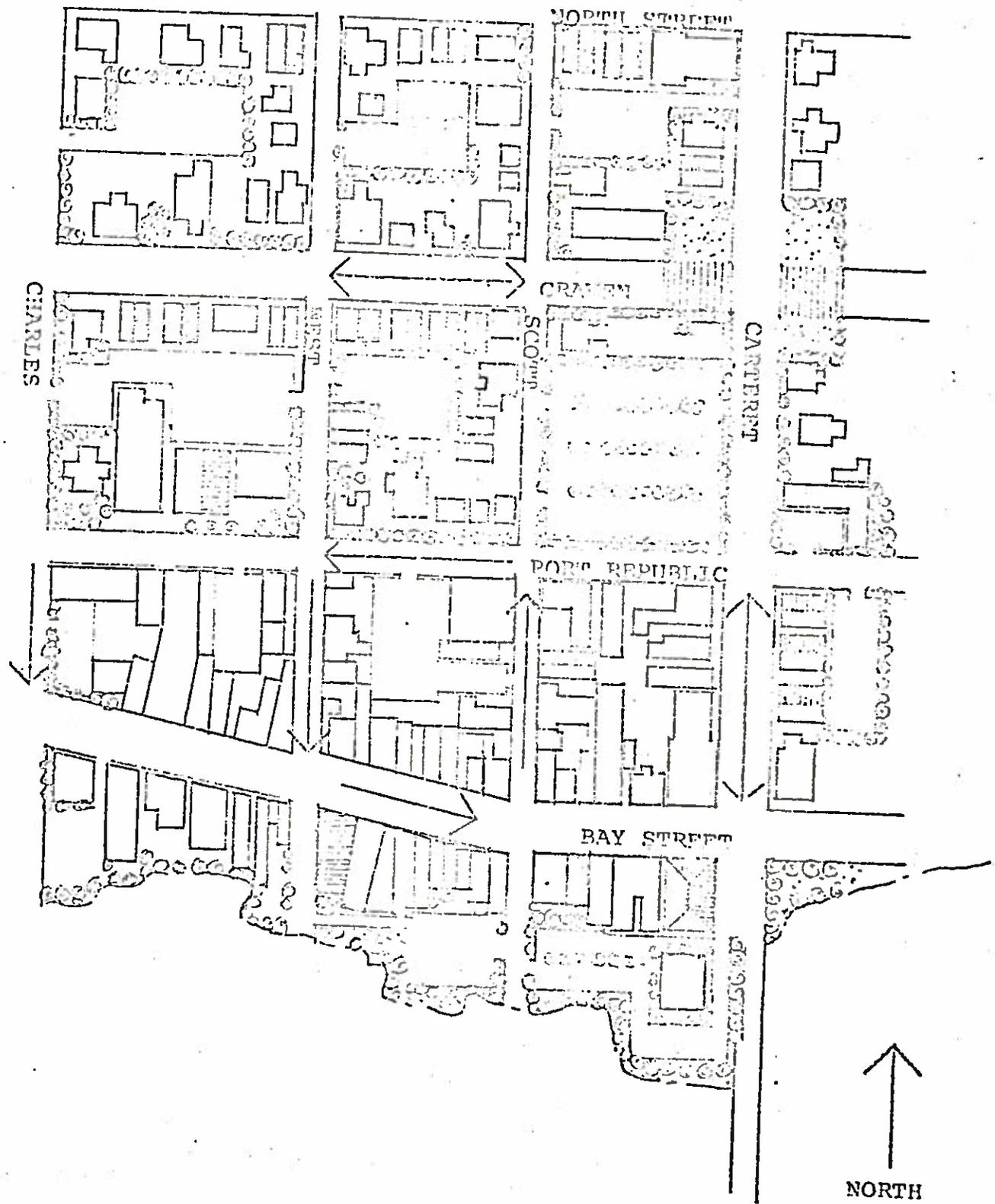


FIGURE 20: BEAUFORT CENTRAL BUSINESS

number of ways. First is the acceptance of the Architectural-Historic Inventory as an official City Document as part of the Development Plan. Such action by City Council will give the survey official status and ensure that its findings will be taken into account during future planning for the City. Secondly, the City should take all necessary steps to see that every building listed in the two top categories in the survey be submitted to the South Carolina Department of Archives and History for possible inclusion on the National Register of Historic Places. While the Historic Beaufort District is on the Register, and theoretically all buildings within its boundaries are protected, recent decisions by the National Park Service makes it mandatory that the most important structures be listed separately. The City should also take the responsibility of identifying and studying all available Federal funds and programs for preservation as they might become available. The City should also be responsible for the coordination of preservation goals and objectives with other City or State programs, and should explore the possible use of tax incentives, easements and other financial aids to owners of historic properties. A property owners advisory service should also be established in Beaufort, possibly a joint effort of the City and the Historic Beaufort Foundation, to provide advice on rehabilitation or restoration and other matters to property owners at cost or as a free service to encourage good design and compliance to the Preservation Plan and other City Codes.

But perhaps the most important contribution that can be made by the City is the continued cooperation with preservationists and the rigid but fair enforcement of the Historic District Ordinance, Zoning Ordinance and other legal devices available to protect the quality, authenticity and value of one of the most important and impressive preservation districts in the United States, Historic Beaufort.

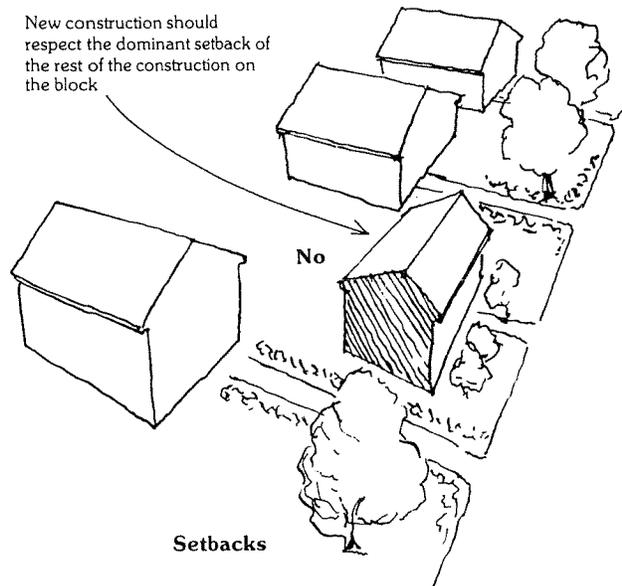
1979
Preservation Manual

can often incorporate these forms in a simplified, contemporary manner, which contributes to the continuum of the form without falsification of design. Just as there is a valid climatic purpose in continuing the principal of raised first floors in Beaufort, similar functional bases exist for incorporating many of the early architectural forms. For example, projecting facade bays of many Queen Anne houses allow a significant increase in natural light through a greater window area. Arched window heads, beyond stylistic considerations, are an honest expression of an appropriate structural configuration of brick.

Combining the principles of form and proportion, it is obvious that horizontal bands of windows, flat or gambrel roofs, "Colonial" bay windows, etc. are inappropriate elements in the District. Every attempt should be made to encourage the continued incorporation of historic forms into new construction, wherever a valid function for their use exists, and where they can be valuable assets to the spatial requirements of the building. It should be emphasized, however, that these forms should be simplified or adapted as necessary to reflect the qualities of good contemporary design.

Siting - New construction should respect the dominant setback line of existing construction. A street which is faced by residences with generous front yards is significantly impaired by new construction which abuts the public sidewalk. In addition, the landscape palette of new construction should not be discordant with that of the rest of the town (see "Landscaping").

