
City of Beaufort - Town of Port Royal – Beaufort County
Joint Metropolitan Planning Commission
Annexation and Rezoning Analysis PR-AX 03-14
Meeting Date: September 15, 2014

Applicant

Lady's Island Real Estate Group LLC.

Site

Approximately 3.94 acres There is a single family home at 547 Broad River Blvd.
The plat map reference for this property is: District 100, Map 28, Parcel 100A 0000. The acreage is located at 547 Broad River Blvd.

Present Zoning

The parcels are currently zoned Suburban under Beaufort County's Zoning and Development Standards Ordinance (ZDSO).

The Annexation

Comprehensive Plan

These parcels are included on **The Future Land Use Map** in the Land Use Element of the Town's Comprehensive Plan. The parcels are within the Future Growth Boundary for the town.

Delivery of Services

The parcels are located in an area served by the Beaufort Jasper Water and Sewer Authority. The Burton Volunteer Fire Department will be the first deliverer of services for this area, with Port Royal as backup. The Town holds an agreement with Burton Fire District. This agreement allocates funds annually from the town to the Burton Volunteer Fire Dept.

The current corporate boundaries are contiguous to and beyond this property therefore:

- The Port Royal Police Department has adequate staff levels to deliver services to this area.
- If developed residentially the town will provide (by contractor or town employee) curb side household garbage pick-up, curbside yard debris pick-up, curbside bulk item pick-up, and mandatory recycling pick-up.

Zoning

Proposed Zoning and Land Use Compatibility

Please see **Exhibit A, the Future Land Use Map** and **Exhibit B, the Town's Zoning Map**.

The parcels are found on the town's Future Land Use Map and are located in a Restricted Growth Sector, Conventional Neighborhood.



The proposed zoning is T4 Neighborhood Center. The Neighborhood Center (T4NC) Zone is intended to integrate appropriate, medium-density residential building types, such as duplexes, townhouses, small courtyard housing, and mansion apartments into a neighborhood framework that is conducive to walking and bicycling. Civic, transit, and commercial functions are located within walking distance.

In addition to use and development standards, the town's development codes regulate tree removal and pruning, traffic impact, and street design standards.

The Stormwater Management and Utility Agreement between Beaufort County and the Town of Port Royal, (June 12, 2012) adopted the design standards of the Beaufort County Best Management Practices (BMP) Manual for Stormwater Management.

Environmental Issues

There are no environmental issues

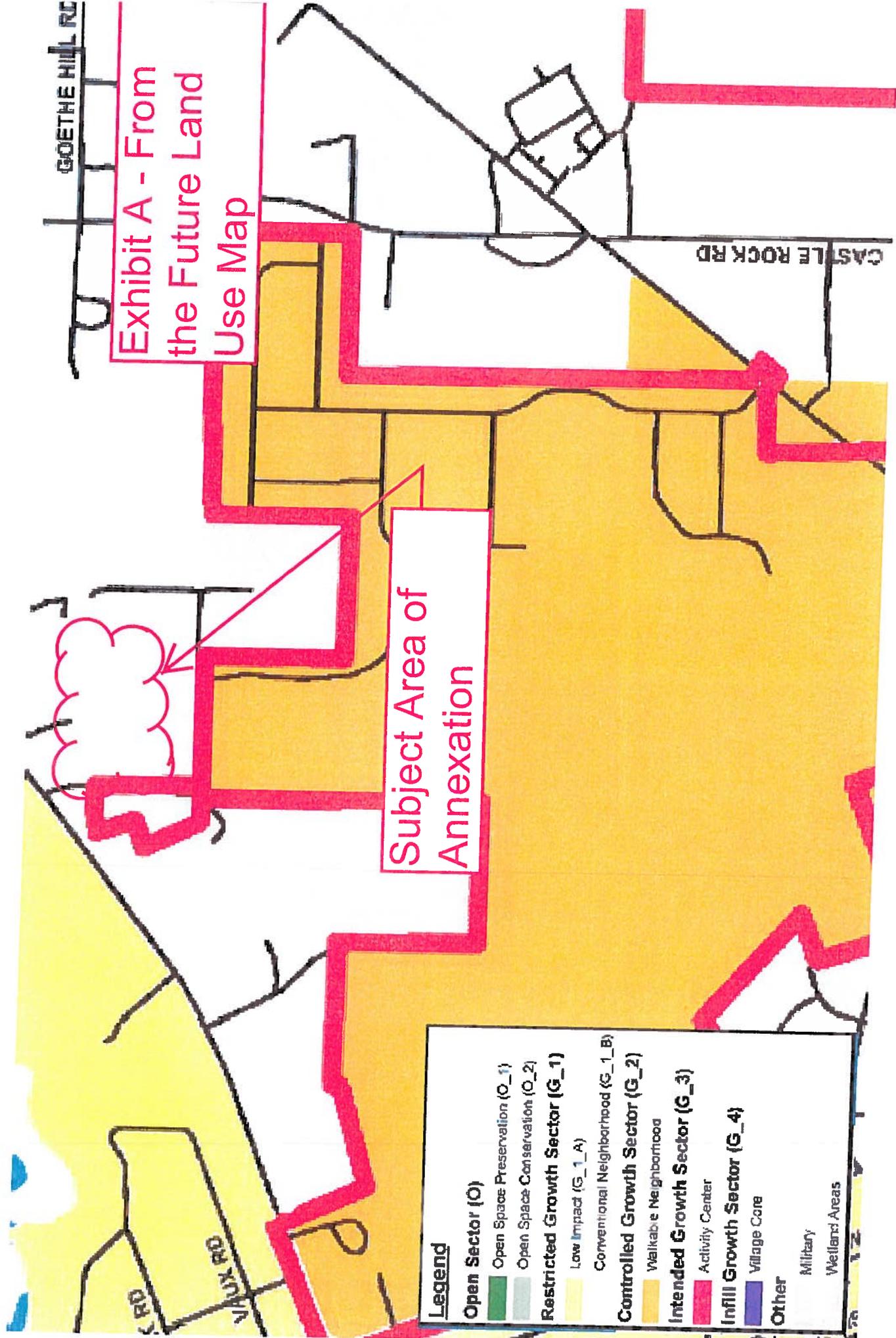
Public Notification

Letters were sent to property owners within 400 feet of the property being annexed and rezoned.

547 Broad River Blvd



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GOETHE HILL RD

Exhibit A - From
the Future Land
Use Map

CASTLE ROCK RD

Subject Area of
Annexation

Legend

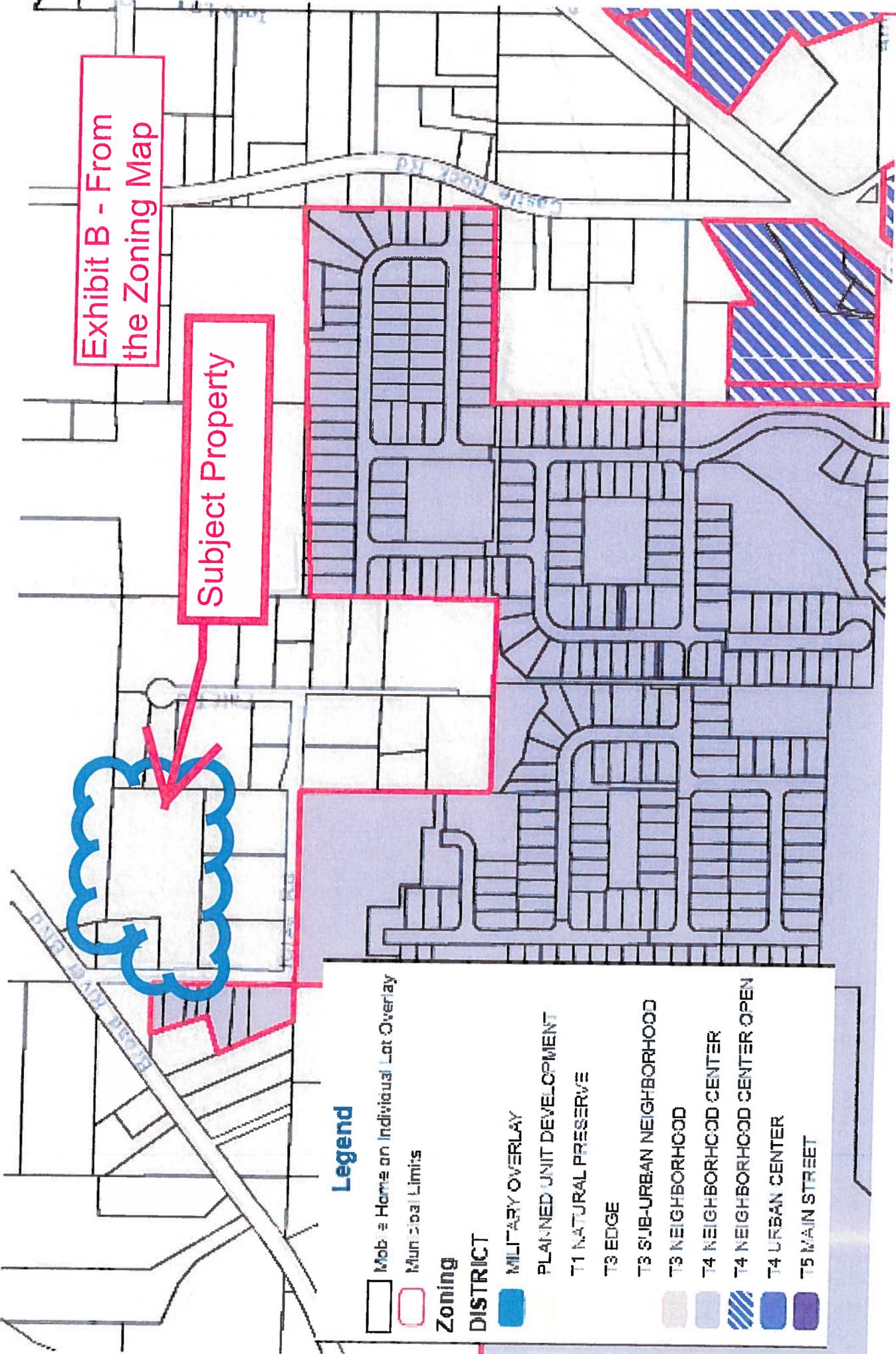
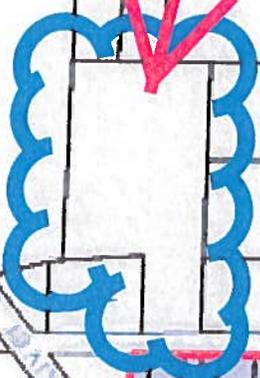
Open Sector (O)	Open Space Preservation (O_1)
	Open Space Conservation (O_2)
Restricted Growth Sector (G_1)	Low Impact (G_1_A)
	Conventional Neighborhood (G_1_B)
Controlled Growth Sector (G_2)	Walkable Neighborhood
Intended Growth Sector (G_3)	Activity Center
Infill Growth Sector (G_4)	Village Core
Other	Military
	Wetland Areas

S. RD

WALK RD

Exhibit B - From
the Zoning Map

Subject Property



Legend

- Mob = Home an Individual Lot Overlay
- Municipal Limits
- Zoning**
- DISTRICT**
- MILITARY OVERLAY
- PLANNED UNIT DEVELOPMENT
- T1 NATURAL PRESERVE
- T3 EDGE
- T3 SUB-URBAN NEIGHBORHOOD
- T3 NEIGHBORHOOD
- T4 NEIGHBORHOOD CENTER
- T4 NEIGHBORHOOD CENTER OPEN
- T4 URBAN CENTER
- T5 MAIN STREET

City of Beaufort - Town of Port Royal – Beaufort County
Joint Metropolitan Planning Commission
Rezoning Analysis 01-14
Meeting Date: September 15, 2014

Applicant

Town of Port Royal

Site

Approximately 3.68 acres. Multiply properties on 7th and 8th Streets between Paris Avenue and London Avenue also identified as District 113, Map 11, Parcel 206 and District 110, Map 11, Parcels 67B, 73, 68, 69, 69A, 70, 67A, 72, 71, 78, 261, 352, 80, 81, 82, and 427.

Present Zoning and Existing Development

The parcels were most recently zone T5 Main Street when we adopted the new Port Royal Code and Zoning Map. The rezoning covers 18 parcels. One parcel contains a utility pump station. Two parcels are municipal parking lots. One parcel contains a residential duplex. Eight parcels are single-family residences. One property is a bait and tackle shop. Five parcels are vacant. T5 Main Street Zone consists of higher density, mixed- use buildings that accommodate retail, rowhouses, offices, and apartments along primary thoroughfares within a neighborhood framework. A tight network of streets defines this Zone as a highly walkable area. Buildings are set very close to the frontages in order to define the public realm. Building Types allowed in T5 are Carriage House, Townhouse, Mansion Apartment, Apartment House, Main Street Mixed Use, Landmark Building, and Large Footprint Building.

Proposed Zoning

The proposed zoning is T4 Neighborhood Center. T4 Neighborhood Center is intended to integrate appropriate, medium-density residential building types, such as duplexes, townhouses, small courtyard housing, and mansion apartments into a neighborhood framework that is conducive to walking and bicycling. Civic, transit, and commercial functions are located within walking distance. Building Types allowed in T4 NC are Carriage House, Detached House – Medium, Detached House – Compact, Cottage Court, Duplex, Townhouse, Mansion Apartment, Apartment House, and Landmark Building.

Land Use Compatibility / Comprehensive Plan

The Future Land Use map from the Comprehensive Plan classifies the parcel as Walkable Neighborhood in a Controlled Growth Sector (G-2).

On a continuum the Controlled Growth Sector is in the middle of the spectrum as follows:



The T4 Neighborhood Center designation will continue the zoning pattern from the blocks north of these parcels. This proposal maintains the T5 Main Street zoning district on Paris Avenue and

also interfaces appropriately with the Port of Port Royal Planned Unit Development.

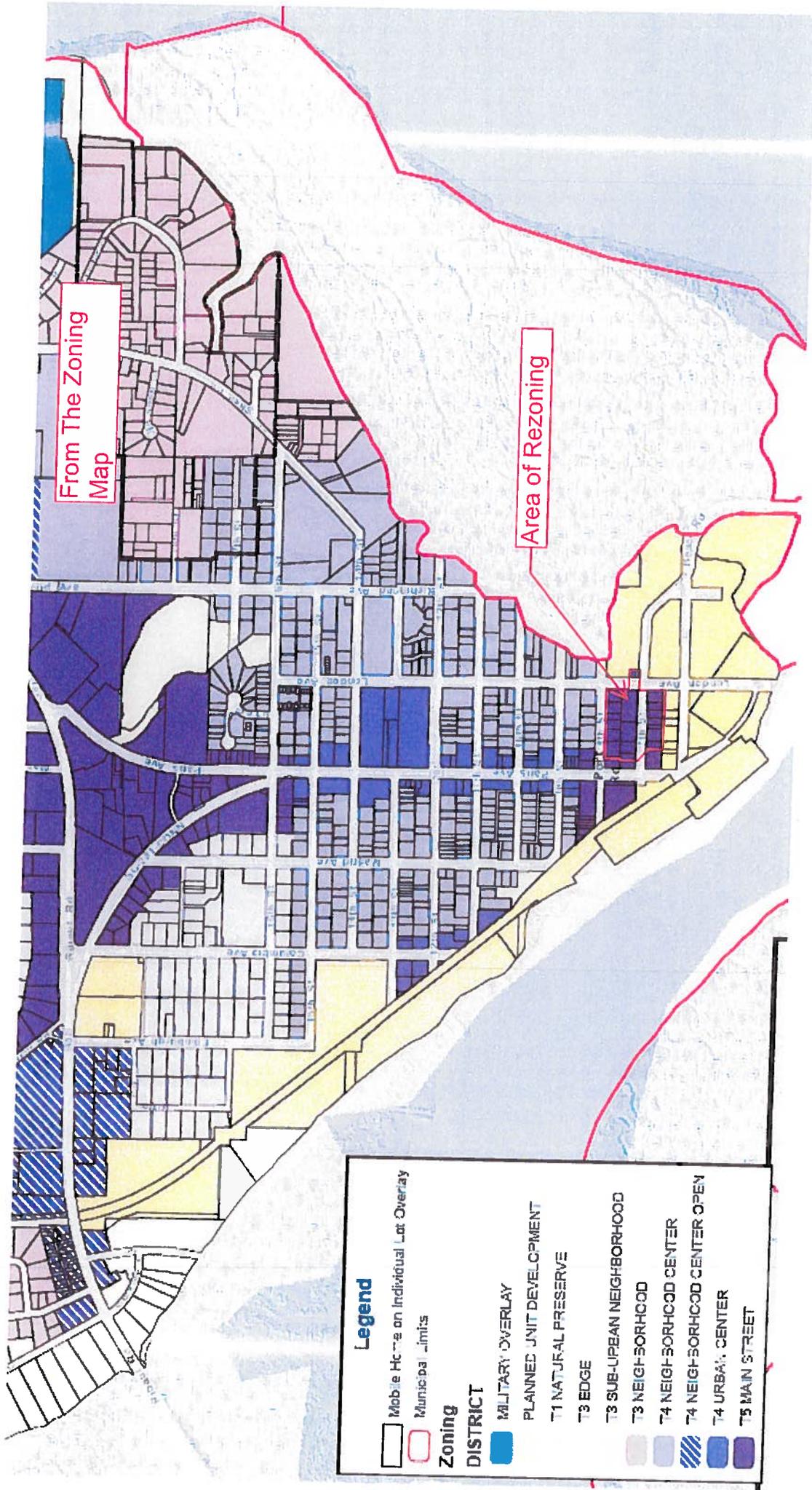
Environmental Issues

There are no environmental issues to consider.

Public Service Issues

None

Letters were sent to property owners within 400 feet of the property being rezoned



From The Zoning Map

Area of Rezoning

Legend

- Mobile Home on Individual Lot Overlay
- Municipal Limits
- Zoning District**
- MILITARY OVERLAY
- PLANNED UNIT DEVELOPMENT
- T1 NATURAL PRESERVE
- T3 EDGE
- T3 SUB-URBAN NEIGHBORHOOD
- T3 NEIGHBORHOOD
- T4 NEIGHBORHOOD CENTER
- T4 NEIGHBORHOOD CENTER OPEN
- T4 URBAN CENTER
- T5 MAIN STREET

Comprehensive Plan Update for the Town of Port Royal 2014

- INTRODUCTION (Page 23) – Add a new label UPDATE. Under this label add this text.

The 2014 Update process was conducted by Town of Port Royal staff under the supervision of the Joint Municipal Planning Commission which served as the steering committee. The update process included the update of all relevant factual information, as well as revisiting and updating the Community Principals, Goals and Strategies.

- EXISTING LAND USE (Page 55) – Refresh Current Land Use data.
(Page 58) – Refresh Building Permit data to include the sentence at the top of the page.
- FUTURE LAND USE (Page 72) – Replace the map with the new map that was adopted April 2013. The new map adjusted the future growth boundary, the negotiated annexation boundary between the town and the City of Beaufort.
- LABOR FORCE AND LABOR FORCE CHARACTERISTICS (Pages 105 to 109) – Update the data in Tables 8.1 through 8.5.
- EXISTING HOUSING (Page 126 to 130) – Update the data in Tables 10.1 through 10.6.
- POPULATION (Pages 138 to 145) – Update the data in all of the Tables to reflect the 2010 census.
- The Executive Summary (Pages 1 through 22) summarizes the Community Principals, Goals and Strategies of the document. Convene a workshop to review the strategies of the document. Note status and update as appropriate.

EXECUTIVE SUMMARY

CONTEXT

Port Royal has a long and rich history. Established in 1562, the Town has evolved from its roots as a major port into a quaint village which retains its strong maritime connection. In its commitment to maintaining the characteristics that define the community, Port Royal recognizes the importance of planning and seeking a common vision. This plan does not begin from scratch, but instead serves to build upon many of the existing plans and documents already developed for Port Royal and the region. The vision for the village core was memorialized in the 1995 Master Plan. Since this time, other planning initiatives have occurred for other areas in the town, including Shell Point, Robert Smalls Parkway, and the Casablanca neighborhood. The Northern Beaufort Regional Plan provides a context for how Port Royal fits into the greater region.

The community seems to now be at a bit of a crossroads. Public input received during this process confirms the love of the Old Village – people want to visit the mom and pop stores, stroll along the sidewalks, enjoy vibrant street life, and regain the connection to the waterfront through the redevelopment of the Port property. The community sees the potential to strengthen the Village as a regional destination and the heart of Port Royal. The public has also expressed a desire to see the pedestrian-oriented characteristics of the Village extend outside of this area to all of Port Royal...that a welcoming, walkable community would be a trademark of the whole Town. Yet there is a bit of a dichotomy as well. We may want these things, but not always the development or commitment needed to support it. Successful retail and businesses, life on the streets, a sense of community... these things rely on a common element – people; and more specifically, a consistent base of people who can conveniently and safely walk to their destination. This also means they have a destination to walk to and that the journey to get there is enjoyable. So how do we get there?

THE PLAN

The comprehensive plan serves as the go to resource for day-to-day decision making as well as long-range planning in how to achieve the community's vision and goals. This document was prepared according to the requirements set forth in the *South Carolina Local Government Comprehensive Planning Enabling Act of 1994*. These requirements outline nine basic elements that must be addressed in the comprehensive plan: population, economic development, natural resources, cultural resources,



community facilities, housing, land use, transportation, and priority investment. Each element is addressed as a chapter in this plan. While this approach allows the reader to understand the existing conditions, needs and goals, and implementation strategies for each major topic, it can be more difficult to appreciate the significant overlap that exists between these community elements.

In thinking about the vision for the community, most people will tend to think about the community as a whole, rather than individually by topics as outlined in the plan. Therefore, the establishment of community principles serves as an umbrella for the community goals and strategies, recognizing the interrelationship between the various elements of the comprehensive plan and providing general guidance and continuity. Goals and strategies for each element are presented for each element in the plan. In the following section, these goals and strategies are organized according to the primary community principle they support. The icons to the left relate to the comprehensive plan element from which the goal or strategy originally appears.

- Transportation  Cultural Resources  Economic Development
-  Land Use  Community Facilities  Priority Investment
-  Natural Resources  Housing  Population

The strategies identified to achieve the stated goals include various approaches, including plans, policies, programs, action items, regulatory approaches, or other types of strategies. Within the following summary, each strategy is identified according to one or more of these approaches, as appropriate.

From Planning to Action: The Implementation Committee

The Joint Municipal Planning Commission will serve as the Comprehensive Plan Implementation Committee, keeping the recommendations of the plan at the forefront of day-to-day and long-term decision-making for community leaders. In order to track progress and maintain the integrity of the plan, the Joint Municipal Planning Commission will prepare an Annual Report of Accomplishments to monitor implementation of the plan. This report will be forwarded to the Port Royal Town Council and made available to the public to keep the community informed and engaged in the planning process.



Community Principle: A Place for People

Streets are for people, not just cars, and should be designed appropriate to their context. Dominance of the automobile should be reduced. Our transportation network will be highly interconnected to maximize mobility and choice.

Goals

- Our transportation system will provide safe and efficient facilities for all users, including cars, cyclists, and pedestrians.
- Cars will be accommodated without driving the design of our community or dominating the landscape.

Strategies

- Establish parking maximums as well as minimums. Parking maximums limit the number of parking spaces which can be provided in order to avoid large, oversized parking lots which typically remain unused.
- Establish a complete streets design manual for Port Royal.
- Work collaboratively with Beaufort County and SCDOT on roadway improvements to ensure that roadway improvements respect and minimize negative impacts on surrounding neighborhoods. All state and county projects should complement the goals and objectives articulated in the Port Royal Comprehensive Plan. In order to ensure consistency, all transportation improvement projects should be reviewed by the Planning Commission. This review should begin early in the process, prior to the commencement of preliminary engineering.
- Continue with streetscape improvements throughout the Town, especially along major roadways such as Paris Avenue, Ribaut Road and Robert Smalls Parkway.
- Encourage the use and ownership of golf carts.
- Develop a comprehensive bike and pedestrian plan to inventory existing facilities and identify new facilities needed.

	Plan	Policy	Program	Action	Regulatory	Other
Establish parking maximums as well as minimums. Parking maximums limit the number of parking spaces which can be provided in order to avoid large, oversized parking lots which typically remain unused.	X				X	
Establish a complete streets design manual for Port Royal.		X				X
Work collaboratively with Beaufort County and SCDOT on roadway improvements to ensure that roadway improvements respect and minimize negative impacts on surrounding neighborhoods. All state and county projects should complement the goals and objectives articulated in the Port Royal Comprehensive Plan. In order to ensure consistency, all transportation improvement projects should be reviewed by the Planning Commission. This review should begin early in the process, prior to the commencement of preliminary engineering.						
Continue with streetscape improvements throughout the Town, especially along major roadways such as Paris Avenue, Ribaut Road and Robert Smalls Parkway.			X			
Encourage the use and ownership of golf carts.		X				
Develop a comprehensive bike and pedestrian plan to inventory existing facilities and identify new facilities needed.	X					



Community Principle: Commitment to Quality Development

Quality and character are inextricably linked. Our buildings and development will strive for quality, for permanence over short-lived. Quality design, materials, and construction will not be sacrificed in the name of affordability, as durability and efficiency produce affordability.

Goals

- Port Royal will continue to build upon its strong planning tradition in placing a high priority on the quality of the built environment.
- Port Royal will promote compatible infill and redevelopment.

Strategies

- Identify infill and redevelopment priority areas. Conduct an infill parcel inventory in the identified priority areas.
 - Establish GIS "screening" criteria to identify possible infill areas or parcels. Consider such factors as zoning, size of parcels, infrastructure, land use, and constraints to development such as wetlands.
 - For redevelopment, look at such factors as ratio of assessed value of improvements to land value, age of structure, etc.
 - Conduct field surveys to supplement GIS screening – condition of structures, surrounding land use patterns.
- Identify impediments to infill development and develop strategies to overcome the impediments.
- Work with established neighborhoods to develop neighborhood plans and/or guidelines for renovations, redevelopment, and new construction. Carefully weigh goals to avoid the unintended consequence of displacing poorer residents.

	Plan	Policy	Program	Action	Regulatory	Other
	X			X		
		X			X	
	X					

Strategies

- Where feasible, streamline the permitting process. In order to attract more business development, consider “pre-permit” non-retail commercially zoned properties, similar to the process established in the Beaufort Commerce Park. This involves the local jurisdiction proactively analyzing the properties, assessing site conditions, determining the location of natural resources, determining appropriate locations for buffers, etc. for the purpose of completing portions of the development permitting process for the applicants.

Plan	
Policy	
Program	
Action	
Regulatory	X
Other	

Community Principle: The Importance of Urban Form

Land development regulations should respect a flexible mix of land uses, allowing the market to operate naturally over time. Regulations should focus on fundamental design issues, such as the proper placement of buildings on their sites. The position and proportion of the building in relation to the public space is far more important than the uses inside it or the style of its architecture.

Goals

- 🏠 We will strive to maintain our unique, quaint coastal character while also accommodating new growth and development to sustain our community.
- 🏠 Affordable housing will be attractive, well-maintained, and integrated into the community.

Strategies

- 🏠 Update Port Royal’s zoning regulations to a form-based code in order to properly consider the importance of community design and character, building placement and proportions, and the impact of private development on the public realm. (A form-based code is a method of regulating development to achieve a specific urban form. Form-based codes create a predictable public realm primarily by controlling physical form, with a lesser focus on land use.)

Plan	
Policy	
Program	
Action	
Regulatory	X
Other	

Strategies

-  Continue implementation of the Shell Point master plan in coordination with Beaufort County.
-  Foster compatible uses. Varying land uses should be primarily accommodated through appropriate design. Buffers should be strategically used where needed, but not serve as an impediment to walkability.
-  Promote the development of commercial “nodes” in areas outside of the Traditional Town Overlay District to prevent sprawling development patterns and to minimize driving.
-  Site all civic projects in appropriate urban locations and ensure compatibility with the principles of the Master Plan.
-  Maximize the retention of existing native vegetation over the practice of clearing and replanting. The types and arrangement of vegetation should correspond with the character. Urban areas should feature more regular spaces of trees and vegetation, whereas rural areas should maintain a more natural and organic look.
-  Work to move existing utilities underground; require new utilities to be placed underground.
-  Provide more flexibility in commercial zoning districts to permit smaller non-retail commercial uses such as contractor’s offices, small assembly facilities, and small light industrial operations that do not adversely impact surrounding retail uses.

	Plan	Policy	Program	Action	Regulatory	Other
	X	X		X	X	
					X	
		X				
	X	X		X	X	
						X

Community Principle: A Town of the Water

Connection to the surrounding natural environment should be maximized, with public access to the waterfronts and clear vistas to the marshes. We will minimize impacts to these important resources through proper stormwater management and land development practices.

Goals

-  Port Royal will re-establish and strengthen its physical, social, and psychological connection with the water. The water's edge will be clearly defined as an important component of the public realm.
-  Port Royal will be protected by baseline standards for natural resources including salt marshes, marsh islands, coastal waters, and marine resources; trees, forests, and wildlife habitats; beaches and dunes; and open space preservation through the adoption of planning policies and regulations.
-  Port Royal will embrace and showcase its rich history and maritime roots.
-  Port Royal will seek to increase the economic development activities related to the water, waterfront activities, and our maritime history.

Strategies

-  Strengthen Port Royal's connection to the Intracoastal Waterway. Ensure public slips are provided in the port redevelopment marina to encourage ICW travel to stop in Port Royal.
-  Redevelopment of the port property is a keystone in the future success of the Old Village. As such, this redevelopment should reflect the unique character of Port Royal while serving as an anchor to draw people into the Old Village.
-  Improve the quality and attractiveness of The Sands, including rehabilitation of the beach.

	Plan	Policy	Program	Action	Regulatory	Other
		X	X		X	
						X



Strategies

-  Establish the vistas at the terminus of numbered streets in the Old Village as pocket parks.
-  Coordinate with Beaufort County to upgrade existing boat facilities as outlined in the SCDHEC/OCRM South Carolina Five Coastal County Boat Ramp Study.
-  Require development adjacent to the marsh and/or water to respect the public realm through the establishment of a public right-of-way adjacent to the resource. The intent of this strategy is to allow the public visual access and enjoyment to these public natural resources. This is especially important in major redevelopments, such as the port property and potential redevelopment of the naval hospital.
-  Develop and implement a public education campaign to help prevent and mitigate nonpoint source pollution, as much of this type of pollution comes from private homes.
-  Develop and adopt subdivision regulations and design standards minimizing the amount of impervious surfaces and modifying current right of way requirements.
-  Utilize zoning to regulate recreational and occupational uses of the rivers that protects natural resources from pollution and inappropriate use.
-  Address the CIP projects identified in the 2005 Drainage Study:
 - South Paris Avenue, 9th and 10th Street Drainage System Improvements
 - Casablanca Area Drainage System Improvements
 - Columbia Avenue, Madrid Avenue, and 14th Street Drainage System Improvements
 - 12th Street Culvert Rehabilitation
 - Indigo Woods Court Culvert Rehabilitation
-  Continue to support the Low Country Estuarium through promotion and financial support. Assist in the promotion of the capital campaign to construct the new facility.

	Plan	Policy	Program	Action	Regulatory	Other
	X			X	X	
				X		
			X			
				X		
					X	
		X				



-  Port Royal will foster and encourage the continued development of the arts community.
-  Port Royal will have a mix of diverse housing options available to provide quality housing for a diverse socio-economic spectrum.
-  Infill development with housing types appropriate for the neighborhood will help stabilize older neighborhoods and strengthen community.
-  Port Royal will participate in addressing affordable and workforce housing needs on a regional basis through a multi-jurisdictional approach.
-  Port Royal will be attractive for a diverse population of retirees, young families, and professionals in order to have a stable population mix and increase prosperity.

Strategies

-  Educate the public about the annexation process and how to evaluate when annexation is appropriate. Land located inside the Port Royal Growth Boundary is anticipated to ultimately annex into the Town with a demonstration that adequate public facilities are available or will be available at the time of development and that negative impacts of development will be mitigated. However, annexation is currently a largely misunderstood issue which needs to be better understood by the public.
-  Continue to present and support town concert series, cultural events, and festivals.
-  Support the Historic Port Royal Foundation (HPRF) with projects and capital improvements.
-  Encourage citizens to restore and preserve historic sites and properties by coordinating with local historic foundations.