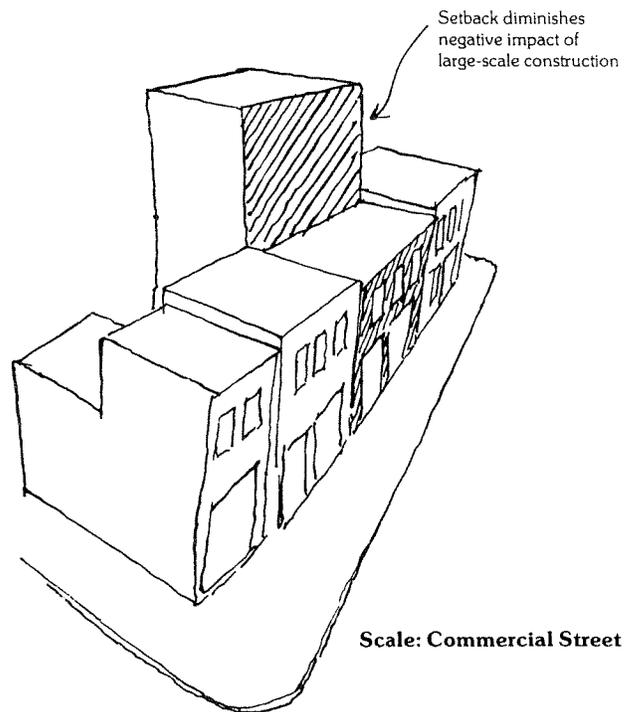
On a purely practical level, the review board should request information on the expected adult size of any proposed tree for new landscaping. Trees should not be planted so close to each other as to inhibit their growth in the future (as is the case with the Sea Island Motel parking lot), nor should they have the potential for physical interference with adjacent construction.



High Density Construction - Ideally, the Historic District of Beaufort would be able to avoid the intrusion of large scale building and mid-to-high density construction ad infinitum. However, the economic growth of a community, development pressures, and increased demands for space can periodically overshadow preservation concerns. From a realistic standpoint, the residents and review board must be prepared to deal with these inevitable (and hopefully rare) instances. While massive construction projects certainly warrant protest on legitimate preservation grounds, the board should be aware of the means

by which the negative impact of large scale buildings can be minimized. In the event that such construction is deemed a necessity by the community-at-large, it should, at the very least, conform to the following design and locational parameters.

- Large scale structures should be set back, preferably beyond the facade lines of adjacent buildings in residential areas, to avoid their becoming the dominant element in a vista or streetscape. Large scale plantings, such as live oaks, can assist in camouflaging upper stories from the pedestrian's vantage point. Large scale structures along a period commercial streetscape should be **strongly** discouraged. If, however, the situation is unavoidable, the upper stories of the facade should be stepped back. From the pedestrian's view on the street, the facade should thus appear consistent in height and proportions with neighboring buildings. The lowermost two-to-three stories should follow the building line of the street and should not create a setback, or gap, in the continuity of the commercial structures.



- "Intra-block" areas should be efficiently utilized for the majority of the building area. The central portions of blocks within Beaufort's commercial area are inefficiently utilized at present. Higher density construction should take advantage of this volume. The degree of frontage of such structures on the streetscape should be limited to the height and width of typical commercial row structures in Beaufort. Such restrictions will encourage both stepbacks in the upper facade stories and more intense utilization of inner block areas.
- The design factors of scale, materials, proportions, etc. outlined in this section should be applied equally to larger scale construction.
- Prior to admitting such construction within the District, the review board and City administration should require that an effort be made to seek acceptable alternative sites beyond the boundaries of the District. Assistance should be provided to the owner/developer in locating such sites as will be mutually beneficial to the town and the property owner.
- No development or large scale construction should be permitted which is predicated upon demolition of historic buildings for its implementation.
- Where multi-story structures include one or more stories

devoted to mechanical and/or storage space, designs should be encouraged which allow these facilities to be housed in an ell, or wing, thus reducing the overall height requirements. Height can also be limited by incorporating subgrade, or basement, levels where high water tables do not present a problem.

- Within the District, locations should be sought which best accommodate larger scale structures; e.g. areas previously intruded upon by modern construction; large lots which can be easily screened; areas containing few or no significant historic structures; areas which can best accommodate parking facilities, etc. In no case should overscaled structures be located so as to block major vistas, particularly at the terminus of streets or in such a way that they become the dominant visible architectural massing of an area.
- Many high density buildings require a substantial amount of associated parking. This can be a more significant detriment than the building itself and Beaufort cannot afford to lose additional early building stock to parking lots. Parking should either be accommodated within the structure, in an intra-block lot (screened from the street), or limited to available on-street parking spaces.

Of course, it is highly desirable to avoid large scale construction altogether by limiting the height, volume and/or plan area of new buildings. However, exceptions will inevitably occur as attested to by several existing banks and motels in the commercial sector of the District. It should be noted that “large scale” construction, as discussed here, applies equally to one or two story structures of extensive floor area. Extremely long, low continuous buildings can negatively impact the District to the same degree as mid-to-high rise structures.

Bay Street Facade Rehabilitation - The City of Beaufort has taken a major step toward the revitalization of its commercial district with the recent completion of the waterfront park development. The commercial area is largely limited to Bay, Carteret, and Port Republic Streets. Of these, the latter two have lost a great deal of their original character through the demolition of early structures, new construction and a proliferation of used car and parking lots. Bay Street, however, retains much of its early appearance, with numerous facades partially or wholly intact. While “remodelings” and new construction have taken place, the opportunity exists to preserve an historically significant commercial street and regain a period setting in mood if not complete physical detail.

Toward this end, schematic facade renovation designs were prepared as a part of these guidelines, illustrating proposed rehabilitative measures for each storefront on Bay Street. Also included are designs for all building elevations fronting on the waterfront park. Since specific building usages are transient, the schematic designs represent appropriate treatments for each particular building based on extant fabric and architectural style, rather than current function. The scope of the project did not allow for detailed structural or use analysis, nor for extensive documentary or investigatory research. Consequently, the designs depict “suggested” levels of treatment for each facade which are intended as **examples** of appropriate rehabilitation. The designs reflect four principles which should be adhered to in any renovative work.

- Do not remove, demolish, or obliterate extant historic fabric, or alter the major forms of the building.
- Respect the period and style of each structure. Do not impose artificial or contradictory stylistic elements in an attempt to “Colonialize” a building. Contemporary structures should be treated as such.
- Designs for renovation should take into consideration the impact that the work will have on neighboring structures, as

well as the practical merchandising needs of the owner or tenant.

- Preservation is preferable to restoration, which is in turn highly preferable to reconstruction. The **complete** restoration of a building facade should only be considered when 1) detailed, accurate information exists regarding its early appearance, 2) a substantial amount of original material exists, and 3) it does not dictate the removal of significant historical material from later periods.

North Side of Bay Street

703 Fordham Hardware

- Carry brick end piers to ground.
- Restore leaded glass window transoms.
- Install new wood doors and storefront windows.
- Paint sign on brick beneath second floor window sills.
- Install brick panels beneath display windows to match existing brick.
- Install canvas awning with signage along edge.

705-9

- Install continuous canvas awning with signage along edge to obscure existing brick projections.
- Construct new full-story height parapet wall with stone belt course and cap and openings so as to bring building more into scale with its neighbors.

711-13 Morrall's

- Install stone plinths at base of brick piers.
- Restore leaded glass door and window transoms.
- Restore painted signs above second floor windows at east and west bay.
- Restore painted sign along top brick band.
- Install new wood storefront.
- Install canvas awning with signage along edge.

715

- Install new wood siding and corner board.
- Install new wood storefront and cornice.
- Paint sign on display window.
- Install canvas awning with signage along edge.