Plan	Policy	Program	Action	Regulatory	Other
	X				X
	X				
	X				

Strategies

	Plan	Policy	Program	Action	Regulatory	Other
 Assess the cultural resources, such as oral history and the physical, built environment, and develop a plan to protect them.	X					
 Should the Naval Hospital site be redeveloped, the existing hospital building should remain and be redeveloped to retain its landmark significance.	X	X				
 Improve public access to the Emancipation Proclamation site.			X			
 Expand and upgrade the Shell Point county park, including increasing connectivity to surrounding areas. Add the adjacent church property to expand the park.		X	X			
 Establish a performance arts venue.			X			
 Establish a dog park with facilities and supplies for waste disposal.			X			
 Work to prevent the negative impacts of gentrification from disproportionately affecting the low to moderate income residents of Port Royal. <ul style="list-style-type: none"> • Explore property tax controls which would ensure that residents who wish to remain in their homes are able to do so. • Encourage the renovation and reuse of abandoned and dilapidated properties within the Town. • Encourage lease-options to increase affordability. 	X			X		
 Pass a mandatory inclusionary zoning ordinance which would require new residential development to address the provision of affordable dwelling units. The inclusionary zoning policy should include provisions for, on a case by case basis, a housing fee in lieu of, off-site inclusionary units, land donation, and incentives such as density bonuses that are greater than the inclusionary Zoning set aside so that the builder can reap the benefit of some bonus market-rate units.				X		
 Hold long-range meetings of the Planning Commission to advance the goals of the comprehensive plan and monitor progress.			X			

Strategies

-  While we cannot control the population of those who chose to live in Port Royal, we can enact strategies which provide the characteristics desirable to this mix, including high quality schools, attractive jobs, and a good quality of life.

Plan	X
Policy	
Program	
Action	
Regulatory	
Other	

Community Principle: The Three E's: Environment, Economy, Equity

As a community which has stood the test of time, we will continue to sustain our community and resources:

- *Environment – The built environment will be designed, built, and maintained to minimize impacts on the natural environment.*
- *Economy – Our government services will be fiscally responsible and maximize return on investment. We will support entrepreneurial ventures which strengthen our local economy.*
- *Equity – A diverse range of household incomes should be encouraged with dignified forms of both affordable and market-rate housing.*

Goals

-  We recognize that sustainable development must consider environmental stewardship, social equity, and fiscal responsibility.
-  Through appropriate stewardship of the land, the Town of Port Royal will preserve and enhance its natural beauty, environmental quality, and natural resources, ensuring harmony between the natural and manmade environment.
-  An integrated ethnic and socioeconomic diversity of the region will be promoted regionally, and in particular the ability of indigenous population groups to remain a contributing part of the region and benefit from the opportunities that come from growth will be protected.
-  Port Royal will support the growth and development of quality jobs and businesses in the community.



Strategies

Lands Acquisition Program by nominating appropriate land within the Town limits for purchase and preservation through the program.

- Encourage conservation and protection of native birds, wildlife and habitats.
 - Establish contact and network with conservation organizations at the local, state and federal level to assist in identifying, preserving, and managing the Town's natural resources.
 - Work with Beaufort County Open Land Trust and other conservation organizations to acquire, manage and preserve open space and scenic vistas.
 - Evaluate the Town's open space development standards to more accurately reflect its urban context.
 - Require future land development practices to be compatible with the existing topography, vegetation, and scenic vistas.

Foster responsible environmental stewardship in both public and private enterprise and endeavors.

- Review existing land and water uses for pollution and habitat degradation. Revise regulations as necessary.
- Encourage and develop incentives for conservation efforts and sound environmental management by both residents and businesses.
- Promote economic development compatible with adjacent land uses and the natural environment.
- Encourage energy consciousness and conservation among the Port Royal residents, businesses, property owners and developers.
- Encourage all new construction to incorporate "green" building techniques and to pursue certification such as the U.S. Green Building Council's Leadership in Energy and Environmental Design (LEED) certification.
- Strengthen and expand the Town's recycling program.
- Explore the possibility of instituting a composting program throughout the Town.

Consider an open space land bank where fees are collected in lieu of open space to apply to the purchase and preservation of larger or more critical lands.

	Plan	Policy	Program	Action	Regulatory	Other
Lands Acquisition Program			X			
Encourage conservation and protection of native birds, wildlife and habitats						
Foster responsible environmental stewardship in both public and private enterprise and endeavors	X				X	
Consider an open space land bank where fees are collected in lieu of open space to apply to the purchase and preservation of larger or more critical lands		X				

Strategies

-  Evaluate incentives to encourage infill development and/or redevelopment on lots or properties already served by water and sewer.
-  Encourage the adaptive reuse of abandoned buildings as an alternative to new construction.
-  Inventory the existing supply of appropriately zoned land and vacant non-residential structures available for non-retail commercial development within the Town of Port Royal and assess the present opportunities they provide for competitive economic development. Make this inventory and assessment available to local and regional agencies involved in promoting economic development.
-  Encourage industries that support sustainable practices by promoting renewable energy and attracting or growing value-added industries that support using locally available resources such as agricultural or seafood products.
-  Consider utilizing high performance green building standards through the Leadership in Environmental and Energy Design (LEED) through the US Green Building Council in all future civic projects to conserve energy and set a precedent of conservation in the lowcountry.

Plan	Policy	Program	Action	Regulatory	Other
X					
X			X		
	X				
X					

Regional Cooperation

Although regional cooperation is a high priority for the Town of Port Royal, it is not articulated as a community principle due to the multi-jurisdictional agreement needed for true regional cooperation to occur.

Goals

-  The Town of Port Royal will coordinate growth with the City of Beaufort and Beaufort County, especially around the current and future edges of the communities.
-  Port Royal will support the vision for Northern Beaufort County to maintain a distinct regional form of compact urban and suburban development surrounded by rural development for the purpose of reinforcing the valuable sense of unique and high quality places within the region.
-  Port Royal will participate with other jurisdictions in Northern Beaufort County in developing methods of creating and permanently preserving a regional open space system.
-  Adequate and timely regional infrastructure and public facilities will be provided in a fair and equitable manner through a cooperative process in which all units of local government participate and act in the spirit of partnership.
-  A strong, vibrant, and healthy economy will be achieved through a successful economic development program in order to ensure the long term success and viability of the Northern Beaufort County region.
-  Port Royal will work collaboratively with Beaufort County and other neighboring jurisdictions.
-  Port Royal will coordinate with other jurisdictions in Northern Beaufort County to maintain a fiscally sustainable system of funding regional capital infrastructure, operating, and maintenance needs.
-  Port Royal will continue to participate in the Northern Beaufort County regional planning effort to implement the regional plan.



Strategies

- Support regional transportation planning efforts, including mass transit and water-borne ferry service between the islands.
- Work with other jurisdictions in Northern Beaufort County to explore and evaluate a range of transportation improvements, including road capacity improvements, transit, pedestrian and bicycle connections, enhanced access management, and operational improvements. It is important to recognize that these approaches should be explored and evaluated, and that flexibility is needed to determine the best specific solutions based upon analysis of changing conditions.
 - Transit and Travel Demand Management – A transit route could reduce the trips made across the key Woods Memorial Bridge and SC 802 river crossings. A circulator between these areas would need to operate with frequent service/short headways to be effective in attracting riders to switch modes from automobile use. A program to provide an organized approach to teleworking, flexible work hours, carpool matching, and vanpool services is recommended for the Downtown Beaufort and Port Royal areas. A second program to focus on U.S. Marine Air Station carpooling is also recommended. The transit and travel demand management strategies will require more detailed study to determine the anticipated level of benefits and feasibility.
 - Pedestrian and Bicycle Connections – Providing local pedestrian and bicycle connections where commercial areas are present near residential communities could reduce trip making along adjacent arterials.
 - Access Management – Access management along major corridors is recommended to maximize the capacity available to move through traffic. Implementation of the Robert Smalls Parkway plan and associated ordinance is key for this corridor.
- Work collaboratively with Beaufort County and the jurisdictions within Beaufort County to seek integrated solutions to complex transportation issues. More specifically, work to implement the regional transportation plan integrating road improvements, bicycle facilities, pedestrian facilities, and transit improvements as developed for the US Department of Transportation (DOT) Transportation Investment Generating Economic Recovery (TIGER) grant program.

Plan	Policy	Program	Action	Regulatory	Other
	X				
	X				
				X	
	X				

Strategies

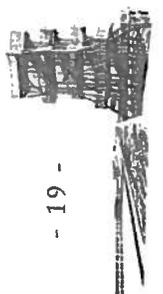
- The Northern Beaufort Regional Plan provides recommendations for the following transportation improvements, at least a portion of which are located within Port Royal.
 - Port Royal to Yemassee Trail - Implementation of a bike corridor along the abandoned railroad corridor west of US 21 is recommended to provide access to an alternative transportation mode for those along the US 21 corridor. This corridor would provide a trail that is separated from automobile traffic, enhancing safety for all users over on-street bike lanes or "share the road" designations.
 - US 21 to SC 170 - Western Bypass (Planning, feasibility analysis, and right-of-way for a 2 lane road with turn lanes and bicycle lanes) - This connection will provide a link from the US 21 corridor to the SC 170 and SC 802 corridors. This connection has the potential to relieve US 21 for traffic traveling to/from SC 170, as well as serving some traffic along US 21 north of Beaufort that is destined for Port Royal, Lady's Island, or St. Helena Island. This project will provide the planning and analysis needed for consideration of this alternative for application beyond year 2025.

 Work together with other jurisdictions in Northern Beaufort County to create and maintain an improved regional growth tracking system, including a land demand and land use forecasting model integrated with other regional models (such as the transportation model) that can be used by all entities for planning purposes.

 Work with other local governments in Northern Beaufort County to establish a common definition and baseline standards for open space. This would address such concerns as whether wetlands can count towards required open space, whether stormwater detention ponds could be placed within open space, and to what degree does open space serve recreation needs vs. preservation needs.

 Participate in the Beaufort County Affordable Housing Consortium and/or other partners to work collaboratively with the Beaufort Housing Authority to address affordable housing needs in the region. Investigate innovative approaches in mixed-use, mixed-finance developments to meet affordable housing needs without segregating those in need of assistance.

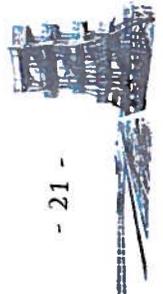
Plan	Policy	Program	Action	Regulatory	Other
		X	X		X
	X				
	X				
	X				



Strategies

	Plan	Policy	Program	Action	Regulatory	Other
Work Cooperatively with the School District: While the School District has the responsibility to plan and provide funding for its capital needs, a framework needs to be established where the Participating Local Governments can work cooperatively with the School District and support its efforts to plan for the future deficiencies and future capital improvement needs for public schools.	X					
Develop an Overall Funding Strategy: The following factors should guide the selection of revenue sources to address the capital and operating funding gap: <ul style="list-style-type: none"> Revenue Potential: Whether the tool can generate substantial sums of monies to fund capital infrastructure; Geographic Application: Whether the tool can be applied across the region; Legislative Authorization: Whether the tool requires legislative authorization; Technical/Administrative Ease: The ease of administering the tool; and Public Acceptability: How citizens will accept the tool. 	X	X				
Focus First on Available Funding Tools: In order to take immediate action on addressing capital funding needs, it is important to concentrate first on revenue sources that the State of South Carolina enables local governments to use to fund capital improvements. These include property taxes, local sales, impact fees, and taxes.		X				
Consider Funding Tools that Require Changes in State Legislation: If the available funding tools are not adequate to address the funding gap, particularly the operating cost gap, it may be necessary to lobby the state to initiate legislation that would enable new funding sources.		X				
Explore New Institutional Arrangements: Where appropriate, new institutional arrangements to facilitate multi-jurisdictional cooperation on funding issues should be explored.					X	
Continue to maintain a good relationship and foster coordination and planning with Parris Island.	X					

-  **Work Cooperatively with the School District:** While the School District has the responsibility to plan and provide funding for its capital needs, a framework needs to be established where the Participating Local Governments can work cooperatively with the School District and support its efforts to plan for the future deficiencies and future capital improvement needs for public schools.
-  **Develop an Overall Funding Strategy:** The following factors should guide the selection of revenue sources to address the capital and operating funding gap:
 - **Revenue Potential:** Whether the tool can generate substantial sums of monies to fund capital infrastructure;
 - **Geographic Application:** Whether the tool can be applied across the region;
 - **Legislative Authorization:** Whether the tool requires legislative authorization;
 - **Technical/Administrative Ease:** The ease of administering the tool; and
 - **Public Acceptability:** How citizens will accept the tool.
-  **Focus First on Available Funding Tools:** In order to take immediate action on addressing capital funding needs, it is important to concentrate first on revenue sources that the State of South Carolina enables local governments to use to fund capital improvements. These include property taxes, local sales, impact fees, and taxes.
-  **Consider Funding Tools that Require Changes in State Legislation:** If the available funding tools are not adequate to address the funding gap, particularly the operating cost gap, it may be necessary to lobby the state to initiate legislation that would enable new funding sources.
-  **Explore New Institutional Arrangements:** Where appropriate, new institutional arrangements to facilitate multi-jurisdictional cooperation on funding issues should be explored.
-  **Continue to maintain a good relationship and foster coordination and planning with Parris Island.**



EXECUTIVE SUMMARY

CITY OF BEAUFORT
REZONING ANALYSIS RZ14-05
PUBLIC HEARING DATE: OCTOBER 14, 2014

Applicant

The applicant is Burton Properties.

Site

Seven parcels of property, located in the Robert Smalls Parkway/Burton Hill Road area, are proposed for rezoning (see attached Site Location Map). The property addresses and property identification number of each lot are outlined below.

<u>Address</u>	<u>District, Map, Parcel</u>
188 Robert Smalls Pkwy	R122, 29, 103F
No address	R122, 29, 233
203 Robert Smalls Pkwy	R122, 29, 242
No address	R122, 29, 243
No address	R122, 29, 244
No address	R122, 29, 490
23 Horton Dr	R100, 29, 481

All the lots are undeveloped.

Present Zoning

All of the lots currently in the city limits are zoned “GC General District” (GC). The GC District is a fairly intense urban commercial zone. All types of office and retail uses are permitted. Restaurants are permitted, but cannot have drive-thrus facilities. Limited Vehicle Service (ex., a “quick lube”) is permitted, but full service vehicle repair is not allowed. Vehicle sales are not permitted in the GC District.

Parcel 481 (23 Horton Drive) is zoned Commercial Regional District (CR) under the County’s Zoning and Development Standards ordinance. The CR district is the most intense commercial zoning district set out in the County code. The district permits a full range of retail, service, and office uses. Restaurants with drive-thrus, and vehicle sales and service are permitted in the CR zone.

The attached map shows the current zoning pattern in the area. As noted, the Robert Smalls Parkway area is currently a blend of GC and HC zoning.

Proposed Zoning

The proposed zoning for all the parcels is “HC Highway Commercial District” (HC). As described in the UDO:

The HC Highway Commercial zoning district is intended to be developed and reserved for general business purposes and with particular consideration for the automobile-oriented

commercial development existing or proposed along the City's roadways. The regulations which apply within this district are designed to encourage the formation and continuance of a compatible and economically healthy environment for business, financial, service and professional uses which benefit from being located in close proximity to each other; and to discourage any encroachment by industrial, residential or other uses considered capable of adversely affecting the basic commercial character of the district.

The HC District permits all types of office and commercial uses. A variety of auto-oriented uses are permitted including drive-thru and drive-in restaurants, vehicle sales and service, gas stations, and car washes. Warehousing, wholesale sales, and light industrial services are also permitted.

Consistency with Comprehensive Plan

The Framework Plan in the City's Comprehensive Land Use Plan designates these lots as Growth Sector 3 "Neighborhood Mixed Use" and "Corridor Mixed Use" (red) (see attached map). According to the comprehensive plan, "The G-3 sector is intended to apply along high capacity regional thoroughfares at major transportation nodes, or along portions of highly-traveled corridors. G-3 land generally falls within areas for higher-intensity regional-serving development, marked by the dark purple ½ mile radius circles . . . Corridor Mixed Use designations (G3-B) are intended for a mixture of regional-serving commercial, residential, and institutional destinations." Appropriate land uses in the G-3 sector include: residential development, neighborhood-serving commercial uses (retail and office), civic uses, and neighborhood centers, regional centers, and industrial districts.

An excerpt from the Comprehensive Plan describing the G-3 district is attached. The proposed rezoning appears to be consistent with the Framework Map in the Comprehensive Plan.

Draft Form-Based Code Map

The draft form-based code map shows the area to be rezoned as T5-Urban Corridor District (UC). The proposed T5-UC transect is comparable to existing Highway Commercial zoning. The draft code shows drive-thru facilities permitted in the T5- UC district as conditional uses.

Land Use Compatibility

The Robert Smalls Parkway corridor contains a mix of intense commercial land uses including auto sales, restaurants with drive-thru facilities, and various other office and retail uses.

Suitability of Property for Uses Permitted in Current Zoning District

The lots can accommodate the commercial uses permitted by the current GC zoning.

Suitability of Property for Uses Permitted in Proposed Zoning District

The property could also accommodate the more intense vehicle-oriented uses permitted in the HC District.

Compatibility of Uses Permitted in Proposed Zoning District with Natural Features.

There should be no more impact on natural features under the proposed zoning than under the current zoning.

Marketability of Property for Uses Permitted by Current Zoning District

The property would likely be more marketable under HC zoning, as a wider variety of uses, including auto-oriented uses, are permitted.

Availability of Infrastructure

The lots are located on Robert Smalls Parkway, a 5-lane arterial road with sidewalks on both sides. The area is served with water and sewer. The Robert Smalls Parkway/Burton Hill Road intersection is signalized.

Public Notification

Letters were sent to adjoining property owners on September 2. To date, staff has received no public comments on the proposed rezoning.

Excerpt from Framework Map from Comprehensive Plan



Excerpt from Current Zoning Map



Framework Map Legend

O1.Preserved Open Space

 Preserved Open Space

O2.Rural Lands/Conservation

 Rural Lands/Conservation

GR1. Growth Reserve

Growth Reserve

G1.Residential Neighborhoods/TND's

 Restricted Growth Sector

G2.Urban Neighborhoods/TND's

 Controlled Growth Sector

G3A.Neighborhood Mixed Use

 Intended Growth Sector

G3B.Corridor Mixed-Use

 Intended Growth Sector

G4.Downtown

 Infill Growth Sector

SD.Industrial/Employment Center



Civic/Hospital



USCB 1/4-Mile Buffer



Neighborhood Centers



Regional Centers



AICUZ



Military





Mixed-use town center development



Mixed-use building in a regional center with residential above retail



Regional centers contain a mixture of higher density commercial and residential uses



Industrial, warehouse, or distribution-type building

FG 1.7 GROWTH SECTOR 3 (G-3): NEIGHBORHOOD MIXED USE (G-3A) & CORRIDOR MIXED USE (G-3B)

The G-3 sector indicated in lighter and darker red (respectively), is intended to apply along high capacity regional thoroughfares at major transportation nodes, or along portions of highly-traveled corridors. G-3 land generally falls within areas for higher-intensity regional-serving development, marked by the dark purple 1/2 mile radius circles. Neighborhood Mixed-Use designations (G-3A) are intended for a mixture of uses intended to serve the surrounding neighborhoods. Corridor Mixed-Use areas (G-3B) are intended for a mixture of regional-serving commercial, residential, and institutional destinations.

Care should be taken to limit the length of G-3 corridor developments to avoid the creation of lengthy, undifferentiated linear strip development. Attention to local geography and environmental conditions can assist in this definition, with special attention given to areas in O-1 and O-2 sectors along water courses and near sensitive lands.

APPROPRIATE LAND USES/DEVELOPMENT TYPES:

The full-range of community types and uses are appropriate in the G-3 sector, including:

- single-family and multifamily residential
- neighborhood-serving commercial uses (retail and office)
- civic uses
- traditional neighborhood developments
- neighborhood centers
- regional centers
- industrial districts

FG 1.8 REGIONAL CENTERS

Regional Centers are mixed-use activity centers with employment and commercial uses that attract people from beyond the immediate neighborhoods and from surrounding communities. These centers are appropriate for commercial and employment development as well as the area's highest density housing. The area of these centers is based on a 1/2 mile radius (a typical 10-minute walk)—the larger circles on the map. Regional centers are envisioned for downtown Beaufort; the emerging city-county government district at Ribaut Road and Boundary Street; around the hospital and technical college campuses; at the intersection of SC 170 and SC 280; and around the intersection of US 21/Boundary Street and Robert Smalls Parkway. These centers will provide the highest concentrations of residential, employment, and commercial services in the Plan area. Regional retail and commercial centers should be located exclusively in the Regional Centers located along SC 280 and SC 170.

FG 1.9 SPECIAL DISTRICT (SD): INDUSTRIAL/EMPLOYMENT CENTERS

As regional employment centers, industrial districts also fall into the G-3 sector. Industrial development is shown around the existing Beaufort Commerce Park and in areas where industrial and distribution facilities are currently located or approved for development by current zoning.

City of Beaufort Department of Planning and Development Services

M E M O R A N D U M

TO: Beaufort--Port Royal Metropolitan Planning Commission
FROM: Lauren Kelly, Project Development Planner
DATE: September 9, 2014
SUBJECT: Boundary Street Redevelopment District Ordinance Revisions

City Council requested that Planning Staff review the Boundary Street Redevelopment District Ordinance and recommend changes to provide more flexibility and clarity within the code language and requirements. Staff is recommending revisions to accomplish these goals. In addition, staff has proposed a number of changes designed to facilitate and stimulate investment by reducing building costs while maintaining the goal and design intent of the ordinance. The Boundary Street Project is getting ready to begin construction within the next six months, so this is a good time to address any areas that could be improved to spur investment in this corridor. The building code and street sections are designed to work together, and now Phase I of the Boundary Street section is coming to fruition. The desire of the city is to see the private investment keep pace with the publicly-funded infrastructure and believe that these code adjustments can help accomplish that.

Section 6.8 of the UDO (attached), addresses building and development standards in the Boundary Street Redevelopment District. Staff is proposing a number of changes to this ordinance. Half of those are simple text clarifications. Eleven minor changes are proposed. About the same number of major changes are recommended and address the following areas:

- **Appeal Process** – Staff is proposing changing the appeal body from the Planning Commission to the Design Review Board (DRB). The purpose of the DRB is to review plans for new construction, and so seems the appropriate board to consider appeals regarding design issues.
- **SC DOT Ownership** of the majority of the roads – Staff is proposing changes that give staff more flexibility to adjust the build-to lines and build-to zones to accommodate building frontages that are not currently be permitted in the SC DOT right-of-way. These include colonnades, arcades and balconies.
- **Two-Story Building Heights** – Staff is proposing to eliminate the two-story building height requirement for non-residential buildings and apply a taller minimum first floor height for one-story buildings. This would apply in all areas EXCEPT in a new two-story overlay zone, proposed to be located at the primary intersections of Ribaut Road &

Boundary Street and Robert Smalls Parkway & Boundary Street. Mezzanines would be permitted to fulfill the two-story requirement in this area, and specific mezzanine standards are added to the code.

- **Frontage Requirements** –
 - A minimum frontage requirement was added, along with the associated definitions. This regulates the percentage of building that needs to be built along the street frontage at the setback or build-to line. Generally it's 60% in the more urban areas, and 40% in the less urban areas. Staff felt that this was very important since the code is focusing on great urban places, rather than building height, and the amount of street frontage that is comprised of buildings is a key part of this.
 - Frontage elements (eg. arcades, colonnades, storefronts, porches, etc...) were consolidated and clarified to eliminate confusion between general glazing requirements, and glazing requirements for specific frontage types.
- **Glazing Requirements** - Glazing requirements were clarified to distinguish between general residential and commercial buildings.
- **Maximum Parking Requirement** – this was added to the code, so it now has minimum and maximum parking standards.
- **Drive-Thru Special Exception** – these standards were updated and clarified.

In the attached ordinance revisions, ~~strikeout text~~ indicates wording to be deleted, and **highlighted text** indicates wording to be added. The amendments are also annotated to denote which type of modification is being proposed: **CLARIFICATION** – minor text change/addition; **CHANGE** – minor change; or **CHANGE** – major change.

Please contact me with any questions.

Thank you.

attachments

6.1 Boundary Street Redevelopment District

A. Purpose

The City of Beaufort seeks to create a Boundary Street Redevelopment District based upon traditional standards for city building. In September 2005 the City created a Master Plan for the Boundary Street Redevelopment District through a design charrette process involving the community and a team of design professionals. These regulations are form-based and reflect the existing character of Beaufort and the surrounding region. The code enables a mixed-use physical environment to further enhance the economic and cultural success of Boundary Street and contiguous areas.

Traditional urban design conventions have been applied to create a palette of street types that form the framework for the Boundary Street Redevelopment District by setting design parameters for how buildings and other elements relate to those streets. These design conventions are derived from the existing conditions in the City and from a number of sources in planning literature as listed in the Appendix. The above texts will be available at the Department of Planning and Development Services for applicants to review. Applications for development in the Boundary Street Redevelopment District do not have to comply with the design specifics of the recommended texts; the texts are for reference and guidance only and are not to be foreseen as regulatory. Where approvals, interpretations, and judgments are subject to reasonable application by City officials, these officials should use the following texts for guidance as to best practices.

B. Application of Standards

In the case of conflict between the standards set forth in the Boundary Street Redevelopment District and any other local land development regulation, these standards shall apply. For existing City approved Planned Unit Developments (PUD) in the district, the PUD shall apply for a period of up to twenty five years from the date of adoption of the Boundary Street Redevelopment District. Once twenty five years passes, said PUDs are no longer applicable and must then fall under the regulations of the Boundary Street Redevelopment District. To the extent that Section 6.8 is silent where other land development regulations govern, they shall apply. Building construction shall conform to the applicable City and State of South Carolina building codes and regulations.

How To Use This Code:

- 1.) Refer to the **Regulating Plan**, to identify your street type.
- 2.) **Section G** will provide provisions governing building placement and character requirements based on **Street Type**.
- 3.) Verify your proposed use in the **Permitted Use Table**.
- 4.) Examine the **General Provisions** which apply throughout the district.
- 5.) Finally, refer to the **Building Elements and Architectural Standards** for specific building regulations.

C. Administration

Interpretation of the standards in this code shall be the responsibility of the City Architect. The role of the City Architect has been established to administer an architectural review procedure for the development of properties within the Boundary Street Redevelopment District. It is the responsibility of the City Architect to review building plans for compliance with the Boundary Street Redevelopment District Code and to provide design guidance when necessary. The City Architect shall be responsible for interpreting the Boundary Street Redevelopment District Code regarding architectural and streetscape standards.

The *Illustrative Master Plan* in Section 6.8.F shall serve as guidance to the City Architect with respect to the City's intent for land development in the Boundary Street Redevelopment District. The images contained in Section 6.8 are meant to demonstrate the character intended for the Redevelopment District, but are for illustrative purposes only. The accompanying text and numbers are rules that govern permitted development.

The code should be used in the following method:

- a) Refer to the **Regulating Plan** to determine street type
- b) Consult **Section G** for standards based on **Street Type**
- c) Verify your use in the Permitted Use Table
- d) Review the **General Provisions**
- e) Obtain specific building regulations in the **Building Elements and Architectural Standards** section

Application Review and Approval Procedures

1. Application materials set forth in Section 3.1 of the Beaufort Unified Development Ordinance for any development proposal in the Boundary Street Redevelopment shall be submitted to the City Architect, designated by the City of Beaufort, for review.

Filing Procedure:

2. A site plan shall be submitted by the applicant for review and approval by the City Architect. Specifically, such plan shall include the following elements, where applicable:

Site Plan

The site plan (drawn to scale by a registered civil engineer, registered landscape architect, or registered architect) shall include the following elements: the exact dimensions of the parcel of land under consideration, a schematic representation of types and locations of land uses, design and dimensions of all site and streetscape elements, tree & topography survey including all trees 8" caliper or greater, the density and intensity of the proposed uses, proposed open spaces or parks, any area-wide drainage systems, overall circulation arrangements and all major roadways, floodplain information, and any other information required by the City Architect.

Building Design:

For all buildings or structures proposed on a site, architectural drawings are required. The architectural drawings (drawn to scale by a registered architect or building designer) shall be dimensioned and include the following elements: floor plans, elevations, sections, details including but not limited to window, door, eave, fascia, columns, and frontage elements as applicable, materials and colors. The location of mechanical equipment should be indicated, along with site triangles to the equipment from all frontage streets (if they are located on the roof).

Written Report

A written report shall be submitted by the applicant for review and approval by the City Architect. Such report shall explain in general the type, nature, intent, and characteristics of the proposed project, and shall specifically include, where applicable:

- a. A general description of the proposal;
- b. A detailed legal description of the location of the site;
- c. A proposed development program, including number of residential units and proposed densities, minimum lot sizes (if any), square feet of other nonresidential uses and generalized intensities;
- d. General plan for the provision of utilities, including water, sewer, and drainage facilities;
- e. Tables showing the total number of acres in the proposed development and the percentage designated for each proposed type of land use, including public facilities;
- f. Tables showing the trees proposed to be retained and removed.
- g. A statement of how the proposed development is consistent with the Boundary Street Master Plan and City Comprehensive Plan;
- h. Exceptions or variations from the requirements of the Boundary Street Redevelopment District if any are being requested; and
- i. Other relevant information as may be requested by the City Architect.

Approval Procedure:

3. Upon concluding that the application materials are complete and in compliance with the Boundary Street Redevelopment District Regulations, the City Architect shall issue an Approval Letter to the applicant, complete a Boundary Street Redevelopment District Application Materials Review/Zoning Regulation Compliance Checklist, in such form as the City requires, and complete the Certification at the end of said form.

4. If the Applicant's application is in accordance with the Boundary Street Redevelopment District, the City Architect has the authority to approve the project.

5. If the project is not approved and if the Applicant disagrees with the determination of the City Architect, the Applicant may bring an appeal to the **Design Review Board** ~~Planning Commission~~ within 30 days of the decision.

Special Exception Process:

For Special Exceptions, applicants shall follow the process outlined in Section 3.16.

D. Definitions

Appurtenances. Architectural features not used for human occupancy, consisting of awnings, marquees, balconies, turrets, cupolas, colonnades, arcades, spires, belfries, dormers, and chimneys.

Arcade. A building frontage that contains a row of arches supported by classical columns or piers which is covered.

Arch, Segmental: A segmental arch is an arch in which the curve is a less than semicircular segment of a circle.

Arch Semi-Circular: A Semi-Circular Arch is the most common type of arch where the center of the arch is in the middle of the diameter.

er.

Balcony. An open habitable portion of an upper floor extending beyond a building's exterior wall that is not supported from below by vertical columns or piers but is instead supported by either a cantilever or brackets.

Baluster. A short vertical member used to support a railing or coping.

Balustrade. A railing together with its supporting balusters or posts, often used at the front of a parapet.

Boundary Street Master Plan. The Boundary Street Master Plan was created during a community design charrette which took place September 23rd – 29th, 2005 and involved input from major stakeholders, consultants, City Staff and the general public. The plan synthesizes community ideas and depicts the idealized build-out for the Boundary Street Redevelopment District. The Boundary Street Master Plan identifies key opportunity parcels for potential development, redevelopment, parking locations, and preservation.

Build-to Line. A line parallel to the property line, along which the front wall of a building shall be built.

Build-to Zone. A build-to zone is a range of allowable distances from a street right-of-way that the building shall be built to in order to create a moderately uniform line of buildings along the street.

Building frontage. The vertical side of a building which faces the primary space or street and is built to the build-to line or build-to zone.

Building Height. A limit to the vertical extent of a building measured in stories from the mean elevation of the finished grade or sidewalk at frontage line, whichever is higher, to the eave of the roof, or cornice for a building with a parapet. The maximum number of stories is inclusive of habitable roofs and exclusive of true basements.

City Architect. The City architect is an locally based Urban Designer, familiar with traditional planning and design principles and with the Boundary Street Master Plan, who will work with prospective developers, builders, and tenants to show how the Boundary Street Redevelopment District can satisfy their site needs in a cost efficient manner. The City architect will work under the direction of the City Planning Department and will assist developers, tenants, citizens, and the City, in achieving the goals of this Code.

Civic Building. Structure used primarily for general public purposes. Uses may include: municipal administration and infrastructure, education, cultural performances, gatherings and displays administered by non-profit cultural, educational, governmental, community service and religious organizations.

Colonnade. A building frontage that contains a lightweight, roofed structure, extending over the a public or private sidewalk and open to the street except for supporting columns or piers.

Cornice. A projecting horizontal decorative molding along the top of a wall or building.

Cupola. A domelike structure surmounting a roof or dome, often used as a lookout or to admit light and air.

Curb Radius. The curved edge of street paving at an intersection, measured at the inside travel edge of the travel lane.

Dwelling, live/work. A mixed-use unit with a substantial commercial component on the ground floor that may accommodate employees and walk-in trade. The upper floors are dedicated for living quarters.

Expression Line. A horizontal line, the full length of a façade, expressed by a material change or by a continuous projection, such as a molding or cornice, not less than two (2) inches or greater than one (1) foot deep. Expression lines delineate the transition between the floor levels.

Finger-jointed wood windows: Finger-jointed wood windows involve a technique used to join two pieces of wood at right angles to each other. It is much like a dovetail

joint except that the pins are square and not angled and usually equally spaced. The joint relies on glue for holding together as it does not have the mechanical strength of a dovetail.

Frontage Line. The property line or lines of a lot which coincide with a right-of-way or other public open space.

Frontage Build-out. The percentage of a site that must contain a building, or buildings, placed at the setback line or build-to line.

Garden Wall. A freestanding wall along the property line dividing private areas from streets, alleys, and or adjacent lots. Garden walls sometime occur within private yards.

Liner Building. A fully functional building built in front of a parking garage, cinema, supermarket etc., to conceal large expanses of blank wall area and to face the street space with a façade that has doors and windows opening onto the sidewalk.

Lot Coverage. The footprint of all structures on a particular lot. Porches, patios, terraces, stairways, walkways, driveways, parking lots and drive aisles do not count as lot coverage.

Marquee: A fixed hood or canopy supported solely by the building to which it is attached extending over part of the public right-of-way.

Mezzanine. A useable primary interior floor space, located above a main level and along a frontage street, not to exceed 1/3 the floor area of the main level.

Mullions. Strips of wood or metal that separate and hold in place the panes of a window.

Parapet. A low guarding wall at the edge of a roof, terrace, or balcony.

Paseo. A pedestrian alley connecting one right-of-way or paseo to another. Paseos shall be designed for pedestrian comfort, either shaded by trees or by the buildings that line the space.

Plaza. An unroofed public open space with a majority of paved surface. Plazas are fronted with buildings.

Principal Façade. (For purposes of placing buildings along build-to lines or build-to zones) The front plane of a building not including stoops, porches, or other attached architectural features.

Regulating Plan. The Regulating Plan denotes the specific location of street type standards for the Boundary Street Redevelopment District. (See Section G)

Shared Parking. A system of parking, typically applied to buildings of differing uses that each have peak parking demands at different times within a 24 hour period, thereby allowing some parking spaces to be shared.

Square. An open space surrounded by streets or other vehicular passages.

Stoop. A building frontage that contains a small platform and /or entrance stairway at a house door, commonly covered by a secondary roof or awning.

Storefront. ~~The portion of a building at the first story of a retail frontage that is made available for retail use.~~ A building frontage where the building facade is aligned close to the Frontage Line, with the building entrance at Sidewalk grade. This frontage is conventionally used for Retail use. It has a substantial glazing on the Sidewalk level and may be used in conjunction with Awnings, Colonnades & Arcades.

Story. A floor level within a building.

Structured Parking. Layers of parking stacked vertically.

Turret. A small tower or tower- shaped projection on a building. A mechanical room, HVAC system, or mechanical elements are not considered a turret.

Veranda: A porch or balcony, usually covered by a roof and often partly enclosed, extending along the outside of a building.

DRAFT

E. Permitted Uses

All uses shall be allowed in the Boundary Street Redevelopment District, except as outlined below.

1. Uses Permitted by Special Exception

The following uses are permitted by Special Exception as described in Section 3.16:

- Any use except for banks, that includes a drive-thru
- Fuel Sales
- Institutional Uses
- Passenger Terminals

2. Prohibited Uses

The following uses are prohibited:

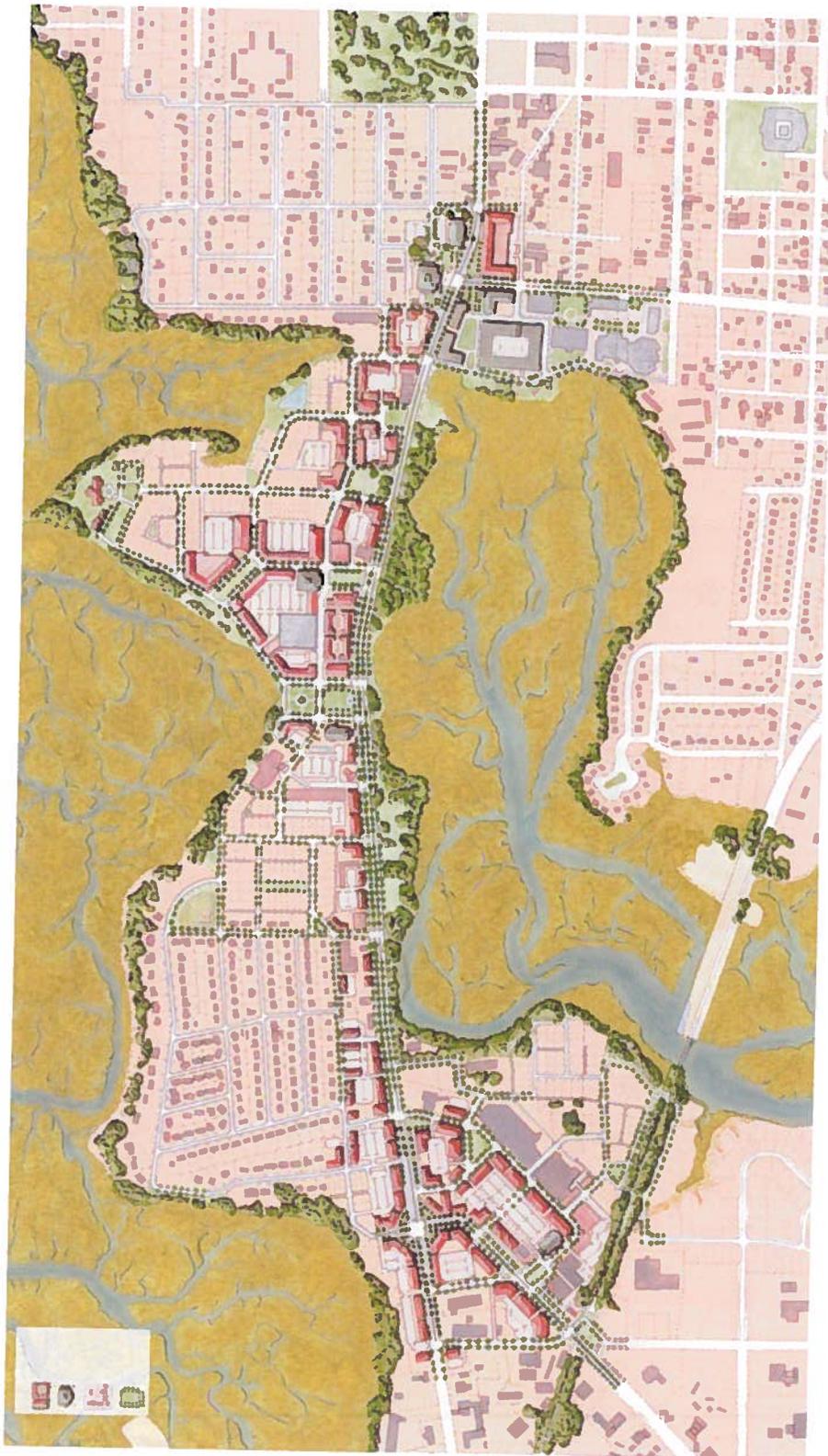
- Agriculture
- Aviation Services
- Car Wash, unless located entirely within a building
- Major Utility (ex., sewage treatment plant)
- Manufacturing and Production
- Outdoor Commercial Recreation Facilities
- Outdoor storage of materials and equipment (except during construction)
- Passenger Terminals (ex., airport; does not include bus terminals for mass transit)
- Self-storage on the street level
- Recreational Vehicle Park
- Restaurant, drive-in
- Signs prohibited in Section 7.2.C
- Sexually-Oriented Businesses
- Telecommunication Towers
- Truck Terminal
- Vehicle Sales and Service, Other, including boats, unless conducted entirely within a building
- Vehicle Service Limited, unless conducted entirely within a building
- Vehicle Service and Repair, unless conducted entirely within a building
- Warehousing
- Waste-Related Service

F. Master Plan

The Boundary Street Master Plan was created during a community design charrette which took place September 23rd – 29th, 2005 and involved input from major stakeholders, consultants, City Staff and the general public. The plan synthesizes community ideas and depicts the idealized build-out for the Boundary Street Redevelopment District. The Boundary Street Master Plan identifies key opportunity parcels for potential development, redevelopment, parking locations, and preservation. A large version of the Boundary Street Master Plan is available at the Department of Planning and Development Services.

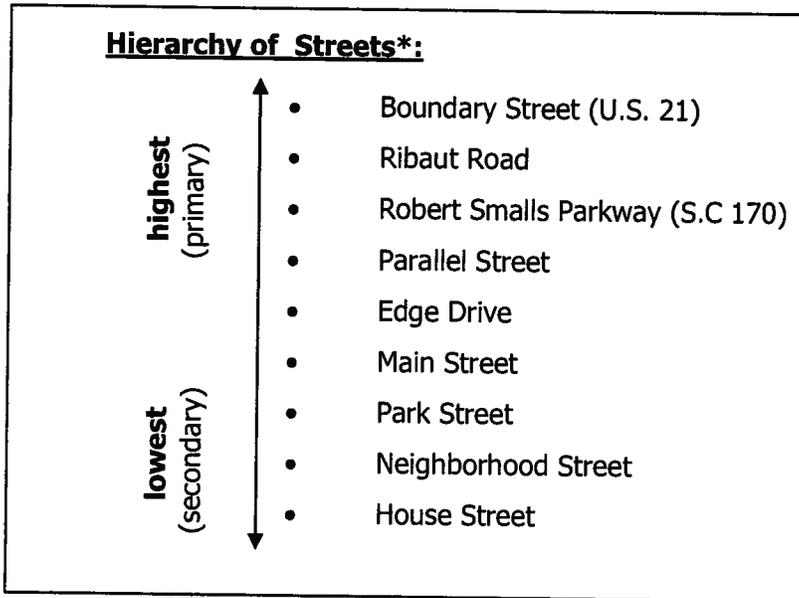
DRAFT

Article 6: District Development Standards
Section 6.8: Boundary Street Redevelopment District



G. Street Types

Development under this code is regulated by street type. The streets are related to each other in a hierarchical manner. When streets intersect, the primary street is determined by its higher order in the hierarchy. The code regulates individual parcels of land based on which type of street they front. The front of a building and its main entrance must face the primary street.



* Alleys are covered under General Design Standards, as they are never fronted by main structures.

The physical location of streets and street types are identified in the Regulating Plan. The Regulating Plan works hand-in-hand with the street type standards for rules regarding the particular details for each of the street types, such as building placement, building volume, uses, and street sections. The Regulating Plan identifies both existing and proposed Boundary Street Redevelopment District streets. Where a street currently does not exist, the developer shall build the street if access is needed to the site for project approval. In less immediate circumstances, property owners will deed to the City the portion of land needed to build a proposed street. In this case, the City, or other public entity, would build the street as time and resources permit. All streets built by either a private developer or the City shall conform to the dimensional requirements set forth in the corresponding street sections and to S.C. Department of Transportation standards for materials and construction methods. In addition the hierarchy of streets will serve to act as an informal phasing plan, wherein primary streets will receive prioritized attention.

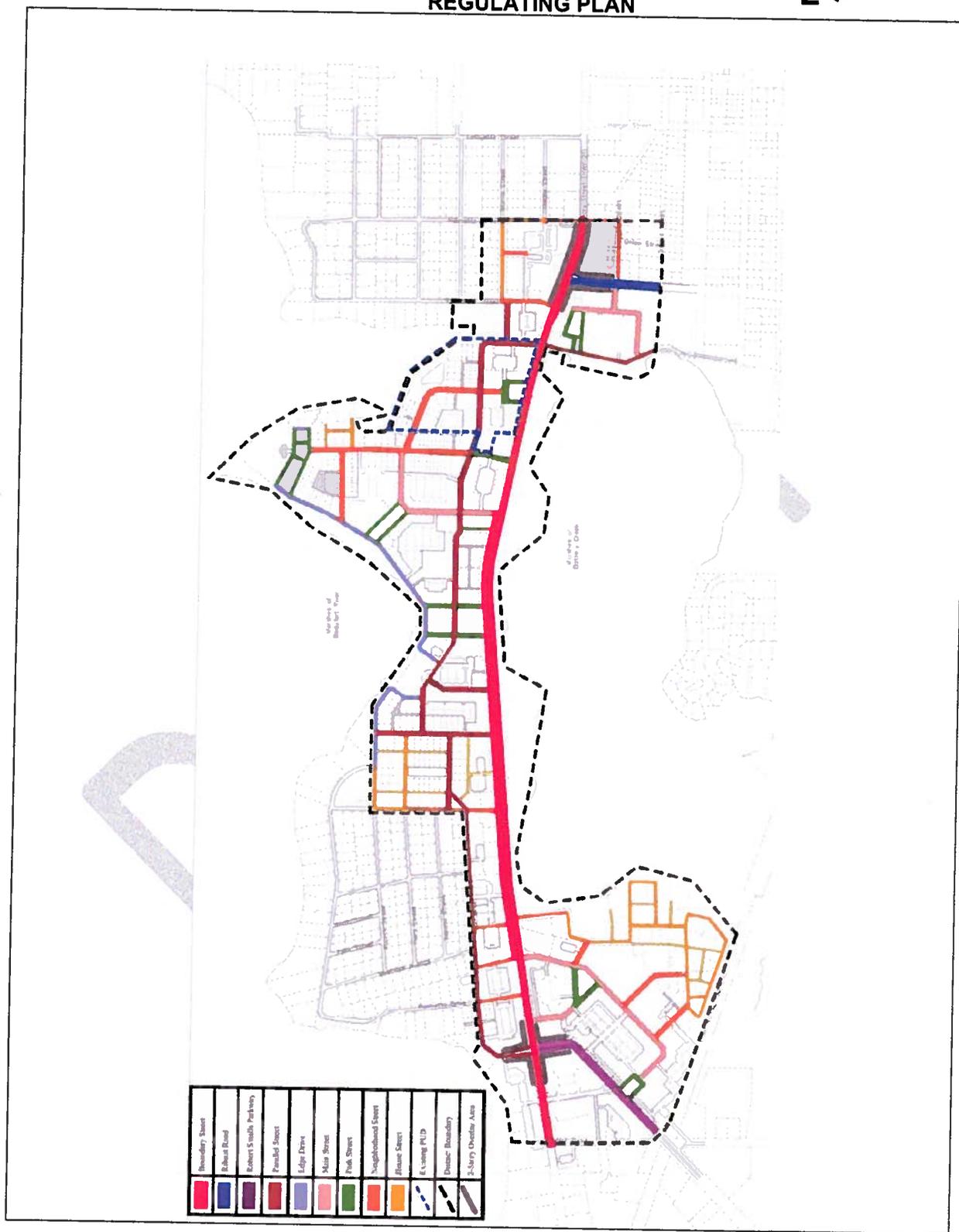
Any proposed roadway improvements must maintain the integrity of the Boundary Street Redevelopment District street types and corresponding street sections. In regards to street width, the right-of-way widths and street section specifications found

in the following Boundary Street Redevelopment District street type descriptions shall apply.

With respect to existing buildings within the Redevelopment District, their use may be maintained despite changes made to allowable uses. In the event that the building undergoes either structural or cosmetic modifications, if the cost of improvements exceeds 50% of the present building value, it will be subject to the standards set forth by this code. The City Architect will have final jurisdiction over this matter. Signs not conforming to the requirements of this section shall be altered, removed, or otherwise brought into compliance with the requirements of this section at the time that the required building improvements are made.

On the following pages, diagrammatic examples are used to illustrate example building locations, configurations, and dimensions. Particular details of the Boundary Street Master Plan and other sketches, illustrations, drawings and diagrams contained herein are subject to change, at the request of the affected property owner, with approval by the City Architect. The accompanying numbers and text are rules; the graphics are illustrative only.

REGULATING PLAN



1. Boundary Street

Boundary Street is the most important street in the redevelopment district. The street serves as a central roadway in the regional transportation network, connecting the region with Downtown Beaufort. While serving the important function of moving cars in and out of town, Boundary Street should also be a pedestrian friendly street. Due to physical and natural constraints the character of the corridor varies. Buildings along the corridor shall be mixed-use and have doors and windows facing the street.

B. Building Placement

Build-to-line Location: (typical) Varies depending on cross section (BS1, BS2, BS3, BS4, and BS5) (See section E below)

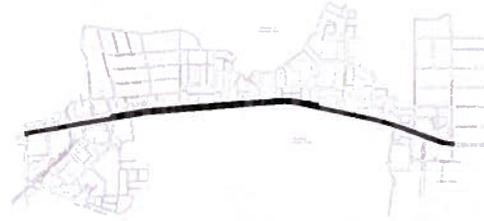
Side Setback: 0 ft.

Rear Setback: 5 ft.

D. Notes

1. Appurtenances may extend beyond the height limit.
2. Building fronts are required to provide shelter to the sidewalk or public entry by means of at least one of the following: arcade, colonnade, marquee, awning, or 2nd floor balcony.
3. For permitted uses, see Section 6.8.E.
4. The alignment of floor-to-floor heights of abutting buildings is encouraged to allow for shared use of elevators.
5. Where roads are owned by the SC DOT, or encumbered by utility easements at the frontage line, buildings may be setback enough to permit frontage elements if desired.
6. *See section 6.8.H.1 for more information on building heights.

A. Locator Diagram



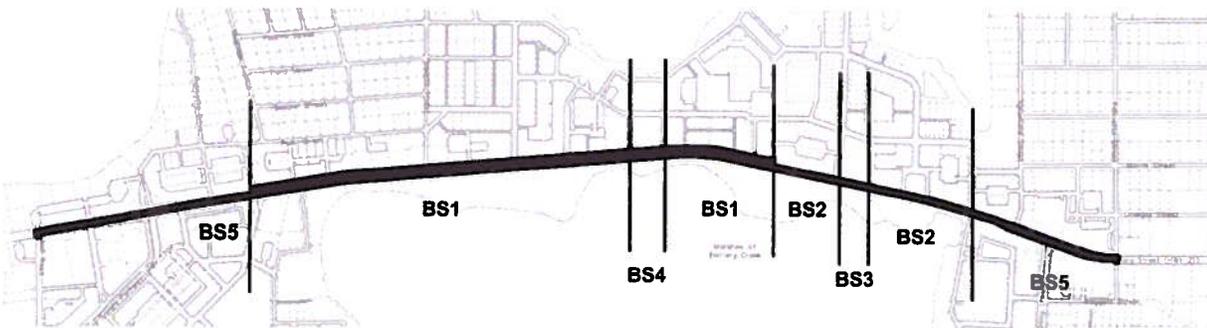
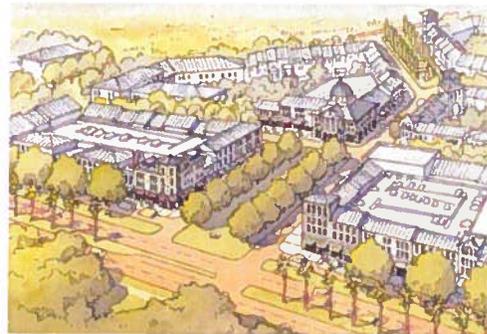
C. Building Volume

Building Width: 16 ft. minimum
160 ft. maximum

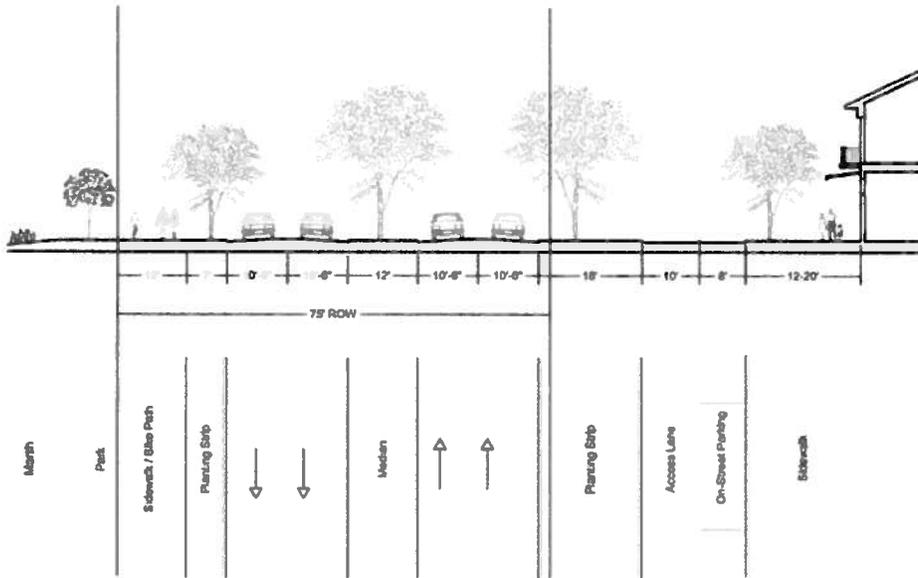
Building Height: *2 story minimum
5 story maximum

Lot Coverage: 80 % maximum

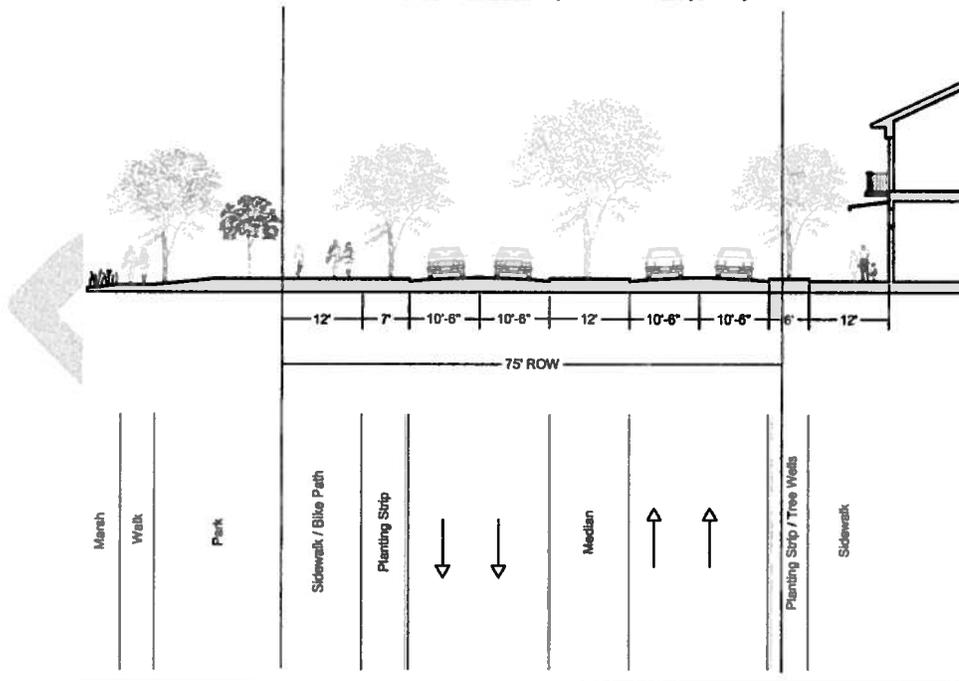
Frontage Build-Out: 60 % minimum



E. Street Section

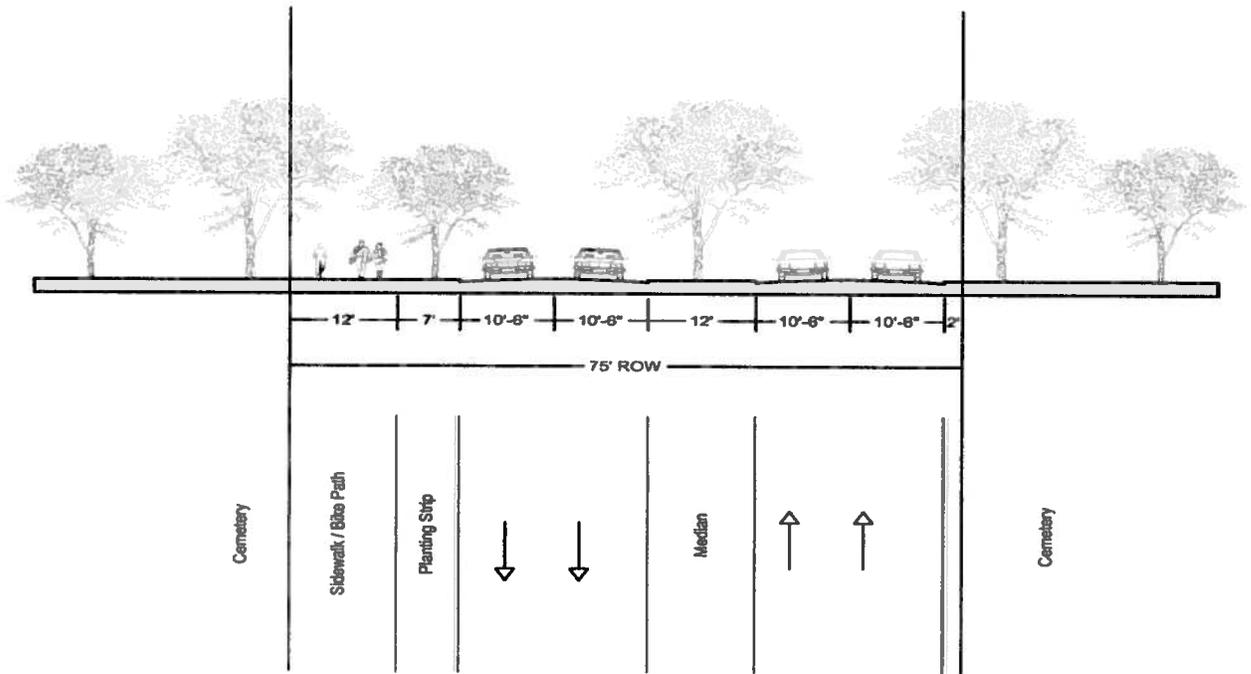


Section for Boundary Street 1 (BS1)

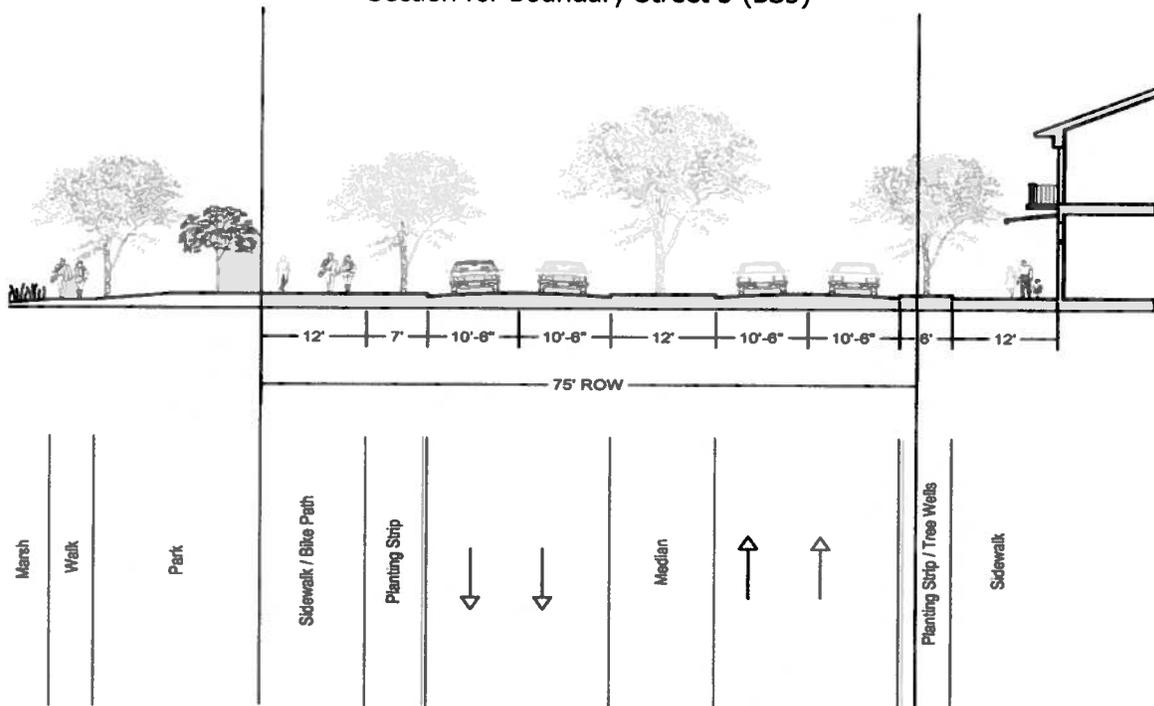


Section for Boundary Street 2 (BS2)

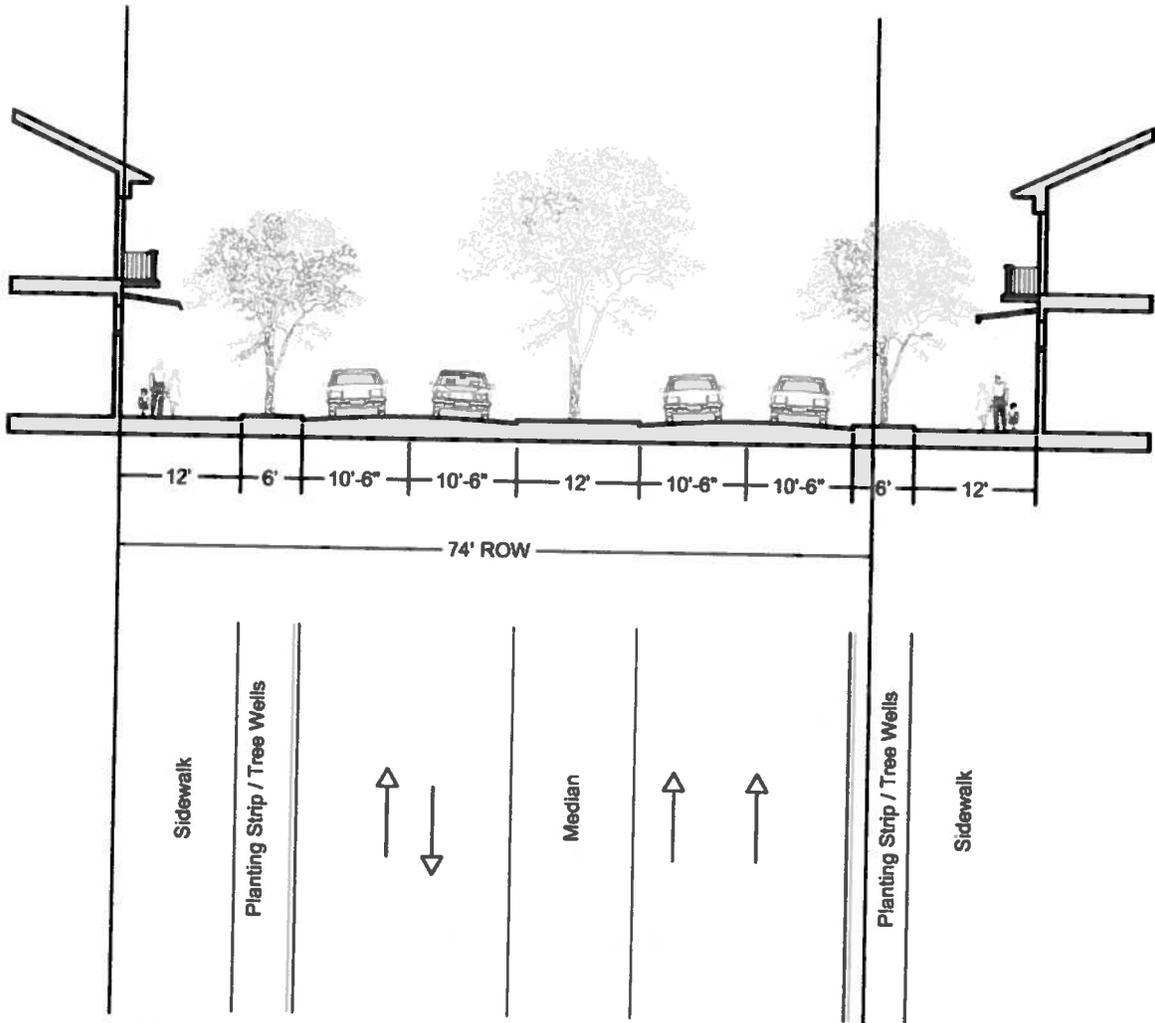
Article 6: District Development Standards
 Section 6.8: Boundary Street Redevelopment District



Section for Boundary Street 3 (BS3)



Section for Boundary Street 4 (BS4)



Section for Boundary Street 5 (BS5)

2. Ribaut Road

The intersection of Ribaut Road and Boundary Street marks an important gateway to the historic downtown. Ribaut Road is terminated by the proposed Beaufort City Hall. Ribaut Road is transformed into a mixed-use, walkable street. Multi-story buildings will frame the street. The standards for Ribaut Road apply to the segment from Boundary Street south to Greene Street.