719 Discount Sewing

- Install new wood cornice.
- Paint sign on brick between cornice and top of awning.
- Install new canvas awning with signage along edge.

723 Beaufort Hardware

- Remove brick veneer and repair/replace wood siding beneath.
- Restore original windows and shutters at second floor.
- Install new wood display window with cornice at head and paneled kick plate beneath.
- Install new wood entry doors at corner entrance.
- Install new canvas awning with signage along edge.

803 Verdier House

- No alterations recommended.

805 Hollingsworth Barber Shop

- Install new canvas awning to match those installed at 807-13.
- Install new wood storefront.
- Install new clapboard siding.
- Paint sign on new clapboard siding.

807-13 Allied Department Stores

- Remove brick and aluminum storefront.
- Install wooden Doric pilasters at pier lines.
- Install new wood storefronts between each pilaster.

1986
Urban Design Plan

TABLE 1

DOWNTOWN BEAUFORT
CHANGES IN DOWNTOWN BY LAND USE

Land Use Change	Number of Changes 1975 - 1985
Retail to another Retail	14
Retail to Service	9
Retail to Vacant	5
Retail to Demolition	1
Service to another Service	15
Service to Retail	7
Service to Vacant	8
Residential to Vacant	1
Residential to Service	1
Vacant to Retail	2
Vacant to Service	2
Vacant to Demolition	1
Total	66

4. REVISION CONSIDERATIONS:

The revised CBD Plan is based upon the following considerations:

1. Preserving the character and scale of downtown Beaufort.
2. Determining the manner (land use and locations) in which downtown should grow and the economic forces that cause this to happen, based upon existing unique conditions and not solely on market potential.
3. Meeting the present and future parking needs.
4. Providing the amenities and aesthetics that are needed downtown in order to keep it viable.
5. Developing a management plan and organization to adequately control new buildings and renovations (scale, character, parking, etc.).
6. Encouraging mixed use and the adaptive reuse of existing structures downtown.

G. Library/Art Gallery

Through the efforts of the City, the Library expansion project has been redesigned for two stories, creating physical facilities for cultural programs, rooms for art instruction, galleries, meeting rooms, administration, and other needs.

H. Craven Street

Portions of the two blocks, south of Craven Street between Charles and Scott could serve as a valuable area for expansion should economic forces pressure the downtown area into growth. As previously mentioned, this study is not based upon a market analysis, but is based upon land potential should the economics for growth occur. Craven Street is a charming unique street with its own scale and character.

It is the frontage for the historic Arsenal and the Secession House. It is lined with palmettos and oaks, a church, offices and residences. This existing character must be respected. The proposed uses along the two blocks fronting on the south side of Craven Street are predominantly residential with townhouses and offices. New construction could fill in between existing structures. Parking to serve these uses could be located southward in the middle of the block.

I. Port Republic Street

Port Republic Street is proposed to continue as predominantly retail stores and offices. The street frontage could also be served by a parking lot in the center of the block. The two parking lots would channel pedestrians toward Bay Street via West and Scott Streets

J. Hospital:

The hospital is being studied for future expansion needs in its present location. Plans are being considered to take advantage of it's great site location and dramatic views.

1989
Land Use and Preservation Plan

b. Transportation Goal:

- To provide a coordinated regional transportation system with easy accessibility and the safe, efficient movement of people and goods that respects and promotes the City of Beaufort's historical and environmental qualities.

c. Environmental Goal:

- To recognize the contribution of the natural environment to the health, safety and economic well-being of the residents of Beaufort and to strive to maintain the integrity of the City's marshlands and water quality.

d. Community Facilities Goal:

- To develop the wide range of city services and community facilities essential to the City and to locate these in a manner allowing efficient operations.

3.2 Major Land Use Issues/Policy Recommendations

This section discusses the major land use related issues confronting the City with respect to its future growth and development. The ways to address the issues and work toward satisfaction of the stated goals are listed in the form of policy recommendations. Issues 1-6 relate to the accommodation of anticipated change and growth, and Issues 7 and 8 relate to the maintenance and improvement of the quality of community facilities.

Issue 1: Protection and Enhancement of the City's
Historic Resources

The Landmark Historic District, which occupies approximately 133 blocks of the City, is a positive asset that plays a vital role in the economic vitality of the region. Beaufort is recognized nationally for its unique collection of 18th and 19th Century residences, churches, and commercial and public buildings. The attractiveness of the community and its proximity to the coast have resulted, and will continue to do so, in growth pressures which have threatened these resources.

Some of the major growth pressures which impact the continued preservation of these resources are:

- Several historic residential properties have been converted to non-residential uses with particular focus being placed on the area bounded by Carteret, Greene, West, and Boundary Streets. The adaptive reuse of residential structures to retail or office commercial uses is viewed with mixed opinion by area property owners who fear degradation of the residential character of many of the area's blocks. In general, the neighborhood residents felt that Charles and Bladen Street were more appropriate areas for development, redevelopment, and adaptive reuse. Specifically, adaptive reuse should be encouraged on Charles Street, thereby maintaining its

residential scale. Bladen Street would accommodate new development and redevelopment due to the number of vacant parcels.

- The strict adherence to requirements of the Board of Architectural Review (BOAR) and the Historic District Zoning regulations for those structures within the Landmark District which are not historically significant and are non-contributing, limits the opportunity for new construction, demolition, and alterations.
- Density of land use in the Historic Residential District is a concern to many residents who wish to maintain the present lot coverage of historic houses and outbuildings in the Landmark District. Another concern is the threat of high density construction in the core commercial district, which may be in contrast to the massing, scale, and height characteristics of the area.

Policy Recommendations

- 1.1 The community should:
 - a. adhere to the recommendations of "A Preservation Plan for Historic Beaufort, South Carolina" and its recommendations for update of the "Beaufort Preservation Manual";
 - b. continue to support the jurisdiction of the Board of Architectural Review (BOAR); and,
 - c. continue to support the residential character of the Landmark Historic District.

1990
Preservation Manual Supplement

Chapter 3

New Construction, Additions, Demolition, and Signage

Introduction

Both Beaufort's current Zoning Ordinance and the draft Amendment require that all demolition, new construction, and additions or alterations to existing buildings under BOAR jurisdiction receive a Certificate of Appropriateness prior to issuance of a building permit. The Ordinance further provides that in reviewing applications for a Certificate of Appropriateness, the BOAR will consider among other things "the general design, the character and appropriateness of design, scale of buildings, arrangement, texture, material and color of the structure in question, and the relation of such elements to similar features of structures in the immediate surroundings". The Ordinance further stipulates that grounds for refusal to grant a Certificate of Appropriateness may be:

arresting and spectacular effects, violent contrasts of materials or colors and intense or lurid colors, a multiplicity or incongruity of details resulting in a restless and disturbing appearance, the absence of unity and coherence in composition not in consonance with the dignity and prevailing character of the neighborhood in the case of a new building.



Unfortunately, it is easier to define inappropriate construction than it is to prescribe appropriate construction. New construction and additions in Beaufort should blend

harmoniously with the historic fabric of the city. They should have a positive visual and functional relationship to the historic buildings already in the District. New construction and additions should enhance the perceptual quality of the District. These guidelines are intended to encourage excellent contemporary design that is compatible with the character of the District. Specific guidelines follow for new construction and additions to existing buildings, along with a discussion of the issues raised by demolition.

Signage guidelines were included in the *Manual* and were updated and expanded in 1989 by a Pride-of-Place project team sponsored by Main Street Beaufort, USA and led by Thomason and Associates of Nashville, Tennessee. A discussion of these guidelines is also included in this Chapter.