B. Building Placement

Build-to-line Location: 0 ft. from ROW (typical)
 Side Setback: 0 ft.
 Rear Setback: 5 ft.

D. Notes

1. Appurtenances may extend beyond height limit.
2. Building fronts are required to provide shelter to the sidewalk by means of at least one of the following: arcade, colonnade, marquee, awning, or 2nd floor balcony.
3. For permitted uses, see Section 6.8.E.
4. The alignment of floor-to-floor heights of abutting buildings is encouraged to allow for shared use of elevators.
5. Where roads are owned by the SC DOT, or encumbered by utility easements at the frontage line, buildings may be setback enough to permit frontage elements if desired.
6. *See section 6.8.H.1 for more information on building heights.



A. Locator Diagram



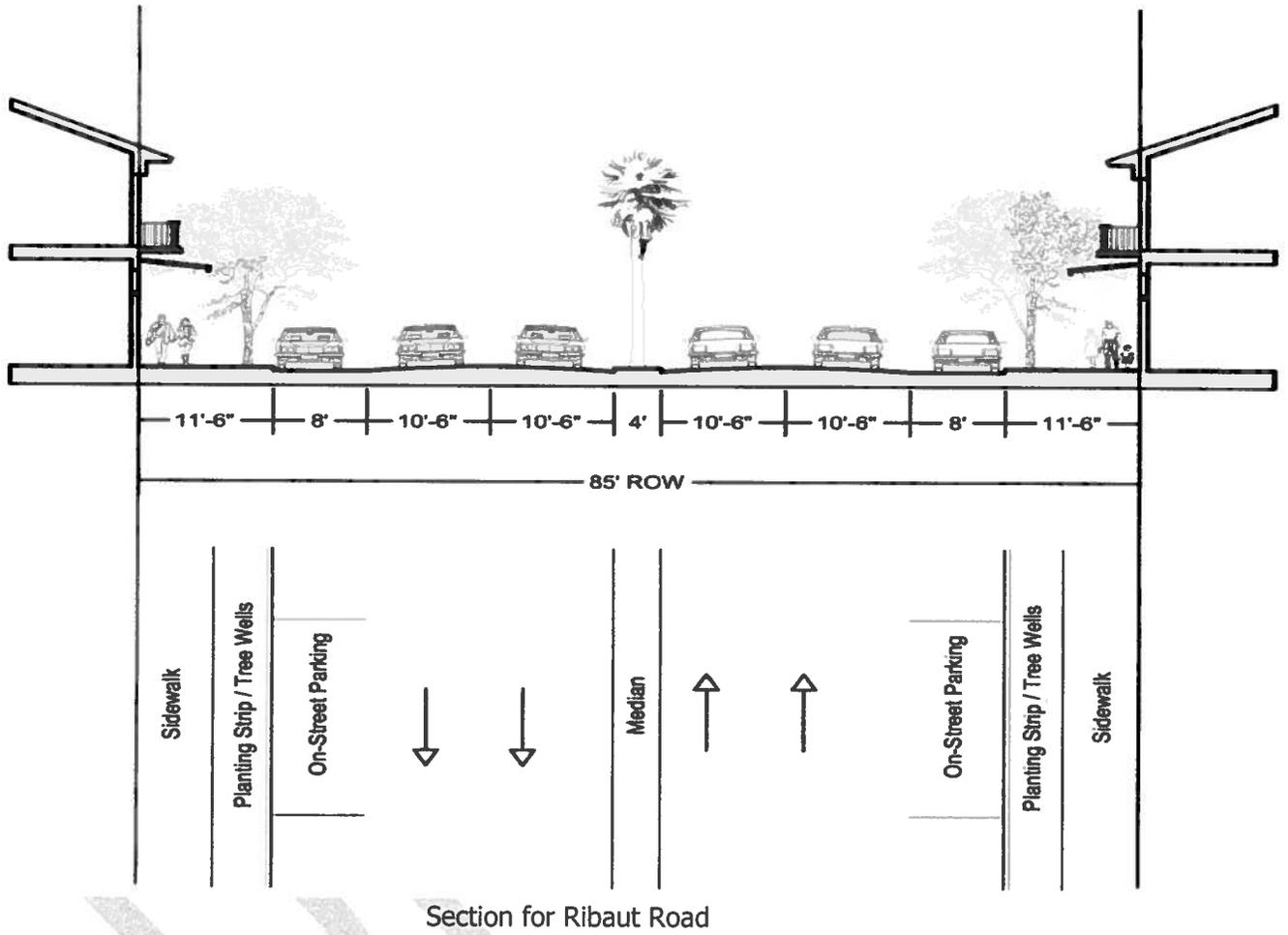
C. Building Volume

Building Width: 16 ft. minimum
 160 ft. maximum
 Building Height: *2 story minimum
 5 story maximum
 60 ft. maximum
 Lot Coverage: 80 % maximum

Frontage Build-Out: 60 % minimum



E. Street Section



3. Robert Smalls Parkway

The intersection of Robert Smalls Parkway and Boundary Street moves cars along these busy corridors, but also serves as the western gateway to town. In order to enhance the intersection, while continuing to improve the traffic flow, a roundabout may be placed at the realigned intersection. The properties along Robert Smalls Parkway should be redeveloped to form a town center. The area would contain a mix of uses and building types, all with doors and windows facing the street.

B. Building Placement

Build-to-line Location: 0 feet from ROW if not providing front access lane, or 40 feet from ROW if providing front access lane. (See section E below)

Side Setback: 0 ft.

Rear Setback: 5 ft.

D. Notes

1. Appurtenances may extend beyond height limit.
2. Building fronts are required to provide shelter to the sidewalk by means of at least one of the following: arcade, colonnade, marquee, awning, or 2nd floor balcony.
3. For permitted uses, see Section 6.8.E.
4. The alignment of floor-to-floor heights of abutting buildings is encouraged to allow for shared use of elevators.
5. Where roads are owned by the SC DOT, or encumbered by utility easements at the frontage line, buildings may be setback enough to permit frontage elements if desired.
6. *See section 6.8.H.1 for more information on building heights.



A. Locator Diagram



C. Building Volume

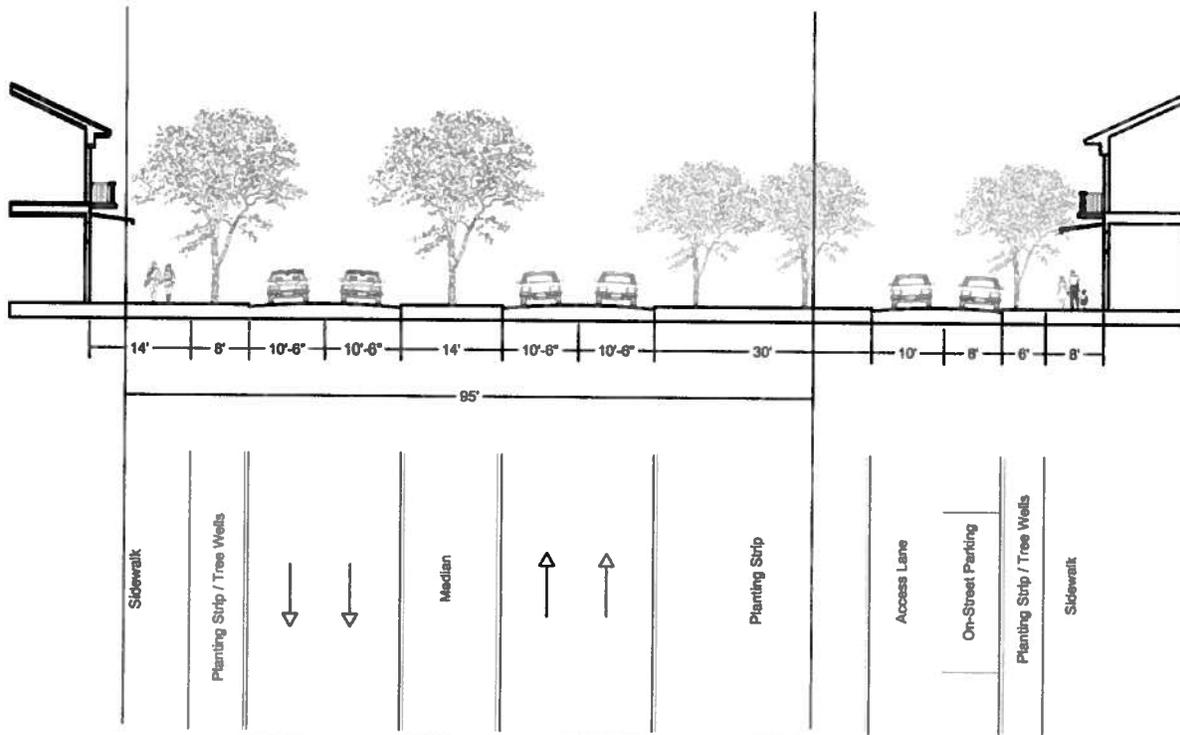
Building Width: 16 ft. minimum
160 ft. maximum

Building Height: *2 story minimum
5 story maximum
60 ft. maximum

Lot Coverage: 80 % maximum
Frontage Build-Out: 60 % minimum



E. Street Section

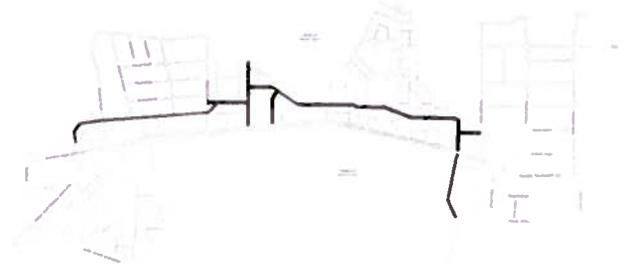


Section for Robert Smalls Parkway

4. Parallel Street

A parallel street is essential in creating an interconnected network of blocks and streets adjacent to Boundary Street. The addition of an east – west parallel street should be created so that all daily trips, especially local ones, do not have to use Boundary Street. The character of the built environment along the parallel street varies from less intense residential development to more intense mixed-use development. All buildings along the parallel street should be street oriented with doors and windows facing the street.

A. Locator Diagram



B. Building Placement

Build-to-zone Location: 0 ft. – 15 ft. from ROW (typical)

Side Setback: 5 ft.

Rear Setback: 5 ft.

C. Building Volume

Building Width: 16 ft. minimum
160 ft. maximum

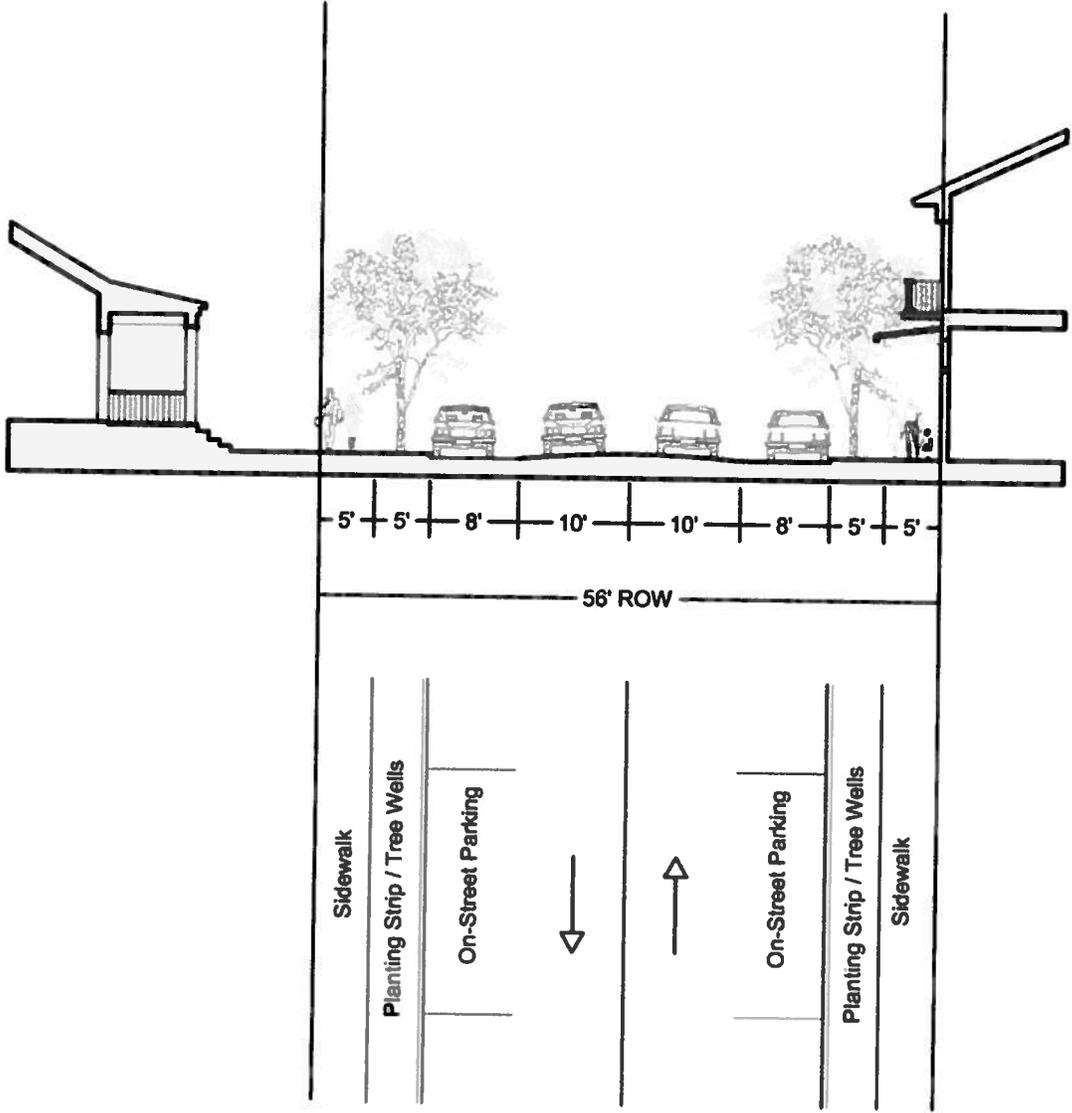
Building Height: *2 story minimum
4 story maximum
60 ft. maximum

Lot Coverage: 75 % maximum
Frontage Build-Out: 60 % minimum

D. Notes

1. Appurtenances may extend beyond the height limit.
2. Building fronts are required to provide shelter to the sidewalk by means of at least one of the following: arcade, colonnade, marquee, awning, or 2nd floor balcony.
3. For permitted uses, see Section 6.8.E.
4. The alignment of floor-to-floor heights of abutting buildings is encouraged to allow for shared use of elevators.
5. *See section 6.8.H.1 for more information on building heights.





Section for Parallel Street

5. Edge Drive

The edge drive runs along the marshfront, offering scenic views of Albergotti Creek. Residential or mixed-use buildings line one side of the edge drive while the marshfront remains unobstructed from view and available to the public to experience. The edge drive also serves as an alternative to east – west travel along Boundary Street and serves as an important element in an interconnected street network.

B. Building Placement

Build-to-zone Location: 5 ft - 15 ft. from ROW (typical)

Side Setback: 0 ft.

Rear Setback: 5 ft.

A. Locator Diagram



C. Building Volume

Building Width: 16 ft. minimum
160 ft. maximum

Building Height: *1½ 2 story minimum
4 story maximum
60 ft. maximum

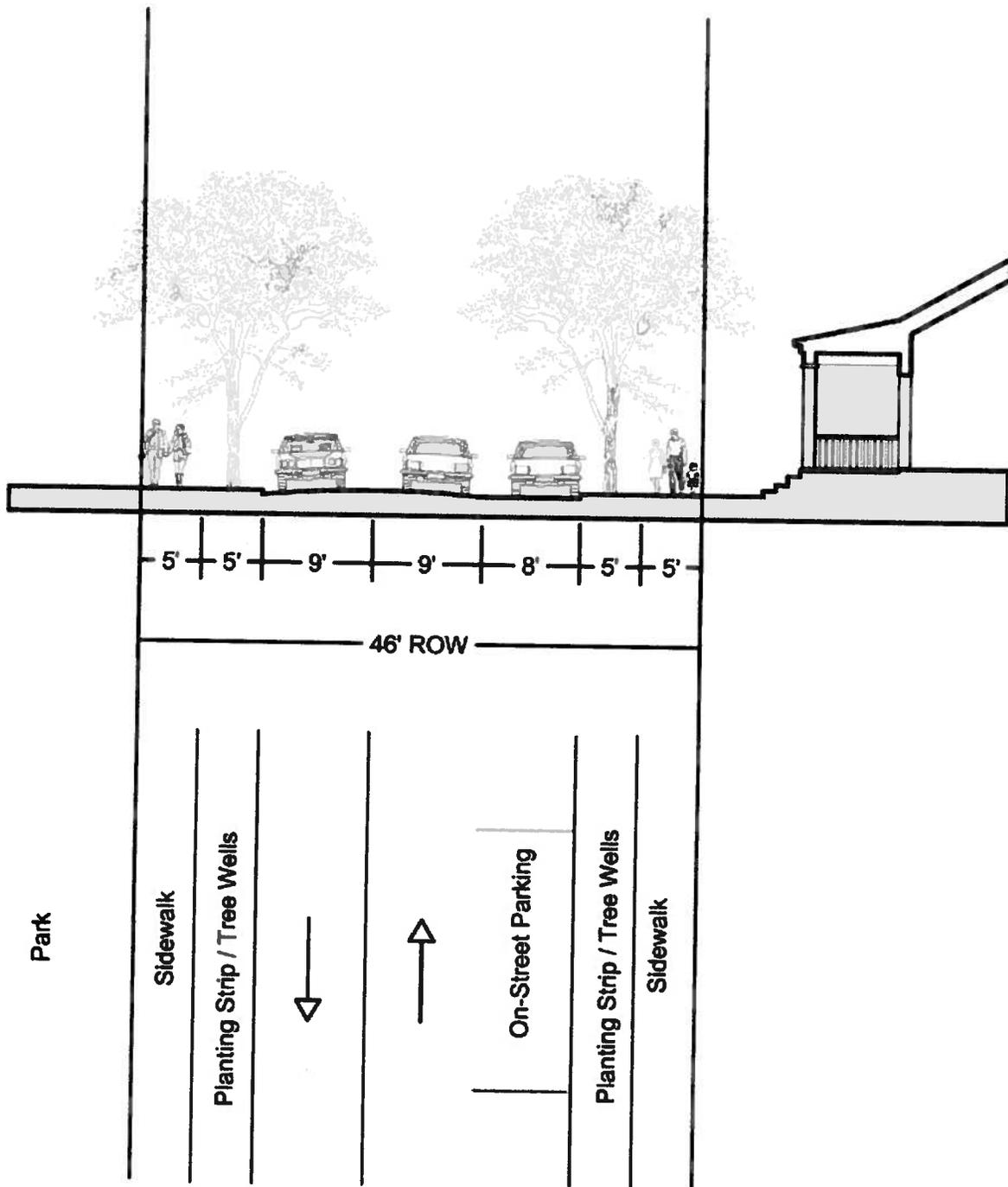
Lot Coverage: 75 % maximum
Frontage Build-Out: 40 % minimum

D. Notes

1. Appurtenances may extend beyond the height limit.
2. For permitted uses, see Section 6.8.E.
3. The alignment of floor-to-floor heights of abutting buildings is encouraged to allow for shared use of elevators.
6. *See section 6.8.H.1 for more information on building heights.



E. Street Section



Section for Edge Drive

Article 6: District Development Standards
 Section 6.8: Boundary Street Redevelopment District



A. Locator Diagram



B. Building Placement

Build-to-line Location: 0 ft. from ROW
 (typical)

Side Setback: 0 ft.

Rear Setback: 5 ft.

C. Building Volume

Building Width: 16 ft. minimum
 160 ft. maximum

Building Height: *2 story minimum
 5 story maximum
 60 ft. maximum

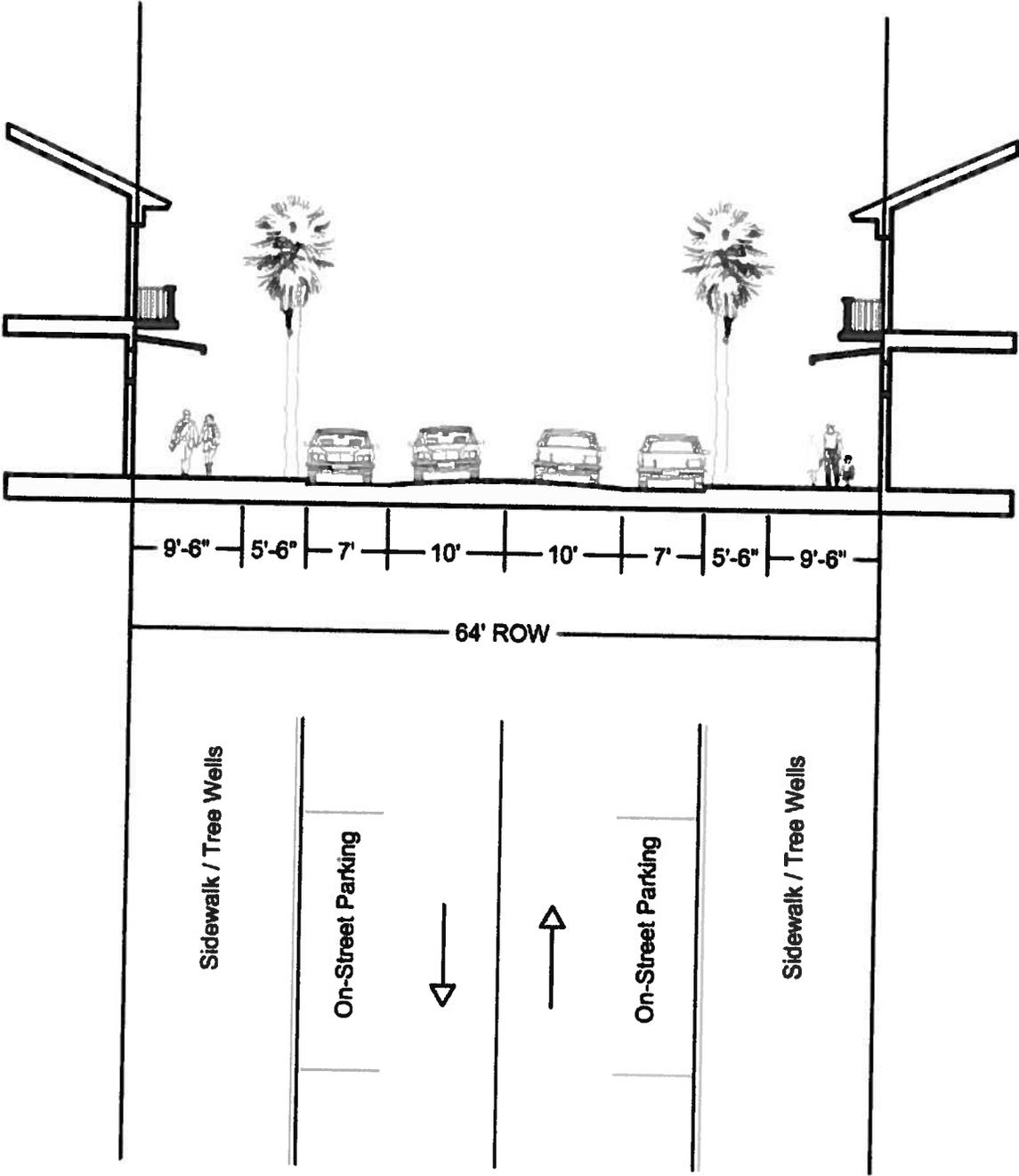
D. Notes

1. Appurtenances may extend beyond the height limit.
2. Building fronts are required to provide shelter to the sidewalk by means of at least one of the following: arcade, colonnade, marquee, awning, or 2nd floor balcony.
3. For permitted uses, see Section 6.8.E.
4. The alignment of floor-to-floor heights of abutting buildings is encouraged to allow for shared use of elevators.
5. Where roads are owned by the SC DOT, or encumbered by utility easements at the frontage line, buildings may be setback enough to permit frontage elements if desired.
6. *See section 6.8.H.1 for more information on building heights.

Lot Coverage: 80 % maximum
 Frontage Build-Out: 60 % minimum



E. Street Section



Section for Main Street

7. Park Street

Park Streets are intended to be either fronted by buildings on one side or have no buildings on either side. In many cases a Civic Building is centered on a park. Parks are important to the character of the Boundary Street Redevelopment District and a series of proposed neighborhood parks are included in the master plan. Parks create value and buildings located adjacent to parks should be of the highest quality. Park Streets serve as drives along parks and open spaces.

B. Building Placement

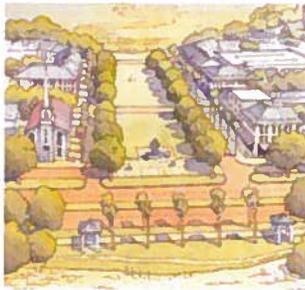
Build-to-line Location: 0 ft. from ROW
 (typical)

Side Setback: 0 ft.

Rear Setback: 5 ft.

D. Notes

1. Appurtenances may extend beyond the height limit.
2. Building fronts are required to provide shelter to the sidewalk by means of at least one of the following: arcade, colonnade, marquee, awning, or 2nd floor balcony.
3. For permitted uses, see Section 6.8.E.
4. The alignment of floor-to-floor heights of abutting buildings is encouraged to allow for shared use of elevators.
5. Where roads are owned by the SC DOT, or encumbered by utility easements at the frontage line, buildings may be setback enough to permit frontage elements if desired.
6. *See section 6.8.H.1 for more information on building heights.



A. Locator Diagram



C. Building Volume

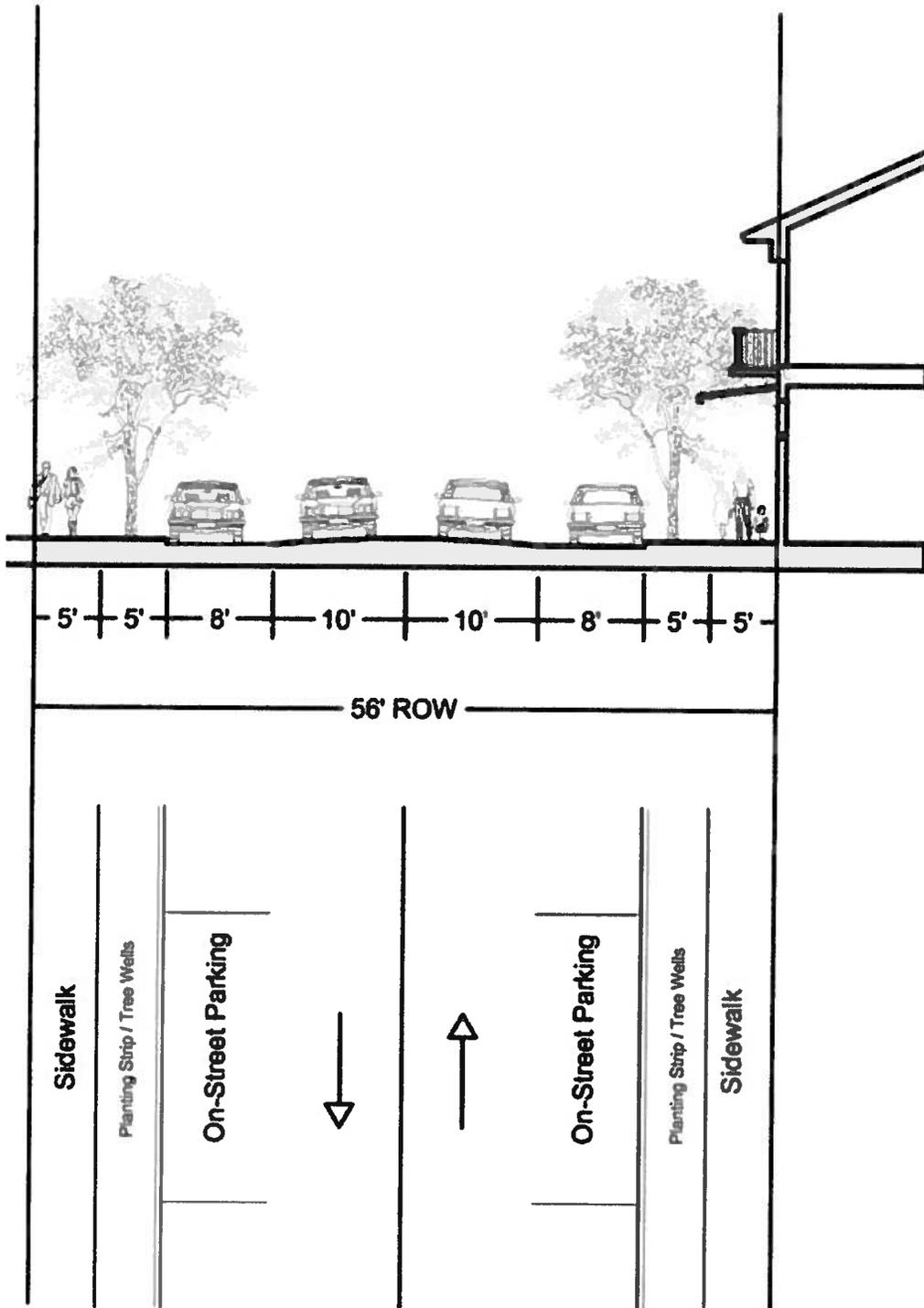
Building Width: 16 ft. minimum
 160 ft. maximum

Building Height: *2 story minimum
 5 story maximum
 60 ft. maximum

Lot Coverage: 80 % maximum
 Frontage Build-Out: 60 % minimum



E. Street Section

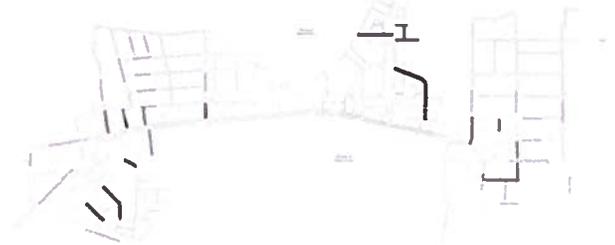


Section for Park Street

8. Neighborhood Street

The Neighborhood Street presents an interesting mix of urban living and traditional building types. Such building types include apartments, condominiums, live-work units, townhouses, and smaller detached houses. Build-to lines are varied. The Neighborhood Street allows for narrow travel lanes and parking on both sides of the street. A green strip is included as well as a wide sidewalk for pedestrians.

A. Locator Diagram



B. Building Placement

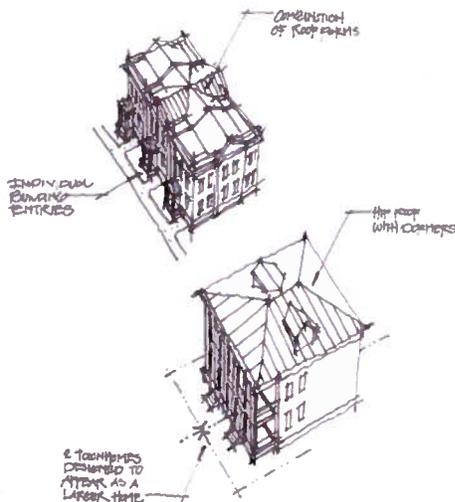
Build-to-zone Location: (typical)	0 ft. – 15 ft. from ROW
Side Setback:	0 ft.
Rear Setback:	5 ft.

C. Building Volume

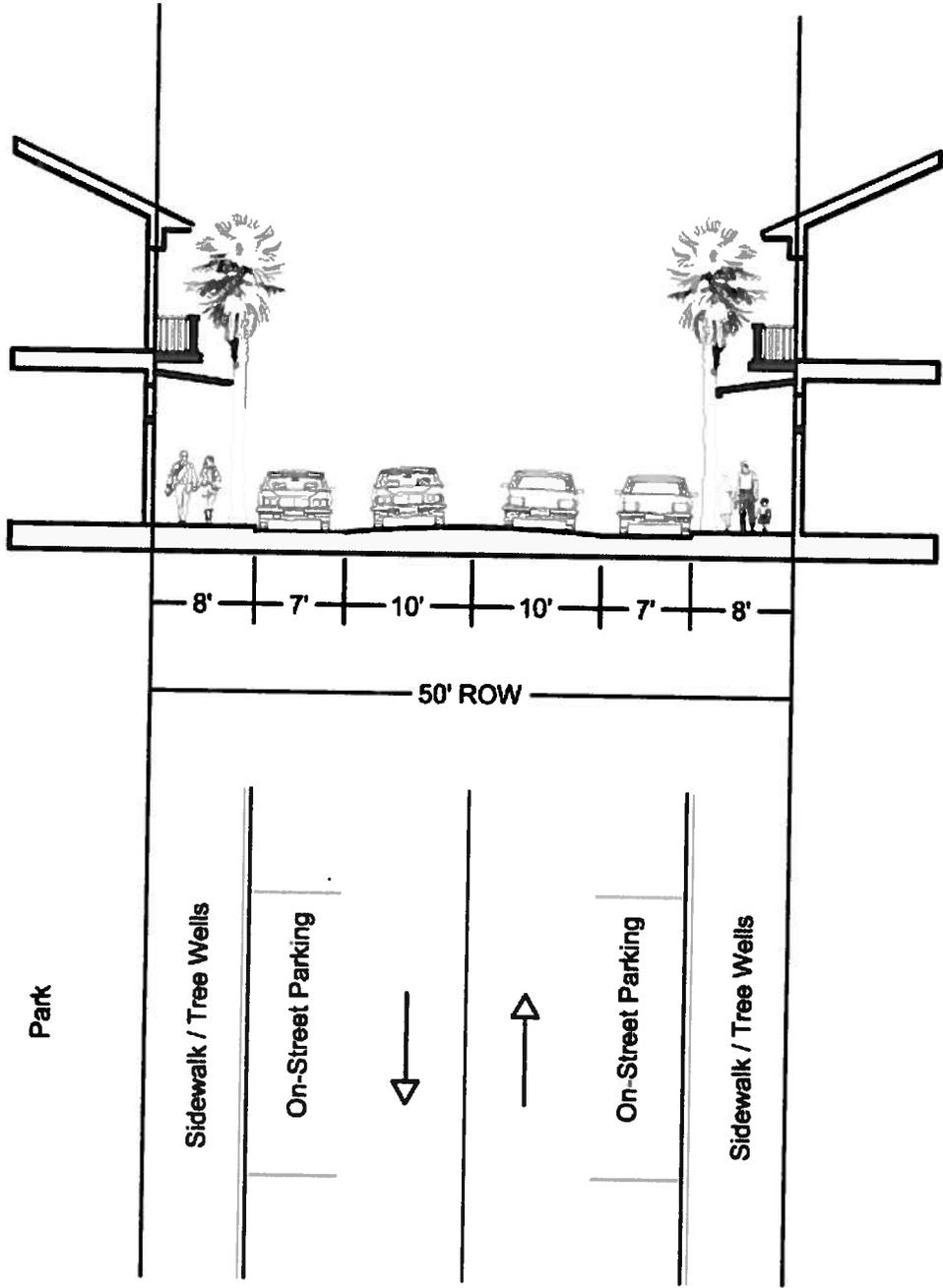
Building Width:	16 ft. minimum 160 ft. maximum
Building Height:	1½ 2 story minimum 4 story maximum 60 ft. maximum
Lot Coverage:	75 % maximum
Frontage Build-Out:	40 % minimum

D. Notes

1. Appurtenances may extend beyond the height limit.
2. Building fronts are required to have at least one of the following: porch or stoop.
3. For permitted uses, see Section 6.8.E.
4. The alignment of floor-to-floor heights of abutting buildings is encouraged to allow for shared use of elevators.



E. Street Section



Section for Neighborhood Street

9. House Street

The House Street is a quieter, more intimate street. The street type evokes the character of historic Beaufort residential streets. Buildings are set further back and buildings reflect the existing character of prominent Beaufort streets such as Craven Street east of Carteret Street.

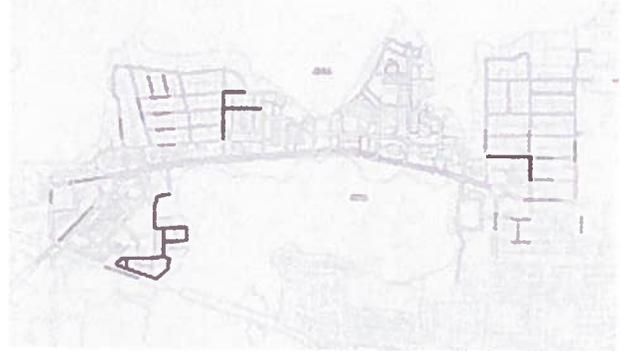
B. Building Placement

Build-to-zone Location: 10 ft. - 25 ft. from ROW
(typical)

Side Setback: 5 ft.

Rear Setback: 5 ft.

A. Locator Diagram



C. Building Volume

Building Width: 16 ft. minimum
40 ft. maximum

Building Height: 1½ story minimum
3 story maximum
60 ft. maximum

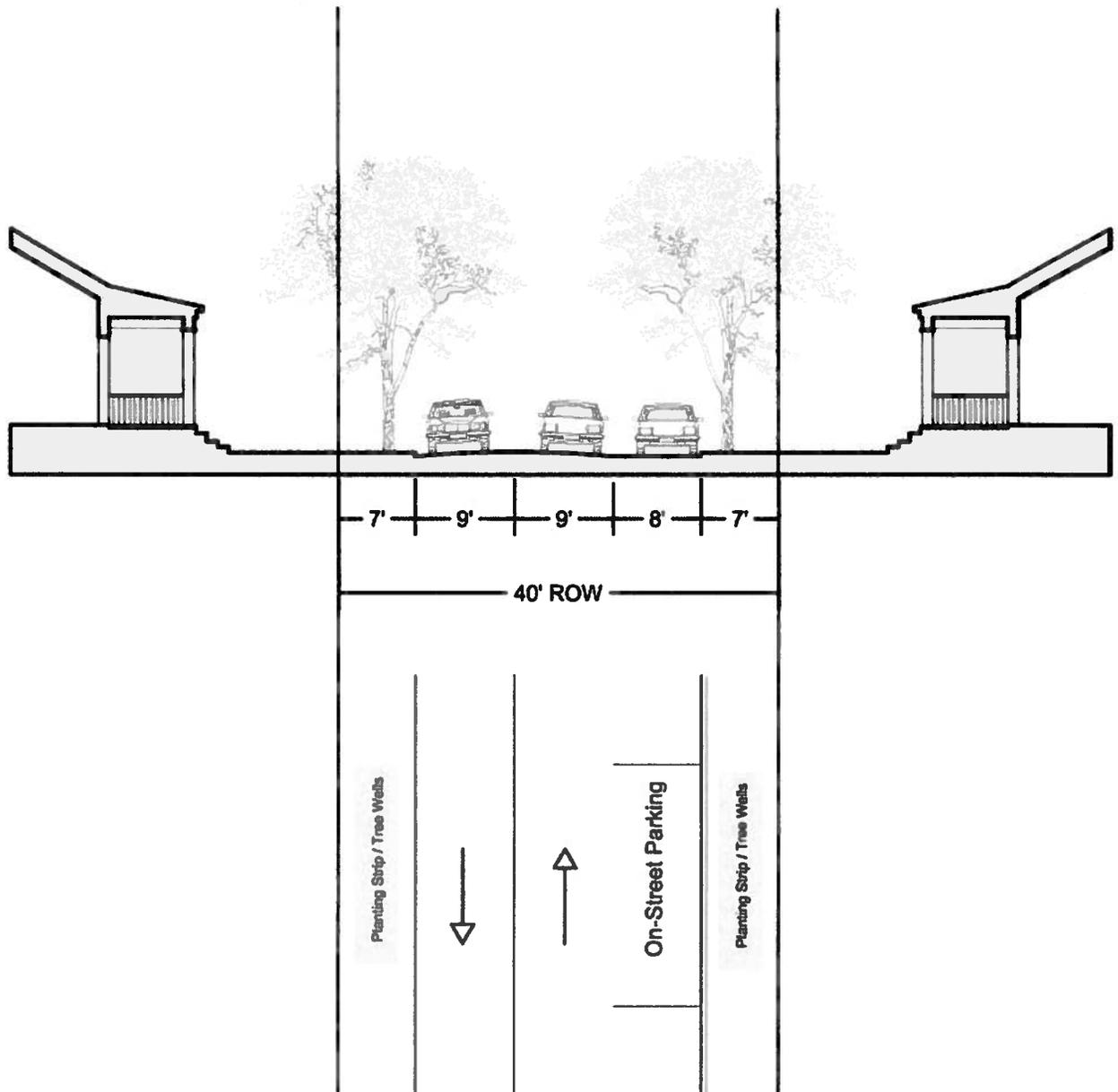
Lot Coverage: 65 % maximum
Frontage Build-Out: 40 % minimum

D. Notes

1. Appurtenances may extend beyond the height limit.
2. For permitted uses, see Section 6.8.E.
3. Building fronts are required to have at least one of the following: porch or stoop.



E. Street Section



H. General Design Standards

1. Building Heights

In the Boundary Street Redevelopment District, overall building heights are regulated by the number of stories, based on the designated Street Types. The following requirements pertain to specific heights of design elements, based on building use and location.

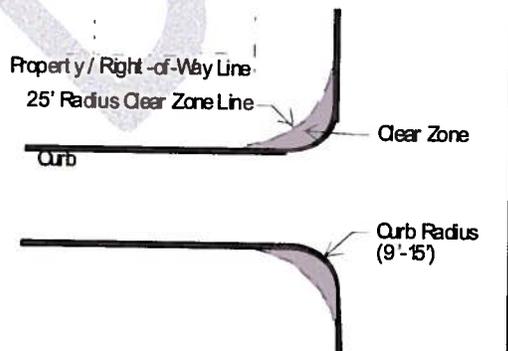
- a. On all streets
 - i. One-story commercial buildings and mixed-use structures are permitted EXCEPT in locations specified as 2-Story Overlay Areas. These occur at the intersections of Boundary Street & Robert Smalls Parkway and Boundary Street & Ribaut Road.
 - ii. The ground floor of one-story commercial buildings and mixed-use structures shall be a minimum of 14' and a maximum of 20'.
 - iii. Residential buildings must be a minimum of 1.5-2-stories, as listed in each specific street area standard.
- b. On Boundary Street, Ribaut Road, and Robert Smalls Parkway:
 - i. The ground floor of multi-story commercial buildings and mixed-use structures with ground-floor commercial shall be a minimum of 12', and a maximum of 16' high from finished floor to ceiling.
 - ii. The ground floor of all residential structures shall be a minimum of 10', and a maximum 14' high from finished floor to ceiling.
- c. On all other streets:
 - i. The ground floor of multi-story commercial buildings and mixed-use structures with ground-floor commercial shall be a minimum of 11' and a maximum of 16' high from finished floor to ceiling.
 - ii. The ground floor of multifamily residential structures shall be a minimum of 10', and a maximum of 14' high from finished floor to ceiling.
 - iii. The ground floor of single-family, townhouse, and two and three-family dwellings shall be a minimum of 9', and a maximum of 14' high from finished floor to ceiling.
- d. Each story above the first story in commercial and residential buildings shall be a minimum of 8' feet and a maximum of 12' high from floor to ceiling. Floors more than 12', as measured from floor to ceiling, will count as additional floors. A half story is a finished living floor which is contained wholly or predominantly within the roof of a structure and is subject to the regulations of the local building code.
- e. 2-Story Overlay Area: The intent of the requirement for two-story buildings along these special intersections is create appropriately scaled street walls, to promote traditional urban mixed use development, and to ensure upper level activity along street facades contributing to the overall vitality of

the area and providing eyes on the street from upper level windows. Where a two-story minimum building height is required, A Mezzanine is also permitted. The intent of mezzanines is to provide upper level activity along street facades. Thus a building with a Mezzanine that pulls away from the primary facades does not satisfy the intent of this code requirement. Mezzanines must meet following conditions:

- (A). All buildings must include a minimum massing of 2 stories extending fully along all street fronting facades and any side fronting facade or facade visible from a corner. If a building cannot meet this standard through the use of a mezzanine, then the mezzanine will not be permitted to fulfill the 2 story requirement.
- (B). A mezzanine must be designed as useable primary interior floor space, not storage, or other secondary, mechanical or service functions.
- (C). All mezzanine floor space must be located in direct contact with street fronting second floor windows.
- (D). A mezzanine level must meet all ceiling height standards of a second floor.
- (E). Mezzanine levels are required to meet all of the minimum-maximum window transparency requirements of a second floor.

2. Corner Radii and Clear Zones

Corner curb radii shall be between 9 feet and 15 feet. Fairly tight turning radii shorten pedestrian crossings and inhibit reckless drivers from turning corners at high speeds. To allow for emergency vehicles (e.g. fire trucks) to turn corners, a 25 foot radius Clear Zone shall be established free of all vertical obstructions such as telephone poles, sign poles, fire hydrants, electrical boxes, or newspaper boxes, etc. Wheelchair accessible ramps will be provided at intersections within the Clear Zone for disabled access.

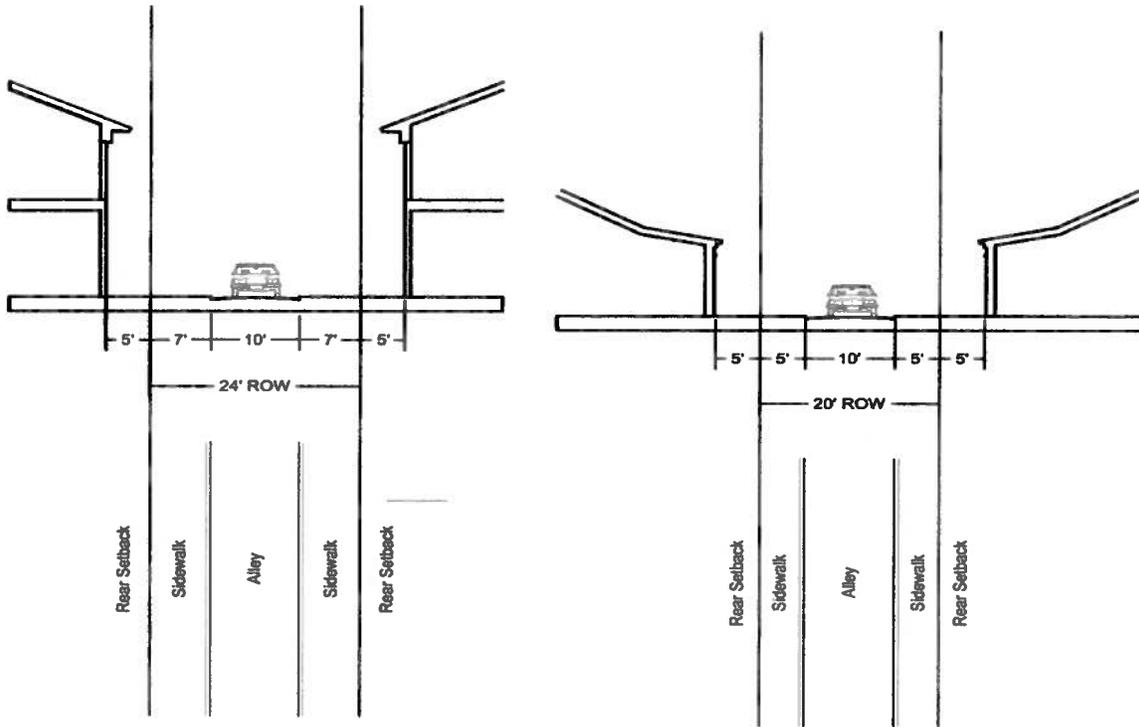


3. Streets

All applications for development in the Boundary Street Redevelopment District must include the design of street elements adjacent to the applicant's property. These designs must be according to the Street Type adjacent to the property, as identified in the Street Types Section (Section F). The Street Type and the appropriate thoroughfare sections inform the applicant regarding how parking, sidewalk, tree plantings and other elements are to be sized and arranged. Contact with City Architect will provide clarification regarding the required design for each street. All streets and alleys shall connect to other streets. Cul-de-sacs and T-turnarounds are not permitted.

4. Alleys

Alleys are required in the Boundary Street Redevelopment District to minimize curb cuts and to provide access to parking and service areas behind buildings. Alley requirements may be waived by the City Architect for access to detached single family residential lots greater than 45 feet in width in situations in which proper streetfront orientation, pedestrian circulation, and parking can still be accomplished. Alley locations and dimensions are not fixed but shall be designed to accommodate the alley's purpose. Alleys may be incorporated into parking lots as drive aisles and fire lanes. Recommended sections for alleys are included below.



Commercial / Mixed-use Alley Section

Residential Alley Section

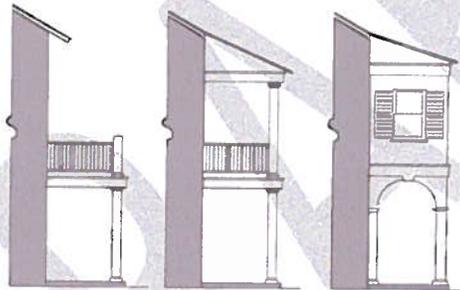
5. Frontage Build-Out

In order to create a continuous built environment along the street frontage, lots are required to have a certain percentage of buildings along their setback or build-to lines, depending on the street. For large parcels that may contain multiple structures, an overall plan must be presented to ensure that the frontage build-out requirement is being met. The following are frontage types that are permitted along these frontages:

a. Commercial / Mixed-use Buildings:

The following standards shall apply to commercial / mixed-use structures in the District. Each are permitted in the district but are not required; however, for buildings fronting Boundary Street, Ribaut Road, Robert Smalls Parkway, Parallel Street, Main Street, and Park Street building fronts are required to provide shelter to the sidewalk by means of at least one of the following: arcade, colonnade, marquee, awning, or 2nd floor balcony. Alternate means may be deemed appropriate as approved by the City Architect.

i Colonnades / Arcades



Note: enclosed useable space not permitted in the right-of-way.

Depth = 8 ft minimum from the principal façade to the inside column face.
18" from outside column face to curb.

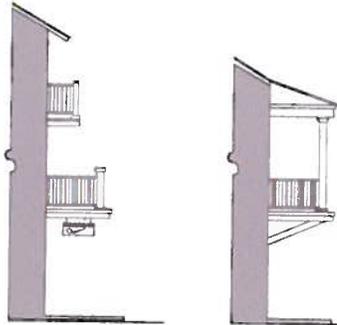
Height = 10 ft minimum clear.

Length = ~~75% - 100% of Building Front (for Storefronts only).~~ 50% minimum of Building Front.

Open multi-story verandas, awnings, and balconies, and enclosed useable space shall be permitted above the colonnade. Enclosed useable space shall be permitted above the colonnade when not located in the right-of-way.

Colonnades shall only be constructed where the minimum depth can be obtained. Colonnades shall occur forward of the principal façade and may encroach within the right-of-way, but shall not extend past the curb line. Colonnades, if located in the right-of-way, may replace street trees along their length. Colonnades that encroach into the right-of-way may not have enclosed useable space above. On corners, colonnades may wrap around the side of the building facing the side street.

ii. Balconies



Depth = 5 ft minimum for 2nd floor balconies.

Height = 10 ft minimum clear.

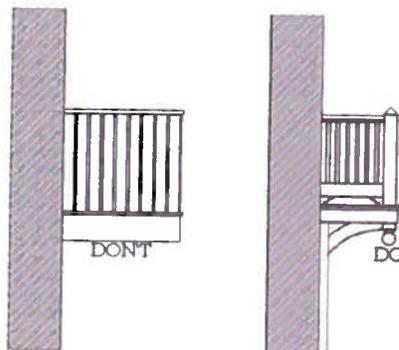
Length = up to 100% of Building Front (for Storefronts only).

Balconies may differ in length and depth.

Balconies shall occur forward of the Build-to Line and may encroach over the right-of-way. If ROW is owned by SCDOT, City Architect can adjust the Build-to Line to accommodate a balcony.

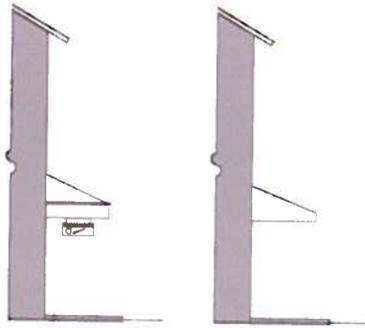
Balconies may have roofs, but are required to be open-air parts of the building; i.e., balconies cannot be screened or glassed in.

On corners, balconies may wrap around the side of the building facing the side street.



Balconies should always be supported in some way and should also appear safe to stand on and under.

iv. Marquees & Awnings



- Depth = 6 ft minimum.
- Height = 8 ft minimum clear.
- Length = 50% **minimum** to 100% of Building Front (for Storefronts only).

The above requirements apply to first floor awnings. There are no minimum requirements for awnings above the first floor.

Marquees and Awnings shall occur forward of the Build-to Line and may encroach over the right-of-way.

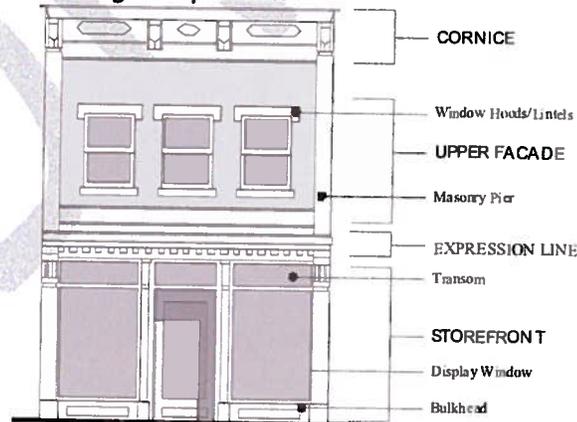
Awnings shall be made of fabric **or metal**. High-gloss or plasticized fabrics are prohibited.

v. Storefronts

Storefronts are encouraged as a building frontage type in retail applications.

Required for all buildings that have storefronts:

(1) Building Components



(2) Opacity

In order to provide clear views of merchandise in stores and to provide natural surveillance of exterior street spaces, the ground-floor along the building frontage shall have untinted transparent storefront windows and / or doors covering a substantial amount of ~~no less than 75% of the~~ storefront wall area. Low emissivity glass with high visual light transmittance is permitted. Bottoms of the storefront windows shall be between 1 and 3 feet above sidewalk grade. Storefronts shall remain unshuttered at night and shall provide clear views of interior spaces lit from within. This frontage type is often used in combination with an awning.

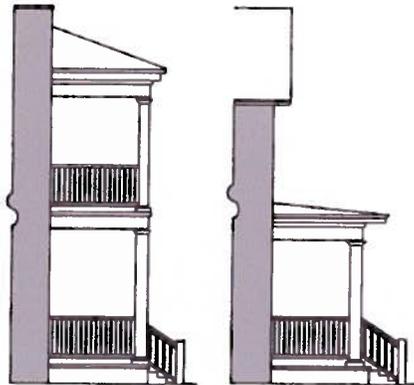
(3) Doors or Entrances

Doors or entrances with public access shall be provided at intervals no greater than 50 feet, unless otherwise approved by the City Architect.

b. Residential Buildings:

The following standards shall apply to residential structures in the District. Each are permitted in the district but are not required; however, for buildings fronting Neighborhood Streets or House Streets, building fronts are required to have at least one of the following: porch or stoop.

i. Porches



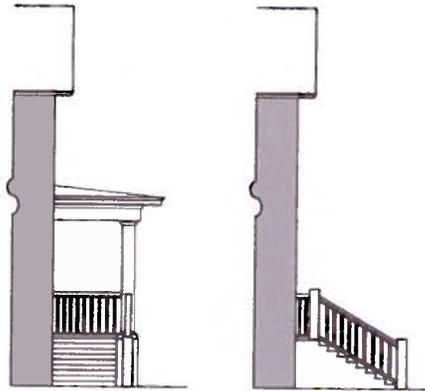
- Depth = 8 ft minimum from building face to inside column face.
- Length = 25% **minimum** ~~to 100%~~ of Building Front.
= 25% **minimum** ~~to 100%~~ of Building Side for wrap-around porches.
- ~~Height = 96" maximum.~~
- Overhang = 2 ft minimum.

Front Porches may be multi-story, with verandas and/or balconies above.

Front Porches may occur forward of the Build-to Line or Zone. Porches shall not extend into the right-of-way.

Front Porches may be screened; however, if screened, all architectural expression (columns, railings, pickets, etc.) must occur on the outside of the screen (facing the street or public space).

ii. Stoops



Depth = 4 ft minimum
Length = 10% to 25% of Building Front. (length does not include stairs)
Height = 30" minimum from grade to top of first finished floor. ~~96"~~
maximum.

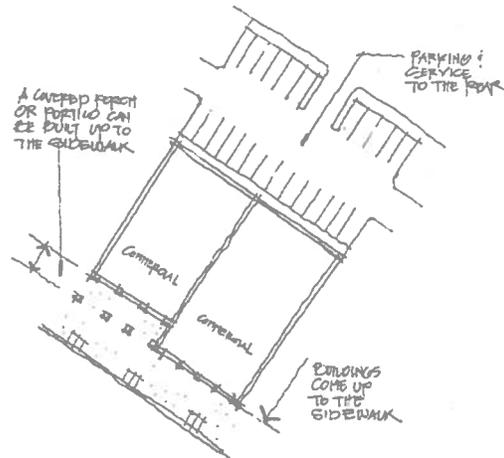
Stoops may occur forward of the principal façade, but shall not extend into the right-of-way. Stoop stairs may run to the front or to the side. Stoops may be shared by two adjacent units as long as both units meet the above dimensional requirements.

Sidewalks shall have a minimum 5' clear access for pedestrians. Stoops may be covered or uncovered.

6. Exceptions from Build-to Lines

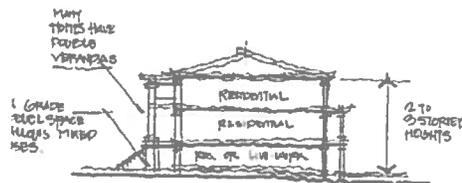
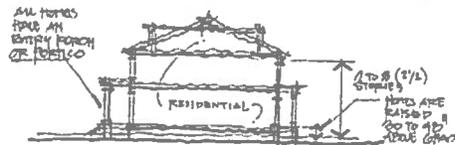
Exceptions from Build-to Lines may be granted by the City Architect for:

- a. avoiding trees with calipers greater than 8 inches.
- b. accommodating frontage elements on streets owned by SCDOT or encumbered by utility easements at the street frontage.



7. Finished Floor Heights/Elevation Above Grade

- a. Commercial structures or mixed-use structures with ground-floor commercial shall have a first finished floor 0-6" above sidewalk grade.
- b. Multifamily structures shall have a first finished floor height raised a minimum of 18" above average adjacent sidewalk grade, except on a House Street where the first finished floor height shall be the same as single-family residential structures (see subsection d below).
- c. Townhouses shall have the first finished floor height raised a minimum of 3' above average sidewalk grade.
- d. Single-family, two-family and three-family dwellings shall have a first finished floor height raised a minimum of 2' above average adjacent sidewalk grade. The first finished floor height can be elevated as much as 5' above average adjacent sidewalk grade without counting the undercroft as a story.



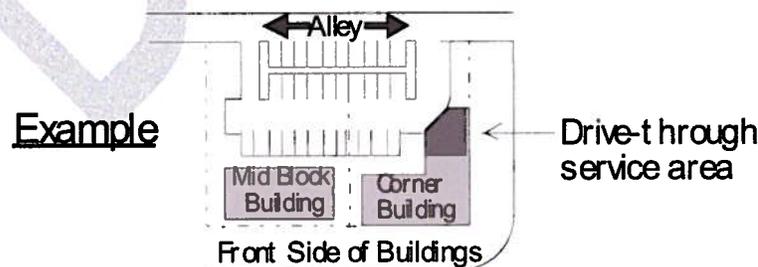
8. Accessory Structures

Accessory Structures for residential uses are permitted and may contain parking, accessory dwelling units, home occupation uses, storage space, and trash receptacles. Accessory structures ~~should~~ shall not be greater than 625 square feet in footprint and shall not exceed 2 stories in height.

9. Drive-thrus

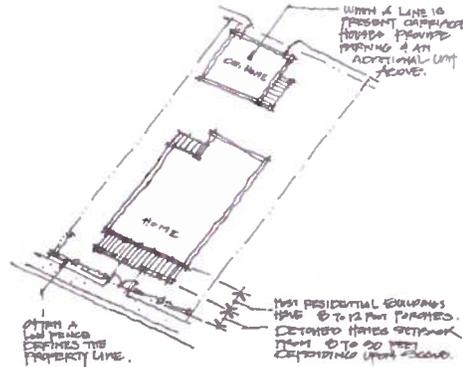
Drive-thrus, drive-thru windows, and drive-ups (collectively called "drive-thrus") are not permitted by-right within the Boundary Street Redevelopment District. Drive-thrus, except those at banks, may only be allowed when granted approval by the Zoning Board of Appeals as a special exception. Banks with drive-thrus may be approved by the City Architect under the conditions outlined below. In order for a drive-thru to be considered for approval by the Zoning Board of Appeals, it must conform to the following conditions:

- Drive-thru service windows must be located in the rear of properties, in mid-block and alley accessed locations;
- There shall be no minimum or maximum stacking requirements for vehicles, however drive-thru shall not circulate around, or be visible from, any street frontage. ~~the maximum stacking allowed for vehicles shall be three vehicle lengths;~~
- There is only one drive-thru window. Drive-thru window shall not be visible from the street;
- ~~There is no outside menu board or order board;~~
- Outside menu or order board may be a maximum of 24 SF in size with a maximum height of 8' above grade.
- The drive-thru window is not located on the façade of the building facing the primary street any street frontage.



10. Fences

Fences shall be a minimum of 25% opaque. Fences **should** shall be constructed of materials that continue the architecture of the building that it abuts.



11. Civic Sites

Civic buildings contain uses of special public importance. Civic buildings include, but are not limited to, municipal buildings, churches, libraries, schools, recreation facilities owned by public or nonprofit agencies, and places of assembly. Civic buildings do not include retail buildings, residential buildings, or privately owned office buildings. Civic buildings should be monumental and should help to enhance the public realm, rather than take away from it. The buildings should evoke a civic character and be carefully designed to reflect the architectural character of Beaufort. In order to provide greater flexibility to create a special architectural statement, civic buildings are not subject to Building Volume or Building Placement requirements.