Design Guidelines for New Construction

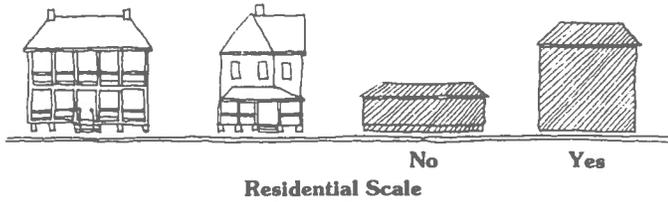
The guidelines below are adapted with few changes from the *Beaufort Preservation Manual*. Restoration, "period architecture" and the rigid quotation of architectural elements and details is not their intent. Rather, their intent is the preservation of the cohesive ambience of the District by compatible, sympathetic, and contemporary construction. They are written with the understanding that the more strict are the guidelines for new construction, the more severe are the limitations placed on creative and innovative design solutions.

The design guidelines below are intended to clarify the elements and principles of appropriate design in such a way as to allow maximum design freedom while allowing plans for new construction to be assessed fairly, objectively and consistently. These guidelines encourage the designer of new construction to consider existing historic buildings as a starting point in the design process, and not as the final goal.

The following guidelines should be considered in permit applications for the construction of new structures under BOAR jurisdiction:

Scale: New construction should reflect the dominant cornice and roof heights of adjacent buildings. This guideline becomes more important as a given street increases in density. In cases where the street does not have a dominant or discernable rhythm of cornice heights, the decisions of the BOAR should

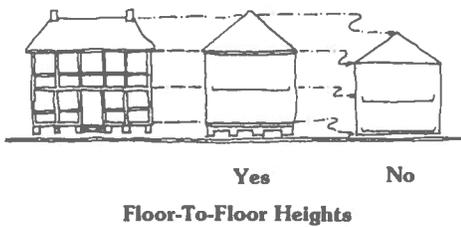
be more affected by the considerations of absolute height and massing described below.



Elevation of the first floor: The typical residential street in the Historic Beaufort District is fronted by houses with prominent steps leading to raised first floor porches. The raised floor is still an excellent response to the climatic conditions of Beaufort as well as the fact that much of the Historic Beaufort District lies within the 100 year flood plain of the Beaufort River. Therefore, raised first floors should be encouraged for new construction wherever possible.



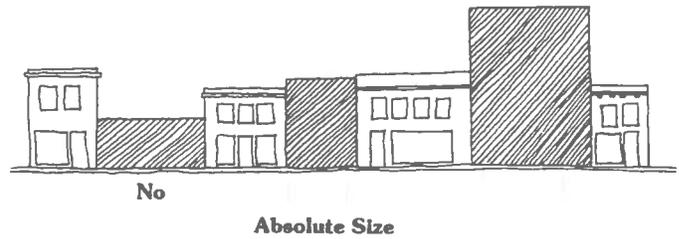
Floor-to-floor heights: This important element of scale is often ignored in new construction, which tends toward lower ceiling heights. The loftier rooms of the nineteenth century provided a far more appropriate response to climatic conditions. The Ordinance in fact specifies that in the HR Zoning District, floor-to-floor heights of new construction must be within 10% of adjacent historic construction. In other Zoning Districts, where a relatively consistent floor-to-floor height is expressed in the facades of a given street, new construction should be encouraged to conform.



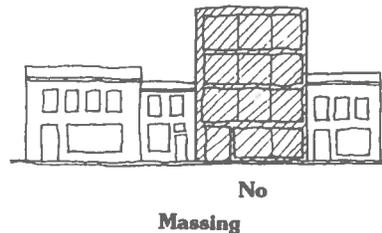
Bays, windows and doors: The scale of a building is strongly affected by proportions, both of the building as a whole, and

of its principal facade components. Proportions, in turn, are largely dictated by the height/width relationships of door openings, window openings, and porch column spacings. These features also divide the building visually into what are commonly termed "bays". For example, a first floor facade which contains four windows and a central door is generally referred to as "five bay". The facade of a proposed building should draw upon the proportion and number of bays contained in neighboring structures, if it is to appear compatible with its surroundings.

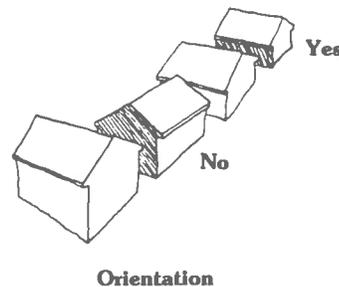
Absolute size: When the scale of neighborhood buildings, or those of an entire community are relatively consistent, new construction should be restricted from drastically altering these relationships. In the case of Beaufort, the two and three story structure is the norm, and structures which digress from this standard to any great degree seriously impact the Historic Beaufort District. If large scale construction is to be allowed, particular attention should be given to the location, siting, set backs, and facade treatments of the proposed building.



Massing: The facades of new construction should reflect the feeling of lightness or weight of its neighbors through the use of similar proportions of solids (siding or walls) to void (window and door openings) and projecting bays and overhangs.



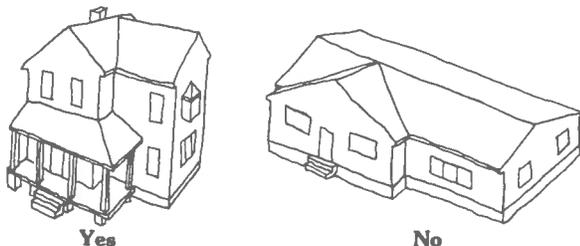
Orientation: The principal facade of new construction should be oriented in the same direction as the rest of the buildings on a street. Facades of new construction on a corner site should differentiate between the two streets. That is to say, new construction with two primary facades or two relatively undifferentiated primary facades is inappropriate.



Proportions: New construction should relate to the dominant proportions of the styles present in the immediate neighborhood. The proposed design should reflect closely the height/width ratios of overall building proportions as well as that of doors, windows, and porch bays.



Proportion Of Openings



Proportion Of Volumes

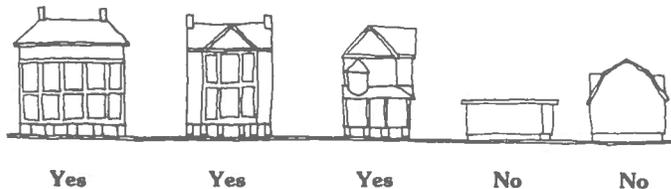
Materials: New construction should use materials in a manner sympathetic to the historic buildings in the Historic Beaufort District. Materials should be of similar or complementary color, size, texture, scale, craftsmanship, and applicability to function performed.

It should be noted that the sympathetic use of materials does not imply that materials used in new construction will replicate the old in detail, nor that new construction attempt to imitate historic structures. Rather, it is a matter of determining the compatibility of the new with the old. Certain materials are potentially so visually intrusive that their use for new construction in the Historic Beaufort District should be discouraged if not forbidden. These materials include:

- * exposed concrete masonry
- * painted concrete masonry
- * ornamental pierced concrete masonry screens and walls
- * "antiqued" brick
- * vinyl and metal siding
- * wrought iron and aluminum porch columns
- * exposed chain link fencing
- * carpeted porch floors
- * flush exterior doors
- * inappropriate window treatments:
 - o jalousie windows
 - o glass block
 - o picture windows
 - o windows with horizontal glazing
- * asphalt siding
- * unpainted wood

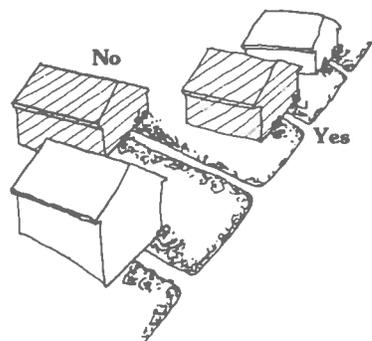
Forms: New construction should reflect and be sympathetic to the form of adjacent historic structures. These sympathetic historic forms include hip and gable roofs, projecting bays or ells, the shapes of window and door

heads, architectural chimneys and overall porch configurations. Conversely, horizontal window bands, flat or gambrel roofs, and "colonial" bay windows, etc. are inappropriate elements in the Historic Beaufort District. Every attempt should be made to discourage their use in new construction in the District.