The design of civic buildings shall be subject to review and approval by the City Architect.

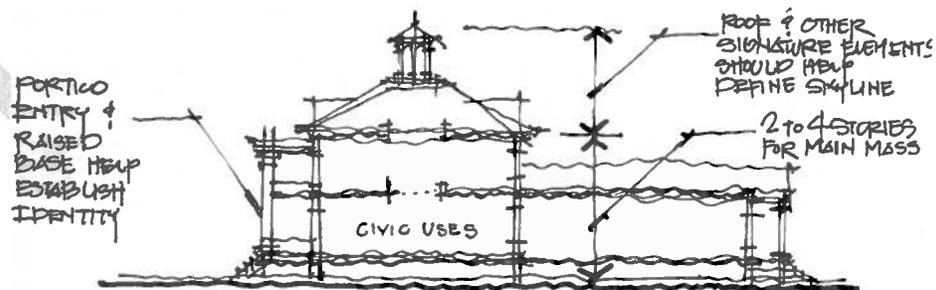
Civic buildings are reviewed on a case-by case basis. Although intended uses will be a significant determinant of form, there are several common design principles inherent to civic buildings. These principles affect their relationship to private buildings and to their setting as a whole.

The following design principles are for review of civic buildings:



Placement

- Civic buildings should be oriented toward the public realm (streets, squares and plazas) in a very deliberate way.
- Placement of buildings and primary architectural elements at the termination of public vistas can provide an appropriate level of visual importance.
- Building entrances should always take access from the most prominent façade(s). Avoid entrances that take access from the rear or are visually concealed.
- Placement of civic buildings, depending upon program and site, can often benefit from being set back from the adjacent build-to lines of private development. This allows the scale of the building to have more visual emphasis and can create a public space in the foreground. The amount of this setback should be carefully determined based on the urban design objectives of the particular site.



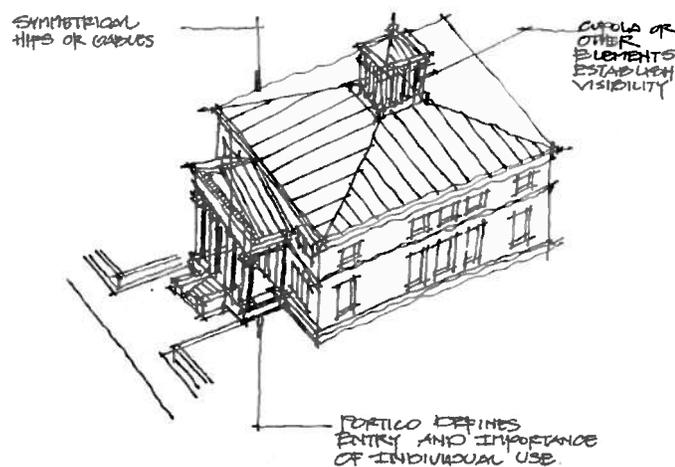
Massing

- The primary massing of civic buildings should be symmetrical in form. The appearance of a balanced design increases the level of formality which is appropriate to the public use.
- Massing of civic buildings, although often larger as a whole, should be divided into visually distinct sections. Massing divisions should provide visual

order to the building and create vertical proportions within individual elements.

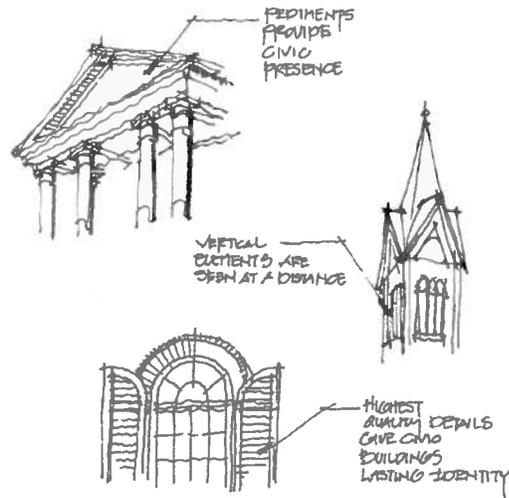
Scale/Height

- The scale of civic buildings should be larger than corresponding buildings in order to be more prominent and visible across greater distances.
- Floor-to-floor heights and architectural details should be proportionately larger than those of private buildings that exist or are anticipated within adjacent blocks.
- Prominent roof forms and additive elements such as cupolas can visually extend the height of the building.



Materials/Details

- It is of great importance that civic buildings be made of durable, high quality materials. The use of long-lasting materials is an expression of confidence in the future of the City.
- Civic buildings should be made of masonry, including brick, stone, and cast concrete. Stucco should be avoided as a material that lacks scale and texture. If used, stucco should be traditional, have integral pigment, and be scored to define human-scaled dimensions on the façade.
- Building details should be designed at two scales. At the larger scale, details should be robust to read from a distance. Closer to the building, the details of the lower levels should have another measure of refinement that can only be seen at the up-close, pedestrian scale.



12. Parking

The intent of these parking regulations is to encourage a balance between compact pedestrian oriented development and necessary car storage. The goal is to construct neither more nor less parking than is needed. The parking requirements and regulations are subject to review and adjustment by the City Architect.

a. Parking Requirements

For properties of one-half acre and less, there are no minimum parking count requirements in the Boundary Street Redevelopment District. For properties over one-half-acre, the minimum parking count requirement shall be 1 parking space for every 1000 square feet of leaseable or saleable building area. These parking spaces may be located either on-site, on-street (directly adjacent to a property), in shared parking scenarios, or in any public parking facility, the closest outside edge of which facility is located no more than 500 feet from the entry to the proposed building. Documentation for any leased or shared spaces must be provided. Credit shall be given for on-street parking spaces located within the public right-of-way that are directly in front of or adjacent to a property.

Minimum parking space dimensions for head-in or diagonal parking shall be 9 feet by 18 feet. Parallel parking spaces shall be 7 feet by 20 feet minimum. Drive aisles in parking lots shall be 22 feet wide for two-way circulation and to provide adequate backup space for 90 degree head-in parking. Diagonal parking and parallel parking spaces can be accessed with one-way 10 foot wide drive aisles.

Parking shall be provided as necessary to meet the requirements of the Americans with Disabilities Act.

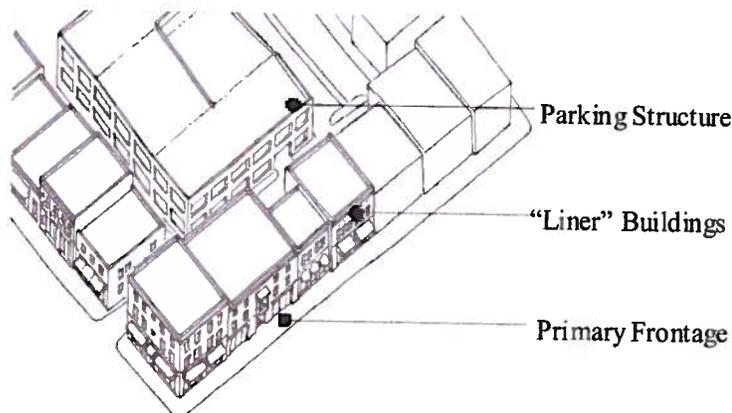
Maximum parking standards shall be governed by Section 7.5.A of the UDO.

b. Off-Street Surface Parking Lot Placement

Wherever possible, parking lots shall be located behind buildings, such that buildings separate parking areas from the street. In no case shall parking be located in front of a building. Off-street surface parking lots shall be set back a minimum of 10 feet from property lines along public rights-of-way, excluding alleys. Outbuildings serving as garages facing alleys shall be permitted within this setback. Parking beneath a building is permitted if the parking is screened from the street by the building. Off-street surface parking lots shall be screened from the street, from park space, and from cemeteries, with shrubbery, walls, fences, or some combination. These screening devices shall be a minimum of 3.5' in height and should have a minimum 50% opacity. If shrubs are used for screening, a minimum of 2/3 of the shrubs shall be evergreen. Shrubs shall be projected to reach their required height within 3 years of installation.

c. Structured Parking

Parking structures shall be set back from the property lines of all adjacent streets to reserve room for Liner Buildings between parking structures and the lot frontage. The Liner Building shall be, at a minimum, the same height as the parking structure and no less than 20 feet in depth. Liner Buildings may be detached from or attached to parking structures. Exceptions to these rules may be granted for lots less than 140 feet deep.

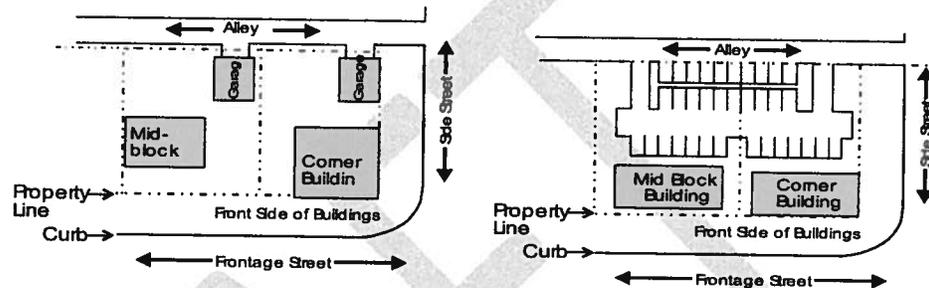


d. Access to Off-Street Parking

Alleys shall be the primary source of access to off-street parking. Parking along alleys may be head-in, diagonal or parallel.

Alleys may be incorporated into parking lots as standard drive aisles. Access to all properties adjacent to the alley shall be maintained. Access between parking lots across property lines is also encouraged.

Corner lots that have both rear and side access shall access parking through the rear (see diagrams below).



Garages should always be accessed from the alley and located in the rear of the lot.

If no alley exists, then efforts should be demonstrated attempting to get cross access across neighboring properties for rear parking.

Circular drives for civic buildings or hotels shall be permitted upon review and approval by the City Architect. Circular drives are prohibited for all other building types and uses.

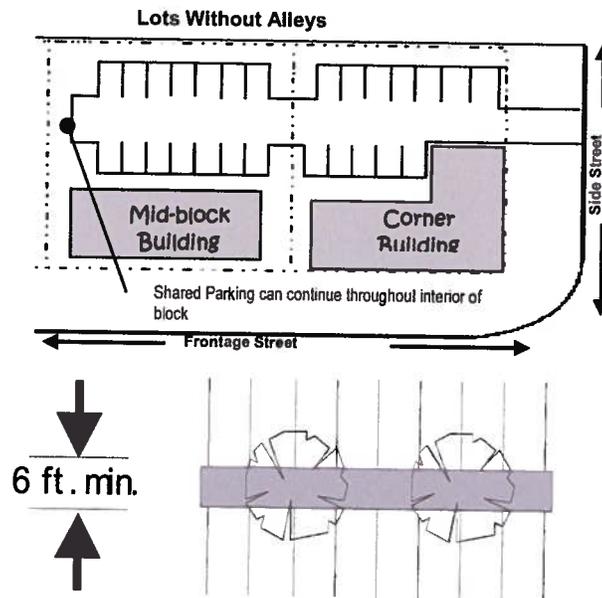
e. Garages where alleys are not present

If no alleys exist, then garage door(s) shall be positioned no closer to streets, squares or parks than 20 feet behind the principal plane of the building frontage. Garages facing streets, squares or parks are limited to one car width; and garage doors shall not exceed 10 feet in width. Where space permits, garage doors shall face the side or the rear, not the front.

Two-car garages are allowed where alleys are not present, so long as the garage is located in the rear of the lot. Garage doors shall not exceed 10 feet, and the driveway shall be a maximum of 10 feet wide in front of the principal plane of the building.

f. Parking Lot Landscaping Requirements

Landscape medians of at least six feet in width shall be provided between parking isles of either head-in or diagonal parking. Each landscape median shall have at least one tree for every 20 linear feet, or portion thereof, and be covered with grass, shrubs, or living ground cover. This spacing may be modified by the City Architect based on the proposed tree species. To minimize water consumption, the use of low-water vegetative ground cover other than turf is encouraged.



In lieu of landscape medians, landscape islands can be provided. No more than 8 consecutive parking stalls are permitted without a landscape island of at least 6 feet in width and extending the entire length of the parking stall. A minimum of one **overstory** tree shall be planted in each landscape island.

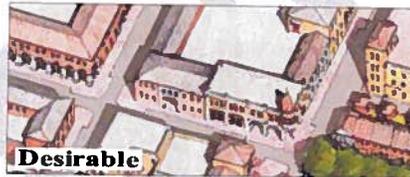
Medians and islands shall be protected by curbing, wheel stops, **or other appropriate means as determined by the City Architect.**

13. Large Footprint Buildings

Buildings with a footprint greater than 20,000 square feet may be built within the Boundary Street Redevelopment District by special exception only. Such

buildings must abide by all rules in this code with the following special limitations:

- e. Buildings may be one story in height and can only be on streets designated as "Boundary Street", "Robert Smalls Parkway", "Parallel Street" or "Main Street," but shall be at least 24 feet in height. This may be accomplished with Liner Buildings, mezzanines, or higher ceiling heights and/ or parapets.
- f. To encourage use by pedestrians and decrease the need for solely auto-oriented patronage, Large-Footprint Buildings must reinforce the urban character of Boundary Street and shall therefore front the buildings to the sidewalks, providing windows and doors at frequent intervals. Operable doorways should occur on an average of every 50 feet for the whole length of the street frontage.
- g. Building footprints shall not be larger than a single block, with a maximum dimension of 350'x350'. Floor area of buildings shall not cantilever over public rights-of-way.
- h. Loading docks, service areas and trash disposal facilities shall not face streets, parks, squares, waterways, or significant pedestrian spaces.



Large Footprint Buildings are wrapped in a liner of smaller buildings with doors and windows



Large Footprint Building has blank facades and sits behind a field of parking.

13. Opacity & Facades

Opacity requirements shall meet the parameters described in *Architectural Standards*, Section 6.8.C.13.2

14. Accessibility

All buildings and streetscapes will be designed in compliance with the Americans with Disabilities Act. Refer to ADA Standards for Accessible Design, issued by the Department of Justice on July 1, 1994.

I. Building Elements

~~General Requirements~~

~~Article 7. Door & Window Openings~~

C. Architectural Standards

This section specifies building materials, details and configurations. Building designs which strictly comply with these standards are to be considered approved for matters of aesthetics and shall not require further discretionary review for architectural character or appearance. Building designs which do not comply with these standards may be permitted, but only after review and approval by the City Architect.

The lists of permitted materials and configurations come from study of traditional buildings found in the Lowcountry and have been selected for their appropriateness to the visual environment and climate.

A primary goal of the Architectural Standards is authenticity. The Standards encourage construction which is straightforward and functional, and which draws its ornament and variety from the traditional assembly of genuine materials. The City Architect shall have authority to approve substitute materials for those listed as options under the Architectural Standards. As an additional reference for architectural standards, refer to Traditional Construction Patterns by Stephen Mouzon, McGraw Hill, 2004.

General Requirements:

The following shall be located in rear yards or sideyards not facing side streets:

- Window and Wall Air Conditioners;
- Air Conditioning Compressors;
- Irrigation and pool pumps; and
- Electrical Utility Meters.
- Satellite dish antennas greater than 18" in diameter (satellite dishes must be shielded from view from the street or public space)

The following shall be located in rear yards only:

- Antennas;
- Permanent Barbecues; and
- Refuse enclosures.

The following are prohibited:

- Undersized shutters (the shutter or shutters must be sized so as to equal the width that would be required to cover the window opening.)
- Plastic shutters;
- Clotheslines;
- Clothes Drying Yards;
- Reflective and/or bronze-tint glass;
- Plastic or PVC roof tiles;
- Backlit awnings;

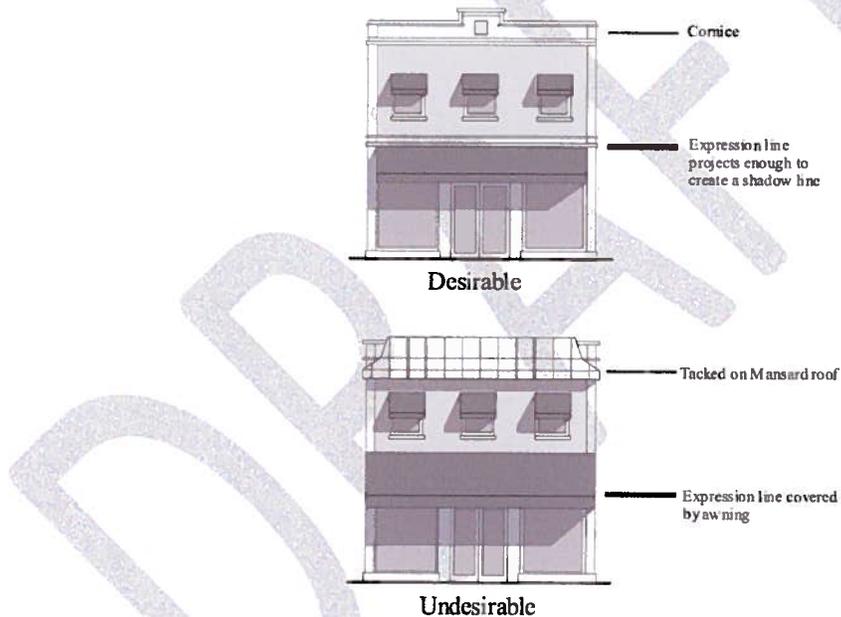
- Glossy-finish awnings; and
- Fences made of chain link, barbed wire, or plain wire mesh.

1. Building Walls

a. General Requirements

Required for all buildings except attached and detached single family houses:

An expression line **should** ~~shall~~ delineate the division between the first story and the second story. A cornice shall delineate the tops of the facades. Expression lines and cornices shall either be a molding extending a minimum of 2 inches, or a jog in the surface plane of the building wall greater than 2 inches.



b. Permitted Finish Materials

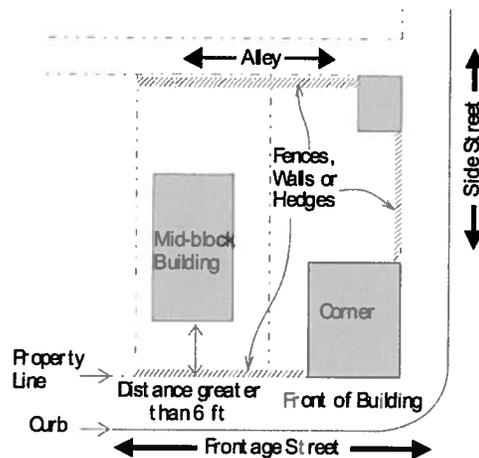
- *Concrete masonry units with stucco (C.B.S.)
- *Reinforced concrete with stucco
- *Fiber cement board such as "Hardie-Plank" siding (50-year siding product)
- *Wood (termite resistant, 50-year siding product): painted or natural
- *Brick
- *Tabby
- *Other materials may be approved by the City Architect.
- *Wherever possible, Green building materials are encouraged in the construction of building walls, including recycled-content sheathing, siding

composed of reclaimed or recycled material, and salvaged masonry brick or block.

2. Garden Walls & Fences

a. General Requirements

Fences, garden walls, or hedges are strongly encouraged and, if built, should be constructed along all un-built rights-of-way which abut streets and alleys as shown in the diagram below. Fences, garden walls, or hedges are encouraged along side yards and rear yards. Fences, garden walls and hedges shall be minimum 25% opaque.



Height of garden walls:

Front Yard: (in front of the primary structure) maximum height of 48 inches. Pillars and posts may extend up to 6 inches more, to a height of 54 inches.

Side and Rear Yards: (behind the principal façade of the primary structure) maximum height of 72 inches. Pillars and posts may extend up to 6 inches more, to a height of 78 inches.

b. Permitted Finish Materials

- *Wood (termite resistant), painted or stained; unpainted wood must be sealed
- *Concrete Masonry Units with Stucco (C.B.S.)
- *Reinforced Concrete with Stucco
- *Wrought Iron
- *Brick
- *Aluminum

c. Permitted Configurations

*Wood:

Picket Fences: minimum 25% opacity, w/ corner posts

Other: to match building walls

*Wrought Iron: Vertical, 5/8" minimum dimension, 4" to 6" spacing

*Brick

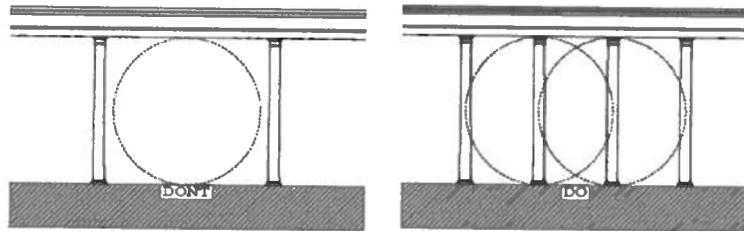
*Stucco: with texture and color to match building walls

3. Columns, Arches, Piers, Railings & Balustrades

a. General Requirements

(1) Column and Pier spacing:

Columns and Piers shall be spaced no farther apart than they are tall.



Generally column bays should be equal and of precise proportions.

b. Permitted Finish Materials

(1) Columns:

Wood structure with finished wood or Hardie-plank cladding

Cast Iron

Concrete with smooth finish

Brick

(2) Arches:

Wood structure with finished wood or Hardie-plank cladding

Concrete Masonry Units with Stucco (C.B.S.)

Reinforced Concrete with Stucco

Brick

(3) Piers:

Wood structure with finished wood or Hardie-plank cladding

Concrete Masonry Units with Stucco (C.B.S.)

Reinforced Concrete with Stucco
Concrete with smooth finish
Cast iron
Brick
Tabby

- (4) Railings & Balustrades:
Wood (termite resistant), painted or natural
Wrought Iron

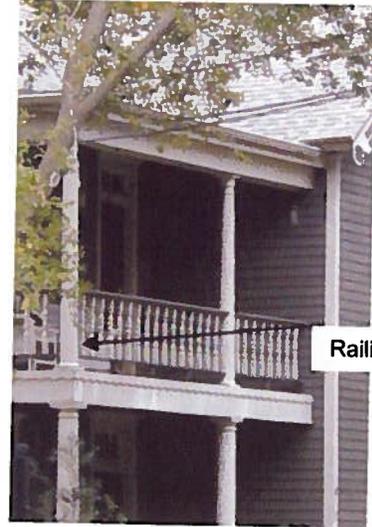
c. Permitted Configurations

- (1) Columns:
Square, 6" minimum, with or without capitals and bases
Round, 6" minimum outer diameter, with or without capitals and bases
Classical Orders (For classical column proportions refer to *American Vignola: A Guide to the Making of Classical Architecture by William R. Ware, W.W. Norton & Company, New York, 1977.*)

- (2) Arches:
Semi-circular & Segmental

- (3) Piers:
8" minimum dimension

- (4) Porches:
Railings 2-3/4" minimum diameter
Balustrades 4" minimum spacing, 6" maximum spacing.
(All dimensions shall also conform to local building codes.)



Railing & Balusters

4. Roofs & Gutters

a. General Requirements

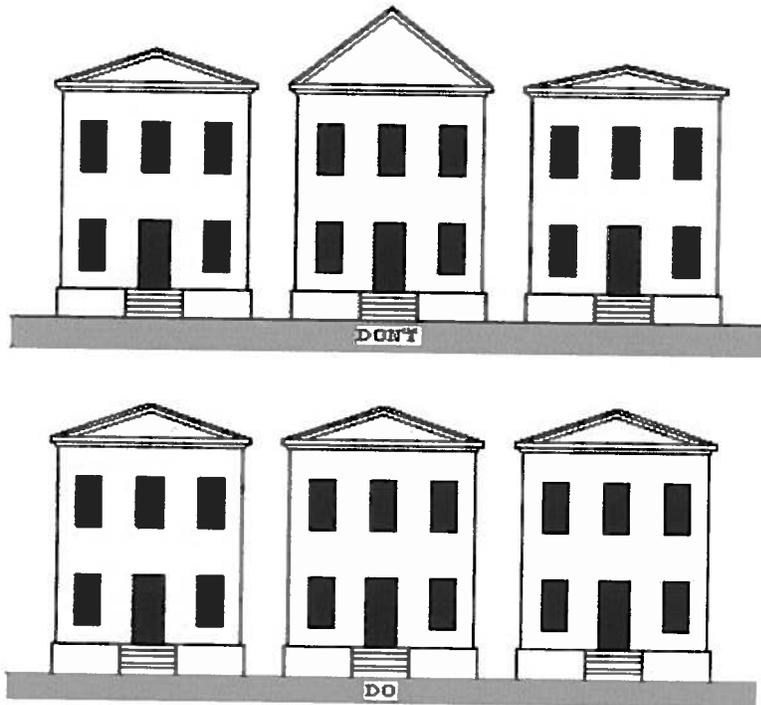
- (1) Permitted Roof Types:
Gabled, hipped, shed, barrel vaulted, flat, mono-pitch, mansard and domed. Shed, flat, and mono-pitch roofs shall be concealed with parapets along the street frontage. Applied mansard roofs are not permitted.
- (2) Exposed rafter ends (or tabs) at overhangs are strongly recommended.
- (3) Downspouts are to match gutters in material and finish.

b. Permitted Finish Materials

- (1) Metal:
 - Galvanized
 - Copper
 - Aluminum
 - Zinc-Alum
- (2) Shingles:
 - Fiberglass or Metal, "dimensional" type
 - Slate
 - Composite slate
 - Cedar shake
 - Asphalt
- (3) Tile:
 - Other options preferred; permitted only if approved by the City Architect.
- (4) Membrane or built-up:
 - For flat and mono-pitched roofs only
- (5) Gutters:
 - Copper
 - Aluminum
 - Galvanized Steel

c. Permitted Configurations

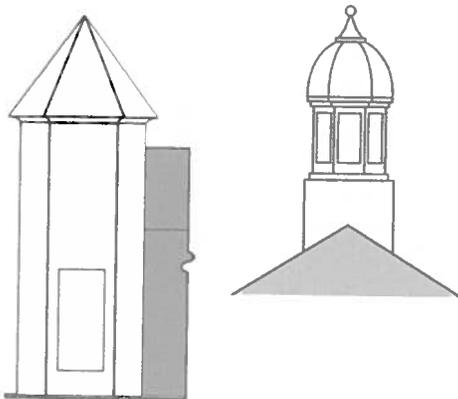
- (1) Metal:
 - Standing Seam or "Five-vee," 24" maximum spacing, panel ends exposed at overhang
- (2) Shingles:
 - Square
 - Rectangular
 - Fishscale
 - Diamond
 - Shield
- (3) Gutters:
 - Rectangular section
 - Square section
 - Half-round section



In regards to roof slope, do not vary the slopes drastically within the same style. Instead base slopes on local syntax. As a generalization 12:12 is appropriate for primary roofs, while ancillary roof slopes may be 4:12. For larger buildings that have monopitch or shed roofs, as a generalization 4:12 is appropriate.

5. Turrets & Cupolas

If a building has a turret or cupola, the following regulations shall apply.



Plan = 20' x 20' Maximum Footprint Area
 Height = If footprint is larger than 10' x 10', then the cupola / turret may extend a maximum of 25' above the roofline of the highest story. If footprint is less than 10' x 10', then the turret or cupola may extend to a maximum of 50' above the roofline of the highest story.

6. Windows, Skylights, & Doors*

a. Location & Transparency

- (1) The primary entrance to all buildings shall be located on the exterior wall facing the frontage street.
- (2) For commercial buildings or floors, each facade facing streets shall contain a minimum of 40% of transparent materials on each story below the roof line.
- (3) For residential buildings or floors, each facade facing streets shall contain 15% to 75% of transparent materials on each story below the roof line.

~~Each facade facing streets shall contain 15% to 70% of transparent materials on each story below the roof line.~~

b. General Requirements

- (1) ~~Window and door openings shall be taller than they are wide.~~ Windows shall be rectangular, square, circular, semi-circular, or octagonal. Rectangular window openings facing streets shall be oriented vertically.
 - (2) Window openings may be grouped horizontally.
 - (3) Masonry Construction: A header and sill are required for all windows.
 - (4) Wood Construction: Windows are required to have trim on all four sides.
 - (5) The following accessories are permitted: Shutters, Window Boxes, Mullions, Fabric or Metal Awnings (no backlighting; no glossy-finish fabrics)
- * The requirements for doors apply to the primary entrance to all buildings which shall be located on the exterior wall facing the frontage street. The requirements do not apply to parking garage doors or loading dock doors because they are required to be located in the rear of the buildings and are to be accessed by alleys.

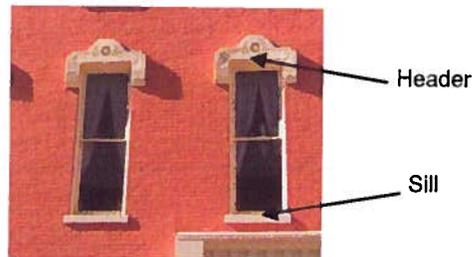
b. Finish Materials

(1) Windows and Skylights:

Wood
Aluminum
Copper
Steel
Clad Wood

(2) Doors:

Wood or Metal*
*In storefront



locations, glass doors are allowed.

(3) Whenever possible, Green building materials shall be used for windows & doors, including wood/composite windows, finger-jointed wood windows, and reconstituted or recycled-content doors.

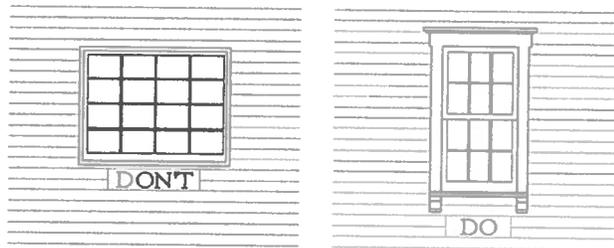
c. Permitted Configurations

(1) Windows:

Rectangular

Square

Round (18" maximum outer diameter)



Horizontal windows do not reflect human proportions. Use vertical windows that respond to the human body. Use vertical or square window panes and restrict them to a few related proportions, such as 1:1.6 or 1:2.

(2) Window Operations:

Casement

Single and Double-Hung

Industrial

Fixed Frame (36 square feet maximum)

(3) Skylights:

Flat to the pitch of the roof

(4) Door Operations:

Casement

French

Sliding (upper floors and rear only)

7. ~~Opacity & Facades~~

~~Each floor of any building facade facing a park, square, or street shall contain transparent windows covering from 15% to 70% of the wall area. Tinted~~

~~glass and reflective glass may be permitted with the approval of the City Architect.~~

8. Signs

a. General Requirements

- (1) All signs shall be subject to review by the City Architect in order that signs are consistent and in harmony with the Boundary Street Master Plan. The City Architect shall use graphics in this section as non-binding guidelines, but shall make a determination of appropriateness on a case by case basis.
- (2) Signs shall be flat against the facade, mounted projecting from the facade, or mounted above the top of the facade.
- (3) Signs can be hung underneath an arcade, perpendicular to the front wall, but only for the purpose of being seen within the arcade.
- (4) Signs shall be externally lit. Individual letters and symbols may be internally lit or back-lit.

b. Finish Materials

- *Wood: painted or natural
- *Metal: copper, brass, galvanized steel
- *Painted Canvas
- *Paint/engraved directly on facade surface
- *Plastic (channel letters only)

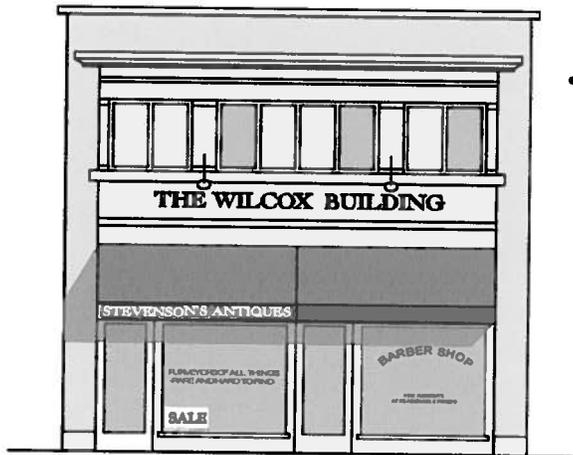
c. Configurations

- (1) Maximum gross area of signs on a given facade shall not exceed 10% of the applicant's facade area.
- (2) Maximum area of any single sign mounted perpendicular to a given facade shall not exceed 10 square feet.
- (3) Signs shall maintain a minimum clear height above sidewalks of 8 feet.