Variety Of Appropriate Forms

Siting: New construction in the Historic Beaufort District should respect the dominant set back line of existing construction, over and above what might be the setback lines prescribed in the Ordinance.



Inappropriately Large Set-Back

High density/large scale construction: It is possible that development pressure in the City of Beaufort will eventually result in proposals for projects involving structures larger than the predominant scale of the District. Whenever possible, alternative sites for large structures should be sought outside the Historic Beaufort District, and the City of Beaufort should provide assistance to the applicant in identifying every possible alternative site that would mutually benefit the applicant and the City. If alternative sites are not available, the means by which the negative impact of large scale buildings must be minimized are as follows:

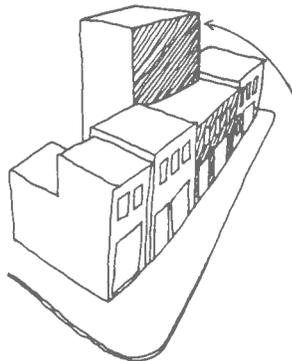
- * Seek the locations within the proposed Historic Beaufort Overlay District which best accommodate larger scale structures, such as areas previously intruded upon by modern construction, large lots which can be easily screened, areas with a few or no historic structures, or areas which can best accommodate parking facilities.

- * Large scale structures should be set back from every street on which they have frontage, including the rear or bay facades of the south side of Bay Street, to avoid becoming the dominant element of a vista or streetscape. Large scale structures along a period commercial streetscape should be strongly discouraged. At the very least, the upper stories of the facade should be stepped back, as was done in the design of the Palmetto Federal Bank.



Palmetto Federal Bank

- * "Intra-block" areas should be used for the majority of the building area. This would require set-backs from each of a building's street frontages, including the rear or bay facades of buildings on the south side of Bay Street.



Large Scale Construction
Set-Back To Respect The
Dominant Cornice Line
At The Street

Set-Back For Large Scale Construction

- * Apply to larger scale construction the same design guidelines regarding scale, materials, proportions, etc., that are outlined in this section. Two recent buildings in Beaufort are instructive. While no one would confuse the Palmetto Federal Bank with a historic building, its composition, meeting of the street, use of small scale elements, and upper story setbacks make it compatible with the Bay Street Commercial District. Conversely, the South Carolina National Bank, which uses the architectural vocabulary of Beaufort at the wrong scale and setback from the street, is not compatible with the character of the town.

- * Do not demolish historic buildings to make way for new or large scale construction.
- * Incorporate parking within the structure, in a lot screened from the street, or limit it to available on-street parking spaces.

Secondary Structures: Secondary structures include but are not limited to garages, studios, and guest houses. Similar to additions, they should be subordinate to the primary structure on the lot and visually complementary to the existing building. New secondary structures should in no way compromise the historic character of the existing structure on the lot. Ideally, the secondary structure should be located so as not to be visible from the street. In any case, secondary structures should be located as far to the rear of a lot as possible.



Garage At Rear Of Lot

Secondary structures should be free-standing and not linked to the primary structure. The design guidelines above regarding proportions, massing, materials, form, orientation, and siting apply to secondary structures as well.



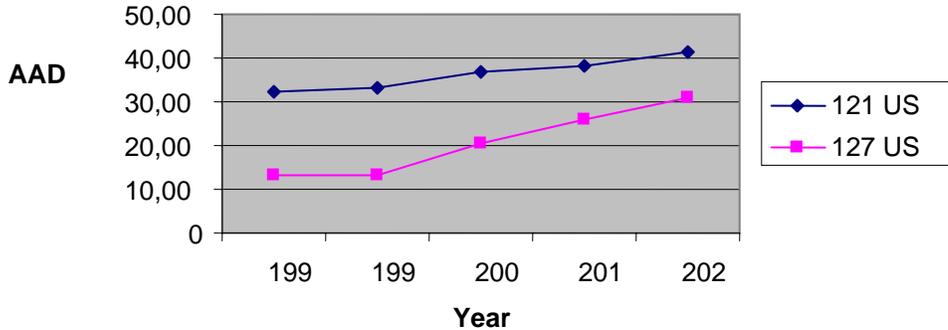
Secondary Structure Linked To Primary Structure

Archeological Resources

The Secretary of the Interior's Standard #8 requires the preservation and protection of archeological resources. There is a strong likelihood that excavation for new construction in the Historic Beaufort District will involve archeological resources. While efforts should be made to consider and protect those resources, the extent to which this consideration

2004
Comprehensive Plan

**Boundary Street
Average Annual Daily Traffic**



AADT= Average Annual Daily Traffic Counts
Source: South Carolina Department of Transportation

Bridges

The Woods Memorial Bridge, built in 1960, is a two lane, steel swing bridge, which provides the only direct access to Lady’s Island from the City of Beaufort proper. The traffic flow delays and congestion associated with the swing bridge openings have been a long-standing issue with the City particularly the impact on the traffic flow in the core downtown. In recent years, the traffic congestion has increased as a direct result of the development on Lady’s Island and growing tourist industry. The City is also considering restricting vehicles with more that 3 axles from crossing the bridge and limiting bridge openings to one per hour on the half hour. A bridge opening schedule has been implemented which stipulates that the bridge remain closed between 7-9 am and 4-6 pm.

Parking

The most recent parking study for the City of Beaufort was the Parking Master Plan, completed in 1998 by Walker Parking Consultants. The Parking Master Plan found that sufficient parking spaces are available to accommodate peak parking demand in the downtown area. The study found that there is a perceived parking problem due to insufficient short-term parking spaces and downtown employees parking in spaces which should be reserved for visitors and patrons. The Master Plan examined parking supply, parking demand, parking adequacy, parking perceptions and parking management. The plan also presented a number of parking management initiatives to promote better utilization of existing spaces and address both short and long term parking needs in downtown.

A previous parking study was completed in 1993 by Main Street Beaufort, USA. The study presented an inventory of existing parking, examined the parking needs related to potential residential development in the core downtown, examined levels of accessibility for the disabled,

and offered recommendations for additional parking and for enhancement for disabled parking. An earlier study was conducted by a private consulting firm in 1984 and addressed an 11 block area of the downtown. The study was prompted as a result of community and business concerns that a parking space deficiency existed. The study concluded that there was no major shortage; however, the study did identify a slight shortage of parking in the central waterfront area and potential shortage in the core district based on proposed development an inn and the expansion of the library. Both of these activities have occurred and considerable development and redevelopment activity has taken place in the downtown since 1984.

Previous recommendations for parking have included the development of a public parking facilities, relocation of meters, the City leasing property for parking, the development of resident permit system, and improved signage and lighting. One other alternative that has not been explored is the use of remote parking lots and shuttle service to the downtown. This approach has been successful for the cities of Charlotte, NC and Charleston, SC and should be further studied. The potential for similar opportunities may exist between the City and the Lowcountry Regional Transportation Authority (LRTA) to develop a cooperative approach to address the parking supply downtown through implementation of a shuttle service through possible funding through the federal government.

Transit Service

Currently the Lowcountry Regional Transportation Authority (LRTA) has one route that provides limited local service in the City of Beaufort. This service primarily provides transportation to workers commuting to resort areas outside of the City.

The most recent mass transportation plan is the Lowcountry Public Transit Coordination Feasibility Study: A Public Transportation Strategy, which was completed in 2003, on behalf of the Lowcountry Council of Governments by Day Wilburn Associates, Inc. The plan was intended to assess transit needs and opportunities in the region (defined as Beaufort, Colleton, Hampton and Jasper Counties); how current services are addressing identified needs and to identify opportunities to improve existing or add new services. More detailed objectives of the plan included:

- determining the relationship between economic development and the provision of coordinated public transit services in the area;
- identify coordination possibilities, including improving or expanding services;
- and formulating an action plan for implementation which responds to coordination needs and provides additional services.

The study found that there is a significant population that either needs transit or appears likely to use it if new or enhanced services were available. The potential users include low income residents, minorities and people 65 years and older, as well as tourists, students and staff at post-secondary educational institutions, the military, and residents and visitors with out of region destinations or origins.

The Lowcountry Public Transit Coordination Feasibility Study outlines an overall framework for the development and implementation of service revisions and new services. A few basic objectives were included in the development of the transit concept outlined in the study, which

Policy 3.C: Maintain high quality service delivery in response to new development.

Strategy 3.C.1: Require developers to pay for the supporting services at the time of occupancy through impact fees with revenues directed to infrastructure and service maintenance and enhancement.

Status Report: The City of Beaufort adopted a Parks and Recreation Impact Fee for all new construction in 1999, and is considering adopting a traffic impact fee. The City of Beaufort also collects a Stormwater Utility Fee on behalf of Beaufort County.

Strategy 3.C.2: Prepare a sidewalk development plan for the City. Include requirements for real estate developers to install sidewalks as a normal part of new commercial or residential development.

Status Report: The updated City of Beaufort Unified Development Ordinance Section 8.2.A.11 requires certain subdivisions to include sidewalks in their development.

Policy 3.D: Establish and enforce anti-pollution standards.

Strategy 3.D.1: Establish and maintain standards for allowable pollution caused by development in the City and require all development to meet those standards.

Strategy 3.D.2: Supply a sufficient number of building inspectors to enforce codes and standards for development as well as redevelopment.

Status Report: A full time Codes Compliance Officer position was created and filled in 2000.

Policy 3.E: Establish standards for activities and development that impact the environment and include the consideration for these standards as part of the development approval process. Include standards for solid waste, water quality preservation, and general ecological and environmental impact.

Strategy 3.E.1: Consider adopting a formal policy and incentive package to promote the use of high performance green building standards in all new construction as well as renovations.

GOAL 4: Maintain an efficient and environmentally sensitive transportation system. The future transportation network for the City of Beaufort and its adjacent surroundings should be structured so as to make the daily activities of its citizen's flow naturally, conveniently and safely from one point to another while protecting the natural and historic character of the City.

Policy 4.A: Work to alleviate congestion on downtown Beaufort roads and throughout

the City; base future transportation planning and decisions on the goal of maximizing the potential of Beaufort's existing network of streets. Encourage connectivity and multiple paths of access within the Downtown and extending to newly developed areas.

Strategy 4.A.1: Implement traffic light systems with computer coordination that are capable of preventing delays and gridlock while enforcing safe speeds.

Status Report: All of the traffic lights in the City have been wired for computer coordination and the light bulbs have been replaced and upgraded with Light Emitting Diode (LED) lights.

Strategy 4.A.2: Consider additional bridges as solutions to the problem of congestion due to the Woods Memorial Bridge. New bridges should seek to enhance the distributed street network through locating where current bridges do not exist. A transportation study should be prepared with the focus of determining possible sites for additional bridges.

Status Report: A Northern Beaufort County Bypass Feasibility Study was conducted by Wilbur Smith Associates on behalf of Beaufort County in August, 2003.

Strategy 4.A.3: Seek to minimize the number of curb cuts on City streets by encouraging the sharing of curb cuts by multiple developments and by closing curb cuts where appropriate as part of a property redevelopment.

Strategy 4.A.4: Develop a plan for curb and gutter on-street parking throughout the City of Beaufort.

Policy 4.B: Protect the Historic District from the damaging effects of through traffic. The knowledge that through traffic on Carteret Street and other streets in the Historic District damages historic structures should be considered as a top factor in any transportation planning decisions.

Strategy 4.B.1: The City should conduct a study to determine what an acceptable volume and type of traffic should be on such streets and future transportation plans should target reducing traffic to those levels. A second study should seek a path for an alternate route for trucking traffic into, out of, and through the City.

Policy 4.C: Solve parking problems through innovative parking solutions which enhance the historic character of Downtown and improve accessibility throughout the City.

Strategy 4.C.1: A parking study should be prepared to determine the best solution to resolve parking problems in a manner that is sensitive to the historic structures and character of Downtown.

Status Report: A Parking Master Plan for the City of Beaufort was developed by Walker Parking Consultants in 1998.

Policy 4.D: Facilitate non-automotive travel in the City.

Strategy 4.D.1: Beaufort should actively support and seek to implement the City of Beaufort Greenway Plan and the Beaufort Open Space Master Plan as an initiative to build an interconnected system of walking and biking paths throughout the City and among City parks.

Status Report: The Woods Bridge walkway was completed in 2003, which provides a safe route of travel for pedestrians and bicyclists across the bridge. The Pigeon Point Greenway was completed in 2001.

Strategy 4.D.2: Additional improvements should be made to the sidewalks and other aspects of the pedestrian realm in Downtown in order to facilitate walking and encourage one-time parking for multiple destinations.

Status Report: Streetscape improvements were completed on Port Republic Street in 2002.

Status Report: The Pedestrian Infrastructure Improvement Master Plan was developed in 2004 and should be implemented as soon as possible within the City. The Plan should also be expanded to include sidewalks and bike routes and paths throughout the City.

Strategy 4.D.3: The use of bicycles as an alternate to automotive transportation should be encouraged through provision of bike racks and lockers in the downtown area.

Strategy 4.D.4: Require that adequate pedestrian facilities such as crosswalks and signals, as well as bike lanes, be included in all new road and street construction.

Strategy 4.D.5: Pursue the retrofitting of all existing roads, beginning with key intersections, to include adequate and safe pedestrian facilities and bike lanes.

Policy 4.E: Encourage appropriate public transportation facilities and services.

Strategy 4.E.1: Work cooperatively with the Lowcountry Regional Transportation Authority and Beaufort County to implement the Day Wilburn Associates "Lowcountry Public Transit Coordination Feasibility Study: A Public Transportation Strategy," which was completed in 2003.

Strategy 4.E.2: Encourage the private or non-profit development of shuttle services in the City to serve the needs of residents and tourists alike.

Policy 4.F: Develop stronger linkages with the County transportation planning function to insure the City's transportation issues and goals are represented at the County level

2008
Historic Preservation Plan



Preservation Plan Update City of Beaufort, South Carolina

Commercial buildings were constructed in the late eighteenth century or early nineteenth century. The Verdier House, an 1804 Federal style house owned and operated by the Historic Beaufort Foundation, is a unique surviving example of an 18th century planter's house located in downtown. A variety of important civic and cultural institutions are located in downtown buildings, including the Beaufort government in the Beaufort City Hall and the Historic Beaufort Foundation in the Verdier House.



21. Bay Street Trading Company

Historic downtowns are complex organisms influencing and being influenced by a variety of economic and community related factors, including the availability of markets to tap and the effectiveness of tapping those markets, as well as the capacity and configuration of historic buildings that constitute the architectural and historic substance of the district. The Beaufort downtown is no exception.