(4) Freestanding monument signs are not permitted.

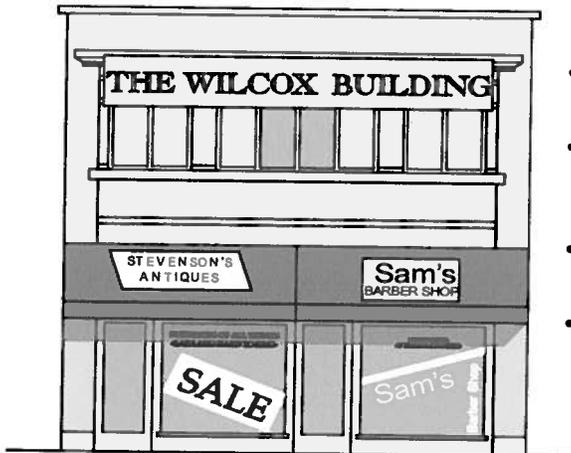
d. Nonconforming Signs

(1) Signs not conforming to the requirements of this section shall be altered, removed, or otherwise brought into compliance with the requirements of this section when improvements to existing buildings exceed 50% of the present building value.



Desirable

- Signs are coordinated in size and placement with the building and storefront



Undesirable

- Building sign conceals the cornice
- Over-varied sign shapes create visual confusion
- Awning sign covers the masonry piers
- Sale sign too large for storefront and poorly placed in display window

Example of Signs Flat Against a Façade:



The sign is centered within the symmetrical arrangement of the window above and shopfront below

Internally lit letters

Internally lit letters

The sign runs horizontally along the expression line



Lamps for external lighting

The sign is centered above the main entrance at the top of the facade

Lamps for external lighting
The top edge of the facade is sculpted to create a special focal spot for the sign



Elegant and reserved cast bronze address plate located at pedestrian eye level

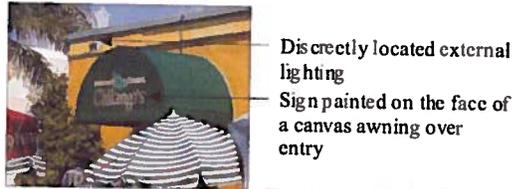
Sign painted directly on the facade above the main entrance

External lighting discreetly located above the awning



Internally lit plastic signs are designed for the "strip", not a pedestrian oriented town center

Example of Signs Mounted Projecting from the Façade:



Discreetly located external lighting
 Sign painted on the face of a canvas awning over entry

Neon signs can provide a warm glow, enhancing night time pedestrian activity



Photograph by Bennett Coleman

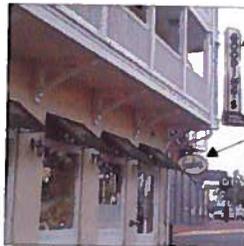


Signs on the sides of awnings are directly in the line of sight of pedestrian customers

Vertical projecting signs are highly visible far down the street



A lower marquee sign caters to people on foot and in cars passing directly in front of the venue



A sign extending from the corner of a building is highly visible along two streets

A second lower sign catches the eye of pedestrians passing in front of the entrance

Signs hanging from the ceilings of arcades command the attention of pedestrian shoppers



Monument signs fit within the deep setbacks of suburban strip development to direct motorists to stores set too far back

Examples of Signs Mounted Above the Top of the Façade:



Signs projecting from the tops of buildings are highly visible from a great distance

This distinctive sign, made of individual letters projecting from the front of the facade and extending above the cornice line, is memorable to shoppers and is highly visible from many directions



A second lower sign marks the entrance to the store



Projecting signs which break the skyline are visible from a variety of distances and serve as beacons to customers when lit at night

Signs projecting above the roof stand out against the sky, adding an architectural flair to a shop's identity



Pole mounted signs are designed to fit in deep suburban setbacks and are not appropriate for pedestrian-oriented environments

Billboards cater entirely to motorists traveling at high speeds



APPENDIX

Civic Art, by Hegemann and Peets;

Great Streets, by Allan B. Jacobs;

The Charter of the New Urbanism, by Congress for the New Urbanism;

AIA Graphic Standards, 9th Edition;

Traditional Construction Patterns, by Stephen A. Mouzon;

The Lexicon of the New Urbanism, by Duany et al, Congress for the New Urbanism;

Shared Parking, by Barton-Aschman Associates, The Urban Land Institute;

The American Vignola: A Guide to the Making of Classical Architecture, by William R. Ware.

City of Beaufort Department of Planning and Development Services

M E M O R A N D U M

TO: Beaufort--Port Royal Metropolitan Planning Commission
FROM: Libby Anderson, Planning Director
DATE: September 9, 2014
SUBJECT: Minimum Finished First Floor Height for New Residential Construction

New residential construction is currently required to be elevated in the Bladen and Boundary Street Design Districts. Townhomes in any district are required to be elevated above grade. All residential development in the flood zone is required to be elevated above the Base Flood Elevation. Residential construction in the Historic District is also usually required to be raised.

There are several reasons to require all new residential construction to be elevated above the existing grade, including prevention of flooding. During the recent rains, there were numerous reports of flooding or near flooding of dwellings build at grade. Staff is proposing to require the minimum first floor of new residential construction to be raised 18" above grade. This is consistent with the requirements in the new Port Royal code and the proposed Beaufort County code. Staff is recommending that Section 6.5.K, "Measurement and Exceptions; Height," be amended by adding a new paragraph 1 to read as follows:

1. First Floor Heights Above Grade

The finished first floor of new residential construction must be elevated a minimum of 18" above finished grade adjacent to the building exterior.

City of Beaufort Department of Planning and Development Services

MEMORANDUM

TO: Beaufort--Port Royal Metropolitan Planning Commission

FROM: Libby Anderson, Planning Director

DATE: September 11, 2014

SUBJECT: Requiring Silt Fencing on Construction Sites--**REVISED**

Currently, there is no specific requirement in the Unified Development Ordinance for silt fencing on construction sites. Large construction projects are required by state regulations to have silt fencing, but small projects, and development on existing platted lots, are exempt from state regulations. At the same time, even small construction projects can result in sediment washing into the street (or onto adjacent lots) and entering nearby water bodies and/or clogging storm drains. Staff is proposing to add a requirement for the installation of silt fencing prior to starting site work. To this end, staff is recommending that Section 7, "General Development Standards," be amended by adding a new subsection 8 to read as follows:

7.8 Silt Fencing

Silt fencing shall be required around all new construction sites and on other construction sites where there will be significant soil disturbance due to construction activities. Silt fencing shall consist of geotextile fabric stretched across steel or wood posts to be used as a temporary perimeter control. Fencing shall be placed along front, side and rear property lines as required by distance and gradient prior to any site disturbance. On existing single-family lots, the Administrator may waive the silt fencing requirement on some or all sides of the lot based on distance to the property line and/or gradient.

Fabric to be cut to a minimum width of 36 inches with 12 inches of fabric placed into a 6-inch minimum depth trench. Minimum post spacing to be 8 foot on center. Bottom of fabric to be securely held in place by backfilling trench with soil and compacting. Hay bales or equal to be substituted for geotextile fabric in tree root protection zones to avoid cutting of roots. In tidal areas, extra silt fence height may be required.

A construction entrance shall be established with gravel or stone to prevent tracking of soil from construction vehicles onto paved street.

Any fabric that is torn, decomposed, or in any way becomes ineffective shall be replaced immediately. Sediment that accumulates to 1/3 of the height of the fence shall be removed. Silt fencing shall be removed within 30 days after final stabilization is achieved.

City of Beaufort Department of Planning and Development Services

M E M O R A N D U M

TO: Beaufort–Port Royal Metropolitan Planning Commission
FROM: Libby Anderson, City of Beaufort Planning Director 525-7012
DATE: September 9, 2014
SUBJECT: Status Report on City Council Actions

UDO Amendment Revising Landscaping and Tree Conservation Ordinance. A public hearing was held at August 26 City Council meeting. First reading of the ordinance amendment is scheduled for the September 9 City Council meeting.

UDO Amendment Pertaining to Gas Station Signage. A public hearing was held at the August 26 City Council meeting. First reading of the ordinance amendment is scheduled for the September 9 City Council meeting.

UDO Amendment Establishing a Side Yard Setback for Attached Garages. A public hearing is scheduled for the September 9 City Council meeting.

Please contact me with any questions on this information.

Thank you.



COUNTY COUNCIL OF BEAUFORT COUNTY
BEAUFORT COUNTY PLANNING DIVISION
Multi-Government Center • 100 Ribaut Road, Room 115
Post Office Drawer 1228, Beaufort SC 29901-1228
Phone: (843) 255-2140 • FAX: (843) 255-9432

TO: Beaufort-Port Royal Metropolitan Planning Commission
FROM: Anthony Criscitello, Beaufort County Planning Director
DATE: September 15, 2014
SUBJECT: The Village at Oyster Bluff PUD, Lady's Island

STAFF REPORT:

A. BACKGROUND:

Case No. ZMA-2014-07
Owner: Bennett McNeal
Applicant: McNeal Land Company
Property Location: On Lady's Island on the west side of Sams Point Road directly north of the intersection with Oyster Factory Road.
District/Map/Parcel: R200-015-000-0051; R200-015-000-0724-0000; R200-015-000-0725-0000; R200-015-000-051A-0000
Property Size: 39.03 acres
Current Future Land Use Designations: Neighborhood Mixed-Use
Current Zoning Districts: Lady's Island Expanded Home Business – 16.71 acres; Lady's Island Community Preservation – 22.32 acres
Proposed Zoning Districts: Planned Unit Development (PUD)

B. SUMMARY OF REQUEST: The Village at Oyster Bluff is a proposed mixed-use PUD with 114 dwelling units and 5.5 acres of commercial. The total acreage of the site is 39.03 acres giving the PUD a gross density of 2.92 dwelling units per acre. The site is an undeveloped series of parcels located between Sam's Point Road and Sunset Boulevard on Lady's Island directly north of Oyster Factory Road. The site is located approximately 0.8 miles north of the intersection of Sea Island Parkway and Sam's Point Road.

The site was originally The Village at Lady's Island PUD which County Council approved in 1996. The Village PUD had a total of 200 dwelling units and a gross density of 5.1 dwelling units per acre along with 12 commercial lots. The PUD was sunsetted under the provisions of Section 106-7(2) which gave a January 1, 2010 deadline for PUDs approved prior to 1999 where less than 50% of the lots have been platted. County Council did not approve an extension of the Village PUD because 0% of the PUD had been platted and the residential density was not consistent with the surrounding existing development and zoning districts. The property was then rezoned Lady's Island Community Preservation (LICP) and Lady's Island Expanded Home Business (LIEHB) (see attached map).

Since that time, the property owner has been working with the Lady's Island Community Preservation Committee to chart a path forward. Originally, the property owner explored using the Planned Community provision available in the LICP district. The applicant found that the 2.6 dwelling units per acre density and 40% open space requirement rendered the site very difficult to effectively develop. Recognizing the limitations of the Planned Community provision, planning staff worked with the Community Preservation Committee to allow Traditional Community Plans on sites located within two

miles from the intersection of Sea Island Parkway (US 21) and Sam's Point Road. This provision is incorporated into the draft Community Development Code that is being considered for adoption by County Council. The applicant chose to pursue a PUD to expedite the project and to make modifications to the Traditional Community Plan provisions that will be found in the new code.

C. **ANALYSIS:** Section 106-492 of the ZDSO states that a zoning map amendment may be approved if the weight of the findings describe and prove:

1. *The change is consistent with the County's Comprehensive Plan and the purposes of this Ordinance.*

The Northern Beaufort County Regional Plan and the 2010 Beaufort County Comprehensive Plan designate the area surrounding the proposed PUD as Neighborhood / Mixed-Use. This designation envisions a low to moderate residential density with new development encouraged to be pedestrian-friendly, have a mix of housing types, a mix of land uses and interconnected streets. Mixed-use developments are encouraged to promote pedestrian access to services and provide internal trip capture. The City of Beaufort's Comprehensive Plan's Framework Plan has this site designated as a G2 Controlled Growth Sector which promotes traditional neighborhood developments and urban neighborhoods. Also, the draft Beaufort County Community Development Code has part of this site designated as a "hamlet" place type which calls for a mix of low to moderate density housing and small scale commercial node with T2, T3 and T4 transect zones. The overall scale, density, mix of land uses, and transportation network of The Village at Oyster Bluff are consistent with the goals of these plans.

2. *The change is consistent with the character of the neighborhood.*

The proposed PUD has greater residential density than the surrounding residential areas, but is consistent with other nearby traditional neighborhood developments. The Village lies in the midst of mature, low-density residential neighborhoods on Sunset Bluff, Wallace and Oyster Factory Roads. However, there are two traditional neighborhood developments within one-third mile of the Oyster Bluff PUD: Newpoint, which is a PUD, and Celedon, which was approved by right under the planned community provisions of the LICP district. Both developments include a mix of residential and commercial development and are stylistically similar to what is proposed at The Village at Oyster Bluff. Newpoint was built at a density of 2.5 units per acre, and Celedon is approved and is building at a density of 2.6 units per acre.

3. *The extent to which the property is consistent with the zoning and use of nearby properties.*

See response to item # 2.

4. *The suitability of the property for the uses to which it has been restricted.*

The property is wooded and may contain a freshwater wetland. It appears that the property is suitable for residential and small-scale commercial development.

5. *Allowable uses in the proposed district would not adversely affect nearby property.*

See response to item # 2.

6. *The length of time a property has remained vacant as zoned, where the zoning is different from nearby developed properties.*

The property is undeveloped.

D. TRAFFIC IMPACTS: A traffic impact analysis (TIA) was submitted as part of the PUD submission. A full analysis of the TIA will be made by the Beaufort County Traffic Engineer when this project is reviewed by the Beaufort County Planning Commission.

E. PUD DEVELOPMENT STANDARDS: The PUD document states that that The Village at Oyster Bluff PUD draws from the development standards of the draft Beaufort County Community Development Code with modifications. Below is a summary of major modifications to the draft code and other concerns about the proposed PUD standards:

1. The Village at Oyster Bluff Modified T3 Neighborhood (T3N) Standards:
 - The modified district does not refer to any of the building types in Article 5, Division 5.1 of the draft code, but refers to a building type called “Oyster Bluff Single Family” which is not included in the PUD document.
 - The modified district reduces the minimum 18” ground floor finish level to 6” therefore facilitating slab on grade houses.
 - Off-street parking, which is intended in the draft code to be at the side or rear of the property, is permitted to be at the front lot line.
2. The Village at Oyster Bluff Modified T4 Hamlet Center (T3HC) Standards:
 - The modified district does not refer to any of the building types in Article 5, Division 5.1 of the draft code, but refers to a building type called “Oyster Bluff Vernacular” which is only included in the PUD document as a concept sketch.
 - The modified district effectively eliminates all setbacks and build-to lines.
 - The modified district reduces the minimum 18” ground floor finish level to 6” for residential therefore facilitating slab on grade houses.
 - The modified district places no restrictions on the location of parking, allowing conventional auto-oriented development that is not consistent with the intent of the T4 Hamlet Center District.
 - The modified district allows commercial buildings up to 10,000 square feet, rather than the maximum 3,500 square feet in the draft T4 Hamlet Center district. The use table is also confusing because it allows for retail “10,000 square feet or greater” which should read “10,000 square feet or less.”
3. In addition to the modified districts, it is not clear which additional standards will apply to development within the draft PUD. The document specifically states that the minimum tree requirements of Division 5.11 of the draft Community Development Code will apply to this PUD. However, it is not clear what standards for parking, lighting, landscaping, signage, architecture, streets, etc. apply to this PUD.
4. There is no master plan for the commercial portion of the site. A concept plan is provided in Exhibit L of the PUD document, but it states that it is not part of the master plan.

F. STAFF RECOMMENDATION:

The overall scale, density, mix of land uses, transportation network and civic space layout of The Village at Oyster Bluff are consistent with the goals of the comprehensive plans of Beaufort County and the City of Beaufort. The proposed street network integrates well into the existing street network while minimizing adverse impacts on the Sunset Boulevard neighborhood. However, the standards in the PUD document are not adequate to ensure that the development is consistent with the goals of the comprehensive plans or with the traditional community plan provision in the draft Beaufort County Community Development Code.

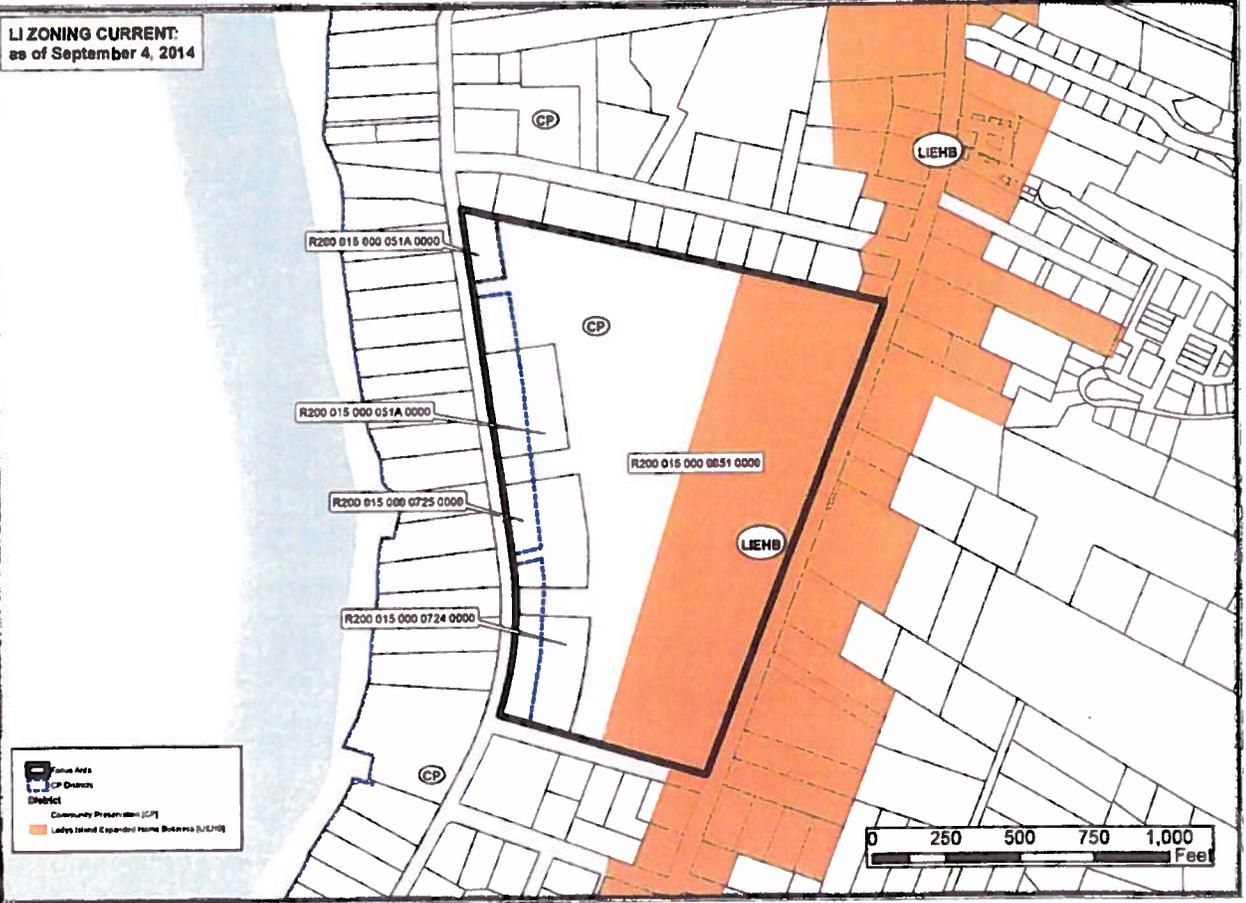
Therefore, planning staff cannot support the adoption of The Village at Oyster Bluff unless the following revisions are made to the PUD document:

1. Modify the Oyster Bluff T3 Neighborhood (T3N) Standards: Within the Oyster Bluff T3 Neighborhood district, more standards need to be provided to ensure that the frontage of the houses are not dominated by garages and driveways, which do not address the street and sidewalk and compromise the safety and integrity of the pedestrian environment. The standards in the PUD document allow over 40% of the front property line to be dominated by parking for the proposed 55 foot wide lots. Staff recommends at a minimum, that the applicant adopt the standards of Article XI, Division 3, Section 106-2379(e)(4) to regulate front loaded garages for lots over 50 feet in width (see Attachment C). Staff also recommends that the applicant draft the Oyster Bluff Single Family building type standards as part of the PUD document that is consistent with the Village House building type in Article 5, Division 5.1 of the draft Community Development Code.
2. Modify the Oyster Bluff T4 Hamlet Center (T4HC) Standards: Within the Oyster Bluff T4 Hamlet Center District, a build-to line is essential to facilitating the type of development that is pedestrian friendly and address the street. In addition, parking needs to be restricted to the side or rear of commercial buildings. This type of development is shown on Exhibit L with the buildings fronting the proposed street that connects from Sam's Point Road to Sunset Boulevard, and on the proposed street that parallels Sam's Point Road. However, the standards in the proposed district do not preclude a development scenario that is strictly auto-oriented and contrary to the concept plan in Exhibit L. Also, the use table needs to be modified for Retail and Office development to set a maximum square footage rather than a minimum square footage.
3. Provide a Master Plan for the Commercial Portion of the PUD: Exhibit L of the PUD needs to be incorporated into the master plan provided in Exhibit A.
4. Specify other Supplemental Standards: The PUD document is not clear on which supplemental standards such as parking, lighting, landscaping, resource protection, signage, architecture, streets, etc., apply to development within the PUD. Staff recommends that the applicable standards of the draft Community Development Code be incorporated into the PUD.
5. Provide a Buffer Along Sunset Boulevard: The applicant should consider incorporating a buffer that is in common ownership to minimize the visual impacts on Sunset Boulevard.

G. ATTACHMENTS

- Attachment A: Map: Existing and Proposed Zoning ([backup](#))
- Attachment B: The Village at Oyster Bluff Master Plan ([backup](#))
- Attachment C: Recommended Standards: Front Loaded Vehicular Garage Access Oyster Bluff T3 Neighborhood ([backup](#))
- Attachment D: Application and Applicant's submittal ([backup](#))
- Attachment E: Notification Letter and List of Abutting Property Owners ([backup](#))

LI ZONING CURRENT:
as of September 4, 2014



LI ZONING PROPOSED CHANGE:
as of September 4, 2014

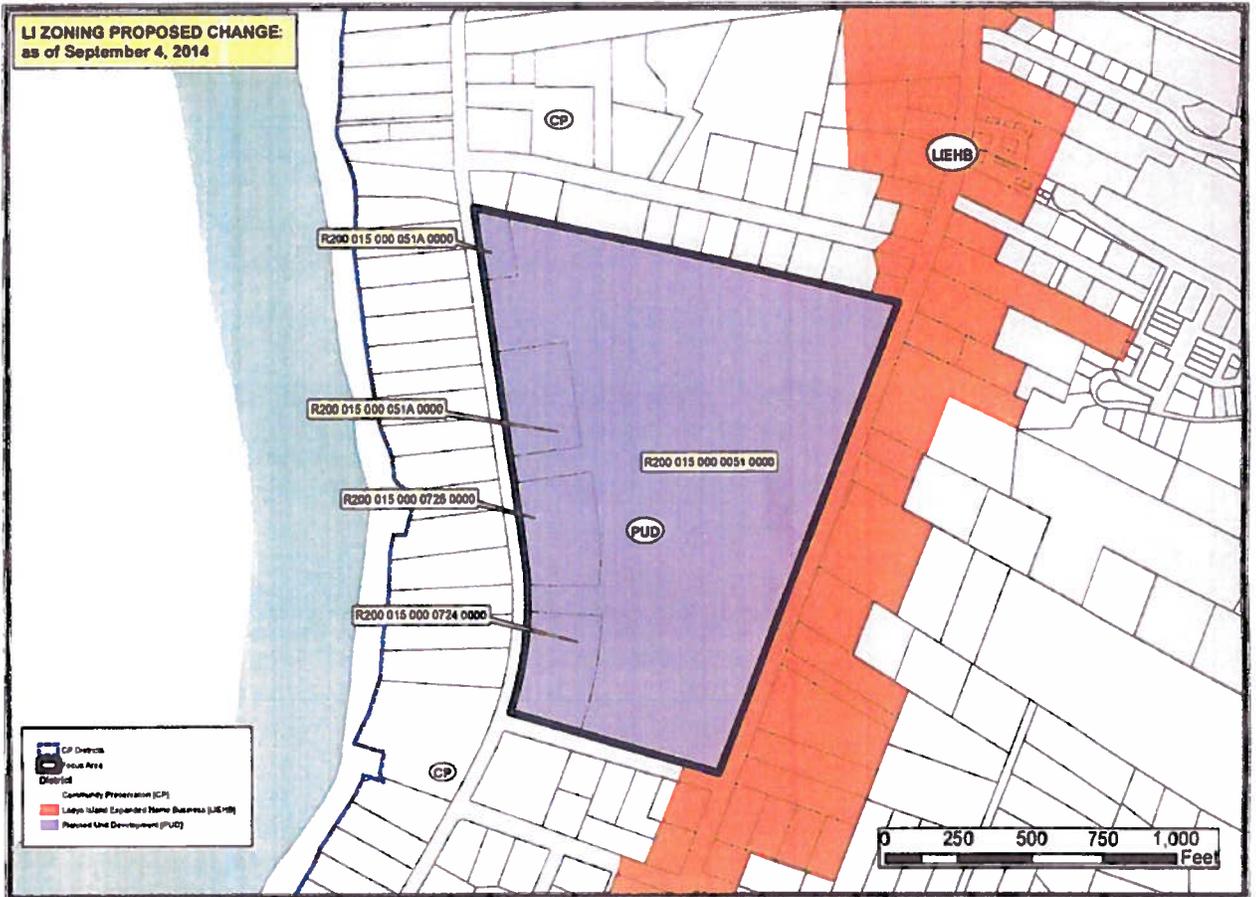
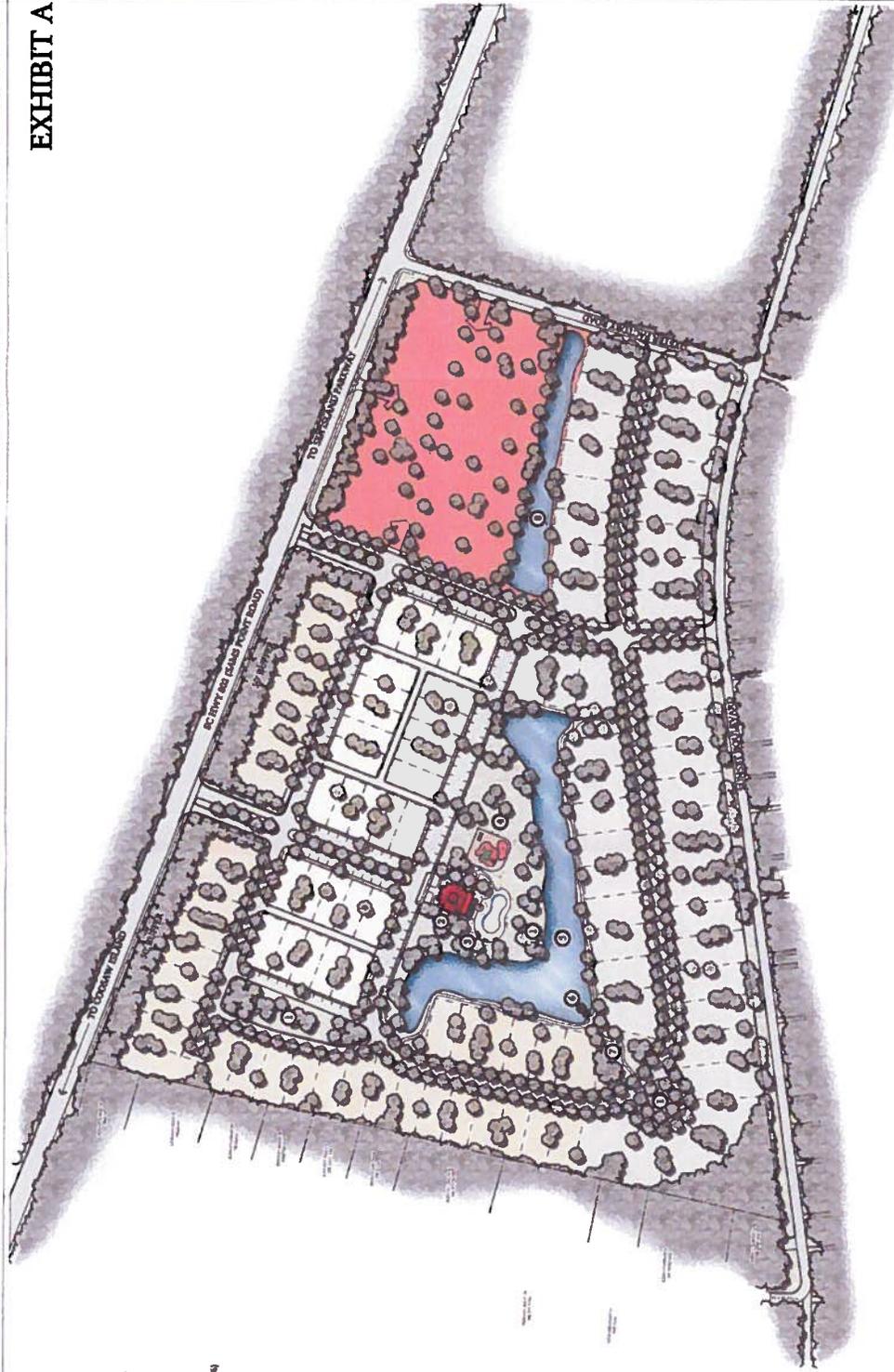


EXHIBIT A



PROPERTY ACREAGE SUMMARY

TOTAL ACRES:	44.3935 ACRES
OYSTER BLUFF T-4	44.5.49 ACRES
CLUBHOUSE CENTER ACREAGE	44.5.49 ACRES
OYSTER BLUFF T-3	44.55.54 ACRES
NEIGHBORHOOD ACREAGE	44.18.77 ACRES (87.3%)
TOTAL OPEN SPACE:	

LOT SUMMARY

49' LOTS* (62' X 110')	30 UNITS
58' LOTS *102' X 110(112)'	37 UNITS
69' LOTS* (69' X 112)	44 UNITS
Oyster Bluff T-4 Clubhouse Center	1 UNIT
TOTAL UNITS	114 UNITS
DENSITY	2.52 UNITS/ACRE

*Oyster Bluff T-3 Neighborhood (T3N)

- LEGEND**
- ① PARK (1.8 TOTAL ACRES)
 - ② CLUBHOUSE
 - ③ POOL
 - ④ PLAYGROUND
 - ⑤ LAKE (1.78 TOTAL ACRES)
 - ⑥ RESIDING GARAGE
 - ⑦ LOOPED FITNESS TRAIL
 - ⑧ LAGOON

PREPARED FOR:
McNeal Land Company

PREPARED BY:
J. K. TILLER ASSOCIATES, INC.
10000 W. 10TH AVENUE, SUITE 100, DENVER, CO 80202
 TEL: 303.733.8888 FAX: 303.733.8889
 WWW.JKTILLER.COM



The Village at Oyster Bluff
Master Plan
 BEAUFORT COUNTY, SOUTH CAROLINA
 August 15, 2014



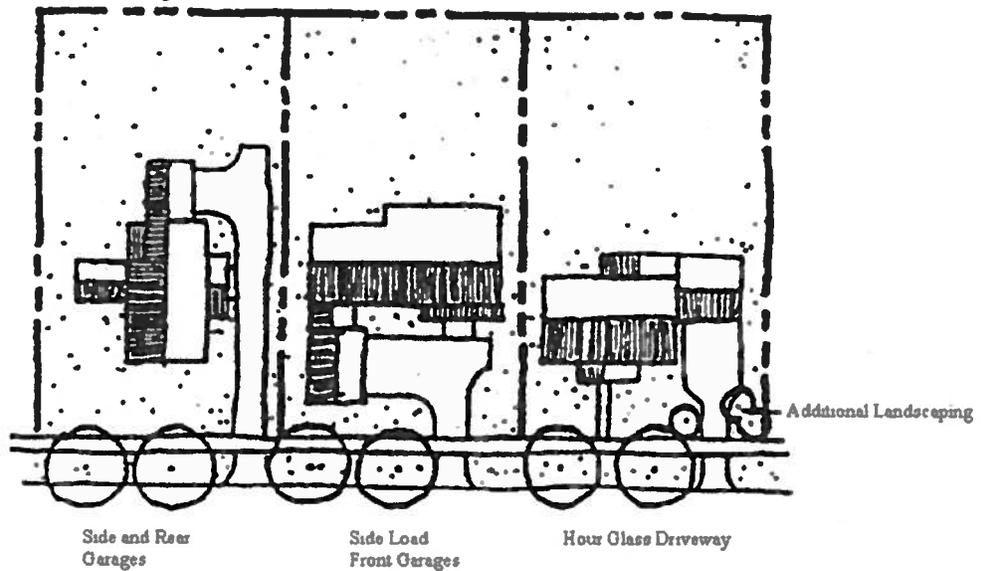
THIS PLAN IS A PRELIMINARY DESIGN AND SHOULD NOT BE USED FOR CONSTRUCTION. ALL PROPERTY LINES, TRACT DESCRIPTIONS AND IMAGINATIVE ENCLOSURES ARE FOR GRAPHIC REPRESENTATION ONLY. IN AN EFFORT TO BE LOCATION AND POTENTIAL LAND USE, AND ARE NOT LEGAL. THE CLIENT HAS BEEN ADVISED OF THIS AND HAS AGREED TO USE THIS PLAN FOR THE PURPOSES OF THIS PROJECT. THE CLIENT HAS BEEN ADVISED OF THIS AND HAS AGREED TO USE THIS PLAN FOR THE PURPOSES OF THIS PROJECT.