22. Verdier House on Bay Street

The emerging commercial areas along Boundary Street and Ribaut Road are impacting the downtown historic commercial area. Other issues and efforts are impacting the downtown commercial area include rising retail space rents, the lack of a sense of security at night, and continued efforts to attract residential living in downtown. There are other development proposals that can impact the historic character of the downtown, including the proposed downtown parking deck, construction of a new three-story mixed-use building at the corner of Bay and Carteret Streets, and the expansion of the Beaufort Inn.



Preservation Plan Update City of Beaufort, South Carolina

5. Infrastructure in Historic Districts

5.1 Infrastructure in Historic Districts – Traffic & Circulation

Issue

The planning process revealed a number of aesthetic- and traffic-related concerns and ideas that impact the character and livability of the historic district and its neighborhoods. Auto-related conflicts can reduce the quality of life in residential areas. Addressing them in the historic district is a preservation issue.

An aesthetic issue related to transportation infrastructure includes the historic brick streets currently covered by asphalt paving. During the planning process, strong support was expressed for recapturing the character of historic brick streets and maintaining them as character-defining features of the district.

Traffic-related issues in the historic district include high rates of vehicular speed throughout district, cut-thru traffic, and poor visibility at intersections due to parked cars and over-landscaping. A traffic-related issue involves the parking capacity & availability in the historic district, particularly in the downtown commercial area. There are several important issues related to the planned parking deck for the site at Port Republic and Scott Streets regarding its impact on the visual character and traffic circulation in downtown.



82. Traffic on Craven Street



83. On-Street Parking on Port Republic



Preservation Plan Update City of Beaufort, South Carolina

Traffic & Circulation Recommendations

Conduct traffic study to provide guidance to solve concerns of high rates of vehicular speeds, cut-through traffic, and unsafe intersections in historic neighborhoods.

It is important to address traffic concerns while maintaining the historic character of the district and improving livability of its neighborhoods. Possible solutions to address traffic problems include the use of four-way stops, particularly in the residential areas of the district, maintenance of traffic signal synchronization to regulate the flow and speed of traffic, and appropriate placement of speed limit signage and visible consistent enforcement to reduce existing high rates of speed through the district.

It is also desirable to remove the forced-turn islands in the district and seek alternative traffic calming measures such as appropriately-placed speed humps, cushions, and tables, raised crosswalks, textured pavements, and intersection and midblock narrowings for safe crossings.

Enforce TMAC ordinances to ensure that tour companies maintain traffic and pedestrian safety and character of historic neighborhoods.

Preserve historic alleys as important features of the district and amenity for abutting property owners. Do not allow alleys to become inappropriate development opportunities.

The building height and design of the proposed parking deck should minimize the physical impact of the structure on the character of adjacent properties and streetscape. Likewise, it is important that the parking structure have ground-floor retail along Port Republic and Scott Street frontages in order to maintain the pedestrian scale and character of these streets.



84. Horse-Drawn Tour Carriage on Craven Street in Old Commons



85. Downtown Site for Parking Deck at Scott and Port Republic Streets



Preservation Plan Update City of Beaufort, South Carolina

The increased number of vehicles attracted to the parking deck has the potential of impeding circulation on the relatively narrow commercial and pedestrian streets between the downtown and Old Commons. Measures should be taken to minimize the potential for circulation issues and traffic accidents, and to maximize pedestrian safety in the area. It is important that business owners and employees commit to using the parking deck on a regular basis in order to retain the surface parking spaces for customers and patrons.

Outcome

The implementation of these recommendations will improve traffic volumes and circulation, and maximize automobile and pedestrian safety in the historic district. These recommendations will also improve identifying and directional signage to and throughout the historic district.

2009
Comprehensive Plan



▲ DOWNTOWN INFILL

DEVELOPMENT TOTALS

Extend Waterfront Park Redevelopment of Post Office Mixed Use Infill

Ground Level Commercial

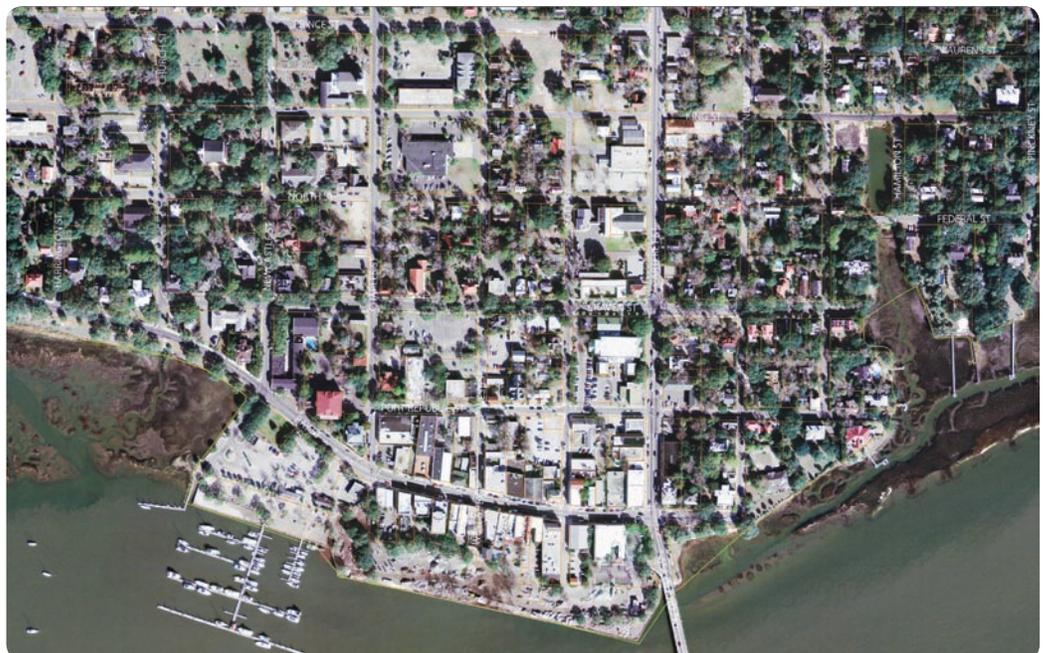
Retail/Office: 212,250 SF

Residential

233 Units on Upper Stories
 19 Detached Homes
 252 Total New Housing Units

Parking

Deck A: 208 Spaces
 Deck B: 368 Spaces
 Existing Surface spaces lost to New Development: ~250
 Net Gain: 326 Parking Spaces



▲ EXISTING CONDITIONS



Downtown Beaufort, SC

need of less than 10% (about 100 additional spaces). In truth, the historic growth rate will not generate new parking demand. Rather, it will be the replacement of existing surface lots with buildings as well as the construction of new development that will generate the need. A comprehensive approach to parking in the downtown should include the potential for remote lots with shuttle services, more efficient pricing strategies, improved signage and wayfinding, as well as the potential for a well-located structure. [\$\$\$] [O] [1-5]

CS 2.4 Reconfigure the marina parking lot and maximize its return on investment. The current arrangement of parking spaces, the tour bus dropoff, and carriage ride base of operations in this lot is inefficient. A quick study revealed that an additional 10 spaces can be achieved through basic re-striping and slight alteration. In addition, a likely outcome of the Parking Strategy Plan will be a demand- and location- based pricing strategy that will help to yield more money from this lot to offset any needed improvements as well help to fund other parking initiatives. [\$\$\$] [O+C] [1-5]

CS 2.5 Construct a public parking structure. As previously noted, the anticipated replacement of existing surface lots with buildings will likely drive the demand for parking structure in the next ten years. Because of the cost of such a structure, a community like Beaufort can typically only afford to construct one such facility in a 10-20 year period. As a result, it is important that due consideration be given to it location, efficiency, and design to ensure that it fits in appropriately with the scale of the area. Ideally, the structure should be designed to have liner shops/offices so as not to disrupt the flow of pedestrian throughout the downtown. [\$\$\$\$] [O+C] [10-20]

CS 2.6 Expand the waterfront park west to reclaim the existing marina parking lot. Great care and expense have been given to the Waterfront Park through the years. This park, in combination with the three blocks of Bay Street that border it form the most vivid memories of Beaufort for visitors and residents alike. It is unfortunate, therefore, that the wide public view of the Beaufort River that is afforded nearly to Ribaut Road is interrupted with the marina surface parking lot. Does the largest parking lot in the downtown area need to be located on the waterfront? The adopted 2002 Master Plan by designed by Sasaki suggests a more limited amount of parking and an expansion of the park. Also worth considering is the suggestion made by the conceptual infill/redevelopment plan on the previous page, that indicates some limited private mixed-use development occur on the eastern edge of the lot to provide the needed capital to partially fund a parking structure elsewhere to replace the lost spaces. [\$\$\$\$] [C] [10-20]

CS 2.7 Encourage increased density of development in the downtown. For the downtown to be more than simply an outdoor museum it must have a sufficient level of development to provide off-peak (daily and seasonal) activity. Ideally, this will translate into an increased number of daytime jobs and full-time residences. The goal of this intensification is the ability to support neighborhood-based stores such as a small grocery store, a full-sized drug store, or both. This will enable the residents to be able reach more of their daily needs on foot or by bike and lessen congestion on the surrounding thoroughfares. To achieve this goal, infill and redevelopment on key parcels will be necessary. The opportunity map on the previous page illustrates a number of key opportunities for new development. These are expected to be investments made largely by the private sector with a large amount of governmental advocacy (e.g., improved regulatory structure, streamlined permitting process) and a minimal amount of monetary assistance. [Private \$] [O] [On-going]



VIEW LOOKING WEST ALONG PORT REPUBLIC STREET AT SCOTT STREET



▲ INFILL BUILDING WITH LINER SHOPS AND UPPER STORY PARKING

2014
Downtown Revitalization Plan



RETENTION & RECRUITMENT

Problem: Downtown does not have a sufficient mix of retail and restaurant offerings; property owners are sometimes more interested in having a tenant rather than having the *right* tenant.

Solution: Develop a multifaceted recruitment strategy for downtown retail/restaurant/mixed-use.

How?





RETENTION & RECRUITMENT

Recommendation: Develop smaller spaces for retail incubation; structured parking could provide this (as well as live/work opportunities).





TYING IT ALL TOGETHER

Recommendation: Continue to explore structured parking, especially as part of a mixed-use development.



2014
Civic Master Plan

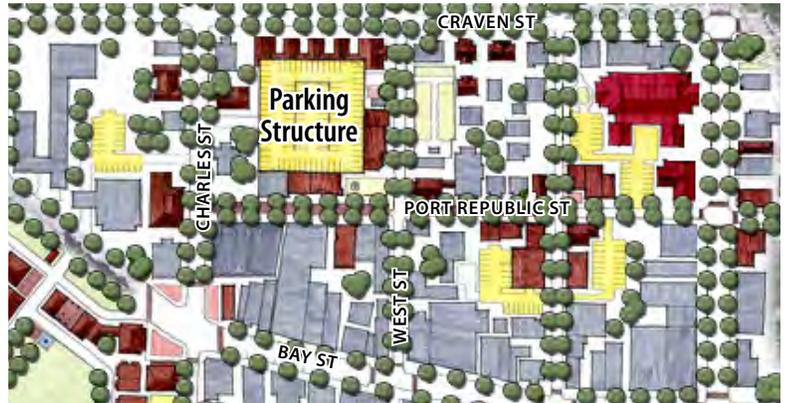
3.3 Port Republic Street

Sector: 1

Project Type: Public | Private

Civic Investment Required: Port Republic
Festival Street

This plan proposes infill commercial development along Port Republic Street to extend the shopfront environment of Bay Street through downtown. New retail opportunities would be facilitated by a civic investment that transforms Port Republic Street into a festival street that can be easily closed off to vehicular traffic and function as an event space. The new Port Republic Street design would not have a raised curb, but would instead utilize consistent decorative paving from building face to building face with intermittent bollards to separate pedestrians from vehicular circulation. This mix of pedestrian and vehicular environments at an intimate scale would slow traffic speeds and better serves the retail character of the area.



▲ CONCEPTUAL ALTERNATIVE INFILL DEVELOPMENT



▲ EXISTING CONDITIONS



▲ CONCEPTUAL ILLUSTRATION OF INFILL DEVELOPMENT ALONG PORT REPUBLIC STREET (looking west)



▲ CONCEPTUAL LOCATION OF A PARKING STRUCTURE



▲ EXISTING CONDITIONS (view from Craven Street Looking East)

The conceptual infill scheme for this area also imagines a prominent new commercial building on a current parking lot at the west end of Port Republic Street along Charles Street to provide a visual terminus of this pedestrian-oriented area.

3.4 Parking Structure

Sector: 1

Project Type: Public | Private

Civic Investment Required: Parking Structure

See Also: 2.2

Acknowledging the existing parking issues in downtown that will be exacerbated by additional attractions, one conceptual location this plan illustrates is a parking structure in the middle of the block bound by Port Republic Street, Craven Street, Charles Street, and West Street. As shown in the rendering below, the parking structure would be concealed with ground floor uses or with mixed-use buildings to shield the parking area from view.

On Port Republic Street and West Street, a new commercial space would activate the street for pedestrians. On Craven Street, apartment units would line the parking structure and create



▲ CONCEPTUAL PARKING STRUCTURE LINER BUILDINGS WITH SIDE COURTYARDS (OPTION A)

an appropriate transition to the residential neighborhoods north of the downtown area.

The parking deck would replace the surface parking spaces displaced by new development on the Marina site (see Section 2.2) and provide convenient access to downtown. The structure could accommodate approximately 280 vehicles and promote a vibrant retail environment where visitors park once and then walk between shops. All together, the parking deck would alleviate a parking problem, support businesses, residents and visitors, and extend the Bay Street commercial core to the north without ruining the historic streetscape with concrete walls.

Why Is a Parking Structure Needed in Downtown Beaufort?

According to a recent parking study, the City of Beaufort's parking demand will increase by approximately 100 spaces (less than 10% of the current demand) in the next 5-10 years. However, the anticipated redevelopment of existing surface lots will create a much greater need for new parking spaces in the future and drive demand for a new parking structure.

A parking structure will support the downtown infill development described in the Civic Master Plan in a central, walkable location.

3.5 Carteret Street

Sector: 1

Project Type: Public | Private

Civic Investment Required: Minor Streetscape Improvements

See Also: 2.6; 7.1; 10.2

Carteret Street Corridor

The Carteret Street corridor begins where Boundary Street (east of Ribaut Road) meets Bellamy Curve, and connects to Lady's Island via the US 21 (Business)/Sea Island Parkway Bridge. It is the most significant north-south corridor in downtown Beaufort, and connects key project sites, like Old City Hall, and institutions, like USCB and the Beaufort County Library. Carteret Street has two vehicle travel lanes for the majority of its length, with on-street parking on either side. Carteret Street supports a fairly wide variety of service businesses, including real estate offices, insurance and financial planning firms, and attorney's offices. It also supports several restaurants, a hotel, and religious facilities such as Carteret Street United Methodist and St. Peter Catholic Church chapel.



▲ CONCEPTUAL PARKING STRUCTURE LINER BUILDINGS WITH SIDE COURTYARDS (OPTION B)