**Attachment C: Recommended Standards:
Front Loaded Vehicular Garage Access Oyster Bluff T3 Neighborhood**

Front loaded garages are permitted on lots with widths of 50 feet or greater, and the following shall be used to reduce the impact of drives and garages (Figure 1)

1. Garages shall be recessed from the primary building facade a minimum of 20 feet with a drive of no more than ten feet in width providing access and may include pervious medians.
2. Side load front garages shall be used on at least 40 percent of lots where the garage is not to the rear of the lot.

Figure 1: VEHICULAR GARAGE ACCESS



**BEAUFORT COUNTY, SOUTH CAROLINA
PROPOSED ZONING AND DEVELOPMENT STANDARDS ORDINANCE (ZDSO)
 ZONING MAP / TEXT AMENDMENT / PUD MASTER PLAN CHANGE APPLICATION**

TO: Beaufort County Council

The undersigned hereby respectfully requests that the Beaufort County Zoning/Development Standards Ordinance (ZDSO) be amended as described below:

1. This is a request for a change in the (check as appropriate): PUD Master Plan Change
 Zoning Map Designation/Rezoning Zoning & Development Standards Ordinance Text

2. Give exact information to locate the property for which you propose a change: SEE ATTACHED
 Tax District Number: _____, Tax Map Number: _____, Parcel Number(s): _____
 Size of subject property: 39.09 Square Feet / 0.9 Acres (circle one)
 Location: LADY'S ISLAND, WEST OF SAM'S PT. ROAD, NORTH OF OYSTER FACTORY RD., EAST OF SUNSET BLVD.

3. How is this property presently zoned? (Check as appropriate)
 Urban/U Community Preservation/CP Light Industrial/LI
 Suburban/S Commercial Regional/CR Industrial Park/IP
 Rural/R Commercial Suburban/CS Transitional Overlay/TO
 Rural Residential/RR Research & Development/RD Resource Conservation/RC
 Planned Unit Development/PUD (LADY'S ISLAND (P/PUD))

4. What new zoning do you propose for this property? PLANNED DEVELOPMENT DISTRICT
 (Under Item 10 explain the reason(s) for your rezoning request.) SEE ATTACHED

5. Do you own all of the property proposed for this zoning change? Yes No
 Only property owners or their authorized representative/agent can sign this application. If there are multiple owners, each property owner must sign an individual application and all applications must be submitted simultaneously. If a business entity is the owner, the authorized representative/agent of the business must attach: 1- a copy of the power of attorney that gives him the authority to sign for the business, and 2- a copy of the articles of incorporation that lists the names of all the owners of the business.

6. If this request involves a proposed change in the Zoning/Development Standards Ordinance text, the section(s) affected are: SEE ATTACHED
 (Under Item 10 explain the proposed text change and reasons for the change.)

7. Is this property subject to an Overlay District? Check those which may apply. (NONE)
 AOD - Airport Overlay District MD - Military Overlay District
 COD - Corridor Overlay District RQ - River Quality Overlay District
 CPOD - Cultural Protection Overlay District

8. The following sections of the Beaufort County ZDSO (see attached sheets) should be addressed by the applicant and attached to this application form:
 - a. Section 106-492, Standards for zoning map amendments.
 - b. Section 106-493, Standards for zoning text amendments.

9. Explanation (continue on separate sheet if needed):

R200-015-000-0051-0000 ; R200-015-000-0724-0000 ;
R200-015-000-0725-0000 ; R200-015-000-0514-0000

It is understood by the undersigned that while this application will be carefully reviewed and considered, the burden of proof for the proposed amendment rests with the owner.

J. Bennett Signature of Owner AUGUST 20, 2014 Date

Printed Name: BENNETT MCNEAL Telephone Number: 843-986-7531

Address: P.O. DRAWER 1507, BEAUFORT, SC 29901

Email: BENNETT.MCNEAL@gmail.com

Agent (Name/Address/Phone/email): JOSEPH TILLER, 10 PINCKNEY COLONY RD. STE 101, BLUFFTON, SC 29910, 843-315-9200; jstt@jktiller.com

FOR MAP AMENDMENT REQUESTS, THE PLANNING OFFICE WILL POST A NOTICE ON THE AFFECTED PROPERTY AS OUTLINED IN SEC. 106-402(D) OF THE BEAUFORT COUNTY ZDSO.

UPON RECEIPT OF APPLICATIONS, THE STAFF HAS THREE (3) WORK DAYS TO REVIEW ALL APPLICATIONS FOR COMPLETENESS. THE COMPLETED APPLICATIONS WILL BE REVIEWED FIRST BY THE BEAUFORT COUNTY PLANNING COMMISSION SUBCOMMITTEE RESPONSIBLE FOR THE AREA WHERE YOUR PROPERTY IS LOCATED. MEETING SCHEDULES ARE LISTED ON THE APPLICATION PROCESS (ATTACHED). COMPLETE APPLICATIONS MUST BE SUBMITTED BY NOON THREE (3) WEEKS PRIOR TO THE APPLICABLE SUBCOMMITTEE MEETING DATE.

PLANNED UNIT DEVELOPMENT (PUD) APPLICANTS ARE REQUIRED TO SUBMIT MULTIPLE COPIES TO THE PLANNING DEPARTMENT. CONSULT THE APPLICABLE STAFF PLANNER FOR DETAILS.

CONTACT THE PLANNING DEPARTMENT AT (843) 255-2140 FOR EXACT APPLICATION FEES.

FOR PLANNING DEPARTMENT USE ONLY:

Date Application Received:
(place received stamp below)

Date Posting Notice Issued:

Application Fee Amount Received:

Receipt No. for Application Fee:

The Village at Oyster Bluff, Lady's Island Planned Development District Master Plan Narrative

I. Zoning History, Project Introduction and Overview

The proposed Village at Oyster Bluff on Lady's Island, is a replacement zoning district for the PUD known formerly as The Village at Lady's Island PUD. The proposed Village PUD Master Plan regulatory scheme consists of general narratives, Proposed Layout, Community Standards, Architectural Standards and Projected Land Uses.

The initial PUD was originally approved in 1996 by Beaufort County under the development regulations generally referred to as ordinance 90-3 (as amended) for 200 dwelling units (81 single family and 119 multi-family) and 12 commercial lots at a gross density of approximately 6 density units per acre. At the time the original PUD was approved in 1996, the underlying zoning was Development District (DD), permitting residential development up to 8 units per acre. The property is a +/-39.03 Acre tract of land situated along Sam's Point Road on Lady's Island at the intersection of Oyster Factory Road. The original PUD was designed as a Neo-Traditional community incorporating a mix of housing types and commercial uses. It was a complement to the larger single family home lots created from the holdings along Factory Creek that were the Maggionne Oyster Factory which ceased operations in the early 1980s, and were platted independently of the PUD. The PUD area is the interior area on the other side of Sunset Boulevard that bisected the oyster factory holdings of the Maggionne family.

In December of 2009, a request was made by the applicant to extend the expiration date (December 31, 2010) of the Village PUD. The extension was requested in part because of the 2004 changes to Section 106-7 of the ZDSO that imposed an expiration date on low impact developments, which otherwise would have continued in effect indefinitely, as well as the negative market forces which had developed after the housing/financing crash of 2007-2008, which inhibited construction. A new narrative was prepared addressing the matters required under Section 106-2445 and 2447. Both the original The Village PUD and 2009 new narrative are attached (Exhibit K).

That 2009 submission was not approved, generally because the proposed density in 2009 was considered to be too high by staff and the Lady's Island Community Preservation Committee, and the County was embarking on a revision to its zoning code incorporating form based code principles. The

applicant has continued to work with staff and the Lady's Island Community Preservation Committee to achieve a mixed use plan which is not as dense as that proposed in 2009, but also incorporates concepts from the community code being adopted by the County.

The applicant requests an approval for a new Planned Development District, PDD, The Village at Oyster Bluff PDD that is in keeping with the graduated density and commercial mixed use approach originally approved in The Village PUD. This new PDD proposes a total residential density of 2.92 units per acre, a total of 114 single family units (113 units located in the T-3N and 1 unit located in the T-4 HC). (See Table 1) The proposed density is nearly half the density originally approved back in 1996 and nearly a third less than the underlying zoning at the time. Additionally, and in light of the pending new Beaufort County Community Development Code, this proposed submittal draws from development standards from within the County's new Code and also implement standards that are in keeping with current housing market conditions. In other words, this proposed project's development standards will be an amalgam of both the new Beaufort County Community Development Code and modification to those codes that are constructive to market demand. (See Exhibit J)

Table 1

<u>Transect</u>	<u>Acres</u>	<u>Units</u>
<u>Oyster Bluff T-3 Neighborhood</u>	+/-33.54 AC	113 SF
<u>Oyster Bluff T-4 Hamlet Center</u>	+/- 5.49 AC	1 Apartment
<u>Total:</u>	+/-39.03 AC	114
	<u>Total Density:</u>	2.92 Units/AC

II. Existing Conditions

The subject property owner and applicant, McNeal Land Company, submits this application.

The application seeks approval of The Village at Oyster Bluff PDD based on the conditions of the pending Beaufort County Community Development Code, modifications to the New Code, and the matters contained in the application.

The Village at Lady's Island approximately 39.09 acres have been planned based on available information. Aerial photography was used to identify hardwood tree groupings for master planning efforts. Changes may be required based on constraints identified during the development permit process. This parcel is located along Sam's Point Road on Lady's Island at the intersection of Oyster Factory Road. Tree cover consists of a mixture of hardwoods (See Exhibit B). The property drains west towards Factory Creek. Prior to the widening of Sam's Point Road, and in anticipation of the development of this parcel under the original PUD, cooperative drainage easements were developed from Sam's Point Road to other properties owned by McNeal which ultimately discharge into Factory Creek at an outfall provided by McNeal. The attached Exhibits give detailed information regarding the existing conditions of the property. These items include:

A. Boundary & Wetland Surveys

The boundary survey plat (see Exhibit C) of the property contains the following information: 1) Vicinity Map 2) Boundary and Dimensions 3) Existing Easements 4) Existing Roads and Access Points 5) Property Owners of Adjacent Properties 6) Existing Drainage Ways 7) Setbacks and Buffers

B. Wetlands Permit

There are no freshwater wetlands on the property.

C. Topography

See Exhibit C Boundary Plat

D. Conceptual Stormwater Master Plan

See Exhibit D Drainage Master Plan

E. Conceptual Water Distribution Master Plan

See Exhibit E Water Master Plan

F. Conceptual Wastewater Collection Master Plan

See Exhibit F Sewer Master Plan

III. Development Master Plan

The project will be developed in accordance with the proposed Beaufort County Community Code, as modified herein. Access points, wetlands, archeology and storm water methods have been coordinated. The final location of roads, lagoons, open spaces, buildings, parking, active recreational/athletic sites and other elements may vary at the time of Development Permit Applications. The plan demonstrates a potential arrangement of land uses and road corridors. The final layout will vary based on development needs, market conditions and environmental constraints. The property will be accessed from two separate locations on Sam's Point Road, two separate locations on Oyster Factory Road and one location on Sunset Boulevard. The two entrance locations on Sam's Point Road have been coordinated and approved by SCDOT. In response to community concerns, and contrary to the established pattern of development along Sunset Boulevard, the lots along Sunset Boulevard do not take direct access from Sunset Boulevard, but incorporate an interior alley/roadway system that provides limited interconnectivity to that Road across to Sam's Point Road. The Master Plan allows for the development of single family residential, commercial and active recreational areas in accordance with the pending Beaufort County Community Code as modified herein.

Preliminary phasing for the project is illustrated in the attached Phasing Plan (Exhibit G). Location and engineering for each phase will occur as the market demands and budgets apply.

A. Site Design and Development Standards

The project development standards will emulate the proposed Beaufort Community Code Zoning and Development Standards proposed for adoption for T-3 Neighborhood (T3N) and T-4 Hamlet Center (T-4HC) Transects, as modified herein (Exhibit J) and named The Village at Oyster Bluff Modified T3 Neighborhood and The Village at Oyster Bluff T-4 Hamlet Center Standards, respectively. The applicant intends to responsibly exercise the design functions entrusted to the applicant as the private developer under the Village at Oyster Bluff architectural design guidelines imposed by restrictive covenants. See Exhibit L for an example Conceptual Plan for The Village at Oyster Bluff T-4HC transect.

The Master Plan will meet or exceed the minimum tree requirements as required by Division 5.11: Resource Protection Standards of the proposed Community Code.

Site Development within the Village at Oyster Bluff is governed by the Development Standards included as part of this Master Plan approval.

Exterior buffers and setbacks are shown on Table 2, and internal setbacks are included in Exhibit J.

Table 2:

Street ROW/Boundary	Oyster Bluff T-3 HN	Oyster Bluff T4-HC
SC HWY 802 (Sam's Pt. Road)	50'	10'
Oyster Factory Road	0'	10'
Sunset Boulevard	0'	N/A
Northern Boundary	0'	N/A

B. Storm Water Management

The Stormwater Management Plan is shown on Exhibit D. The storm water layout is preliminary and subject to change pending final design of the drainage system and approval by OCRM and the Beaufort County Engineering department. The storm water will be filtered through the series of interconnected lagoons or equivalent Best Management Practices (BMP) prior to being released into an existing drainage culvert that was previously installed under Sunset Boulevard to convey storm water run off from this tract and the adjacent area into Factory Creek. Additionally, infiltration techniques will be investigated along with other items such as Littoral Shelves at the time of the final drainage system and development permit.

The proposed storm drainage system will comply with the current Beaufort County Ordinance, Beaufort County BMP Manual and OCRM regulations and will meet or exceed these requirements. The final storm water design will be submitted along with other final engineering documents at the time of the development permit application.

C. Utility Services

1) Potable Water Distribution

Potable Water will be provided by Beaufort-Jasper Water & Sewer Authority (BJWSA). An existing water main on Sam's Point Road will provide adequate flow to support this project (See letter of availability from BJWSA).

2) Wastewater Collection

Wastewater Collection will be provided by a combination of gravity sewers, pumping station(s), and force main(s) located throughout the site. The wastewater will be collected and pumped to an existing force main located on Sam's Point road where it will be transported to a wastewater facility owned and operated by BJWSA.

3) Electric & Gas Supply and Service

Power will be provided by South Carolina Electric and Gas; however, gas is not currently available to the project.

4) Telecommunication Service

Telecommunication service will be provided by Hargray Communications. Communications. The telecommunications infrastructure will include voice, data, and video facilities. Initial Master Plan approval does not amend any rights provided to a landowner by the Public Service Commission or South Carolina law.

5) Lady's Island St. Helena Fire District

The community is in the Lady's Island St. Helena Fire District jurisdiction. The water supply system will be designed to provide fire flow to adequately serve the site. (See attached letter of service from the Lady's Island St. Helena Fire District)

D. Proposed Roadways

In addition to the internal drives, a system of pedestrian walks and nature trails is planned. The proposed nature trail will be used by property owners for recreation, exercise and ecological education. A conceptual plan of the proposed nature trail and drive network is

illustrated on the Master Plan (Exhibit A). The actual layout may differ at the time of development permit submission, based upon actual engineering and future planning, so long as the terms of the Concept Plan are respected and followed.

Roadways and drives will be owned and maintained by The Village at Oyster Bluff property association.

E. Ownership and Maintenance of Common Areas and Utilities

1) Common Areas

Development of the property will be owned and maintained by The Village at Oyster Bluff's property association. All easements, buffers, active recreation/athletic areas, open space, nature trails, etc., will be owned by The Village at Oyster Bluff's property association. This ownership will include the maintenance of facilities, lagoons and drainage on the property.

2) Utilities

Beaufort-Jasper Water & Sewer Authority will own and operate the water and sewer facilities necessary for this project. Electrical Power facilities will be owned and operated by SCE&G, or other provider as approved by the Public Service Commission. In addition any Telecommunication Facilities will be owned and operated by Hargray Communications or other provider.

IV. Vesting Provision

The provisions of this PDD, and all phases of development set forth in the Master Plan, shall be vested against any future changes to Beaufort County law or zoning and development ordinances if Owner shall have achieved Substantial Development. "Substantial Development" shall mean (1) the completion of construction (the receipt of a certificate of occupancy) or construction that is underway (the receipt of applicable building or development permits) of not less than twenty-five percent (25%) of the total Development proposed for the property, as shown and depicted on the Master Plan, or 2, the completion of the drainage and paving of the County owned road known as Oyster Factory Road, by the Owner or Developer. Upon the occurrence of either of these alternative events, the Property shall be entitled to complete construction of the as of then undeveloped remaining areas of the Property in accordance with the specifications of the Master Plan.

V. Land Use Parcel Delineations and Uses

The property delineates approximately 39.03 Acres. The land uses are labeled on the Master Plan to identify the most likely uses expected under the applicant's current development forecasts. Of the total acres, approximately 5.49 Acres are commercial, approximately 33.54 Acres are residential with +/-10.77 Acres of common open space.

Development of the property will be subject to design guidelines that comply with the development standards of the pending Beaufort County Community Code as modified herein. All environmental standards, including Best Management Practices regarding storm water runoff, will be demonstrated at the time of final design and Development Permit Application. Future development permits will be issued for site specific development on each site upon individual applications for development approval which demonstrate compliance with these applicable use and site standards.

VI. Traffic Impact and Mitigation

The proposed Village at Oyster Bluff development is located north of Oyster Factory Road between Sam's Point Road and Sunset Blvd. in Beaufort, South Carolina. For the Planned Unit Development (PUD) traffic analysis, the project is assumed to include 114 single family homes, a 16 fueling position gas station, 10,000 square feet (sf) of office space, and 29,000 sf of self storage with one apartment over the storage office. Detailed site planning will identify site circulation plan for the development to ensure proper traffic flow and truck access to the proposed land uses.

Development access is planned via two full access driveways on Sam's Point Road, one right-in, right-out driveway on Sam's Point Road and two full access driveways on Oyster Factory Road. Two full access curb cuts exist today on Sam's Point Road for this property, but would need to be upgraded for the design of the new use of the property. Traffic was assigned to the proposed right-in, right-out driveway (Driveway #3) in the analysis, however, this location is subject to South Carolina Department of Transportation (SCDOT) approval as an additional access point for the site along Sam's Point Road. If this location is not granted by SCDOT, it is expected that these trips would access the site via Driveway #2 or Driveway #4 and would increase the traffic demand at those driveways. The spacing of the access points on Oyster Factory Road is subject to Beaufort County approval. An additional access point is being contemplated on Sunset Boulevard. This access point is projected to have limited use from the residential units. In the analysis, these trips were combined into Driveway #5 trip distribution.

For the purposes of this Traffic Impact Analysis (TIA), proposed development is assumed complete in 2019.

This report presents the trip generation, distribution, traffic analyses, and recommendations for transportation improvements required to meet anticipated traffic demands.

The following intersections were included in this analysis based on the direction of Beaufort County staff:

- Sam's Point Road (US 21) at Sea Island Parkway (US 21 Business)
- Sam's Point Road at Oyster Factory Road
- Oyster Factory Road at Sunset Blvd.
- Project driveways

All intersections currently operate at LOS D or better.

In the 2019 no build conditions, the delay and congestion are projected to increase at study area intersections. Intersection splits were optimized at the intersection of Sam's Point Road at Sea Island Parkway in the 2019 no build conditions but additional roadway improvements were not assumed. The unsignalized study area intersections operate at LOS D or better for both the AM and PM peak hour 2019 no build scenarios. However, Sam's Point Road at Sea Island Parkway operates at LOS E during the AM peak hour and LOS D during the PM peak hour.

The following improvements were assumed in the 2019 build analysis:

- Oyster Factory Road paved from Sunset Boulevard to Sam's Point Road
- Construction of a westbound left-turn lane on Oyster Factory Road from Driveway #4 to Sam's Point Road
- Upgrade of Driveway #1 and Driveway #2 on Sam's Point Road to SCDOT driveway standards each with exclusive left- and right-turn lanes

Sam's Point Road at Sea Island Parkway, Sam's Point Road at Oyster Factory Road, and Sam's Point Road at Driveway #2 are projected to operate with elevated delay during the AM peak hour conditions. Sam's Point Road at Sea Island Parkway is projected to continue to operate at LOS E with an increase in delay of three percent from the 2019 AM peak hour no build conditions. A specific improvement has not been identified at this time for this intersection. It is recommended that the potential for improvements at this intersection be revisited when the final land uses are designed for the site to determine what improvement or improvements, such as installation of turn lanes are needed for the intersection.

The unsignalized intersections of Sam's Point Road at Oyster Factory Road and Sam's Point Road at Driveway #2 are projected to operate at with elevated delay

for side street left-turn operation in the morning. Side street delay at minor approaches on major roadways is not uncommon. Exclusive left-turn lanes are already planned for these movements.



COUNTY COUNCIL OF BEAUFORT COUNTY
BEAUFORT COUNTY PLANNING DIVISION
Multi-Government Center • 100 Ribaut Road, Room 115
Post Office Drawer 1228, Beaufort SC 29901-1228
Phone: (843) 255-2140 • FAX: (843) 255-9432

September 3, 2014

RE: Notice of Public Meetings to Consider a Lady's Island Map Amendment/Rezoning for R200 015 0000 0051, -051A, -0724, AND 0725 (formerly known as The Village at Lady's Island Planned Unit Development (PUD); approximately 39.9 acres total, bordered by Sam's Point and Oyster Factory Roads), from Lady's Island Community Preservation (LICP) and Lady's Island Expanded Home Business (LIEHB) to Planned Unit Development (PUD); Owner: B McNeal Partnership LP, Applicant: Bennett McNeal, Agent: Josh Tiller

Dear Property Owner:

In accordance with the Beaufort County Zoning & Development Standards Ordinance, Section 106-402, a public hearing is required by the Beaufort County Planning Commission and the Beaufort County Council before a map amendment/rezoning proposal can be adopted. You are invited to attend the following meetings and public hearings to provide comments on the subject proposed map amendments in your neighborhood. A map of the properties is on the back of this letter.

1. **The Metropolitan Planning Commission: Monday, September 15, 2014, at 5:30 p.m.** at the City of Beaufort Council Chambers, 1911 Boundary Street, Beaufort, SC 29902.
2. **The Beaufort County Planning Commission (public hearing): Monday, October 6, 2014, at 6:00 p.m.** in the County Council Chambers, located on the first floor of the Beaufort County Administration Building, 100 Ribaut Road, Beaufort, SC.
3. **The Natural Resources Committee of the County Council: Monday, November 3, 2011, at 2:00 p.m.** in the Executive Conference Room, located on the first floor of the Beaufort County Administration Building, 100 Ribaut Road, Beaufort, SC.
4. **Beaufort County Council** usually meets second and fourth Mondays at 4:00 p.m. in the County Council Chambers of the Beaufort County Administration Building, 100 Ribaut Road, Beaufort, SC. County Council must meet three times prior to making a final decision on this case. Please contact the County Planning Department for specific dates, times, and locations.

Documents related to the proposed amendment are available for public inspection between 8:00 a.m. and 5:00 p.m., Monday through Friday, in the Beaufort County Planning Department office located in Room 115 of the Beaufort County Administration Building. If you have any questions regarding this case, please contact the Planning Department at (843) 255-2140.

Sincerely,

Anthony J. Criscitiello
Planning Director

Attachment on back of letter

**PROPERTY OWNERS NOTIFIED OF REZONING REQUEST FOR R200 015 000 0051, -051A, -052A, -0724, AND -0725
from Lady's Island Community Preservation (LICP) & LICP Expanded Home Business Districts to Planned Unit Development (PUD)**

<u>PIN</u>	<u>Owner1</u>	<u>MailingAdd</u>	<u>City</u>	<u>State</u>	<u>ZIP</u>
R200 15 650	ATKISON GEORGE V ELIZABETH MCCORMICK	147 SUNSET BOULEVARD	BEAUFORT	SC	29907
R200 15 51, 51A, 52A, 724, 725	B MCNEAL PARTNERSHIP LP	POST OFFICE BOX 1507	BEAUFORT	SC	29901-1507
R200 15 13S	BEAUFORT BUNGALOWS LLC	821 RIBAUT ROAD	BEAUFORT	SC	29902
R200 14 136	BLOCKER LINDA	4 O'NEAL ROAD	BEAUFORT	SC	29907
R200 15 52B	BOGDAN DONALD V CARTER DEBORAH E J	POST OFFICE BOX 745	BEAUFORT	SC	29901
R200 15 13A	BUQUET ANNA M & LIVINGSTON MARY K	POST OFFICE BOX 975	BEAUFORT	SC	29901
R200 15 656	CHRISTIAN DON RAY & RYAN WALKER	135 SUNSET BOULEVARD	BEAUFORT	SC	29907
R200 15 52C	DUKES EDWARD S WILLIAM J	820 BAY STREET	BEAUFORT	SC	29902
R200 15 672	DYSON GREGORY W & SARAH M	13 WATERSIDE DRIVE	BEAUFORT	SC	29907
R200 15 597	FACTORY CREEK LANDING GROUP LLC	2009 BAY STREET	BEAUFORT	SC	29902
R200 15 633	FARRIOR DAVID P	1 HERMITAGE POINTE	BEAUFORT	SC	29907
R200 15 13M & 598	FLOOD SCHERRA A/K/A SCHERRA WALLACE	12 SUNSET BLUFF	BEAUFORT	SC	29907
R200 15 49A	FRAZIER EVELYN	148 SAMS POINT ROAD	BEAUFORT	SC	29907-9804
R200 15 652	GALLAGHER RAYMOND DIANE	143 SUNSET BOULEVARD	BEAUFORT	SC	29907
R200 15 13P	GLOVER JOYCE E	991 DOWLINGWOOD DRIVE	BEAUFORT	SC	29902
R200 14 126	GLYNN THOMAS W Jr & TONYA M	7 O'NEAL ROAD	BEAUFORT	SC	29907
R200 15 13R	GRAVES ROBERT N & POLLY P	40 WALLACE ROAD	BEAUFORT	SC	29907
R200 15 49B	GREEN CASSIE MAE PINCKNEY	4916 WINCHESTER STREET	CHARLOTTE	NC	28208
R200 15 670	GREEN JAMES R CHARLENE B	127 SUNSET BOULEVARD	BEAUFORT	SC	29907
R200 15 675	HARDEN THOMAS P & SUSAN R	121 SUNSET BOULEVARD	BEAUFORT	SC	29907
R200 15 12	HEAPE EDWIN R KATHY L	3 SUNSET BLUFF	BEAUFORT	SC	29907-1453
R200 15 607	HEAPE PROPERTIES LP	3 SUNSET BLUFF	BEAUFORT	SC	29907
R200 15 743 & 908	HENRY KATHLEEN G	30 WALLACE ROAD	BEAUFORT	SC	29907
R200 15 671	HESTER REVOCABLE TRUST	119 SUNSET BOULEVARD	BEAUFORT	SC	29907
R200 15 44A	HEYWARD ALBERTHA % ALBERTHA SHAND	13 SHORTS LANDING ROAD	BEAUFORT	SC	29907-7110
R200 15 13E, 746, 747, 748	HOOS WILLIAM A	11A SUNSET BLUFF	BEAUFORT	SC	29907
R200 15 27B	JBAILY PROPERTIES LLC	25 KATELYNS WAY	BEAUFORT	SC	29907
R200 15 330	JPMORGAN CHASE BANK NATIONAL ASSOCIA	3415 VISION DRIVE 1ST FL	COLUMBUS	OH	43219 6009
R200 15 52	KAND MARK STEPHANIE	12 FACTORY CREEK ROAD	BEAUFORT	SC	29907
R200 15 13J	KEENAN ROBERT J CLAUDETTE	31 ORCHAROAD DRIVE	ST HELENA IS	SC	29920
R200 15 669	KELLY THOMAS J PATRICIA L	129 SUNSET BOULEVARD	BEAUFORT	SC	29907
R200 15 673	KEVIN D & ROSEMARY R CUPPIA REVOC TRUST AGRMT	125 SUNSET BOULEVARD	BEAUFORT	SC	29907
R200 15 269A	KIMMEL TED S	17 FACTORY CREEK ROAD	BEAUFORT	SC	29907
R200 15 651	KING DONALD R NANCY L	145 SUNSET BOULEVARD	BEAUFORT	SC	29907
R200 15 13C	KNAPP CAROLE S CHARLES R	23 WALLACE ROAD	BEAUFORT	SC	29907

PROPERTY OWNERS NOTIFIED OF REZONING REQUEST FOR R200 015 000 0051, -051A, -052A, -072A, AND -072S
from Lady's Island Community Preservation (LICP) & LICP Expanded Home Business Districts to Planned Unit Development (PUD)

<u>PIN</u>	<u>Owner1</u>	<u>MailingAdd</u>	<u>City</u>	<u>State</u>	<u>ZIP</u>
R200 15 13L	LAVANE AND MARSHA BARNES TRUST C/O GRAVES ROBERT N. SR.	POST OFFICE BOX 31	SEABROOK	SC	29940
R200 15 87G	LUCKEY INVESTMENT PROPERTIES LLC	97 SAMS POINT ROAD	BEAUFORT	SC	29907
R200 15 67A	MALINOWSKI JAN MARTIN TRUSTEE (JAN	123 SUNSET BOULEVARD	BEAUFORT	SC	29907
R200 15 11	MCAS BEAUFORT A DIV OF CPM FCU	3481 TRASK PARKWAY	BEAUFORT	SC	29906
R200 15 12B, 12C & 640	MCCORMICK HARVEY W III JEAN K	7 SUNSET BLUFF	BEAUFORT	SC	29907
R200 15 694	MCCOWAN TODD Z & CAMERON E	8 FACTORY CREEK ROAD	BEAUFORT	SC	29907
R200 15 46 & 47	MITCHELL WILHELMINA D	8321 AMBER LEAF COURT	RALEIGH	NC	27612
R200 14 134	MIZELL MARIBETH J & DONALD HUGH	10 ONEAL ROAD	BEAUFORT	SC	29907
R200 15 654	MOBLEY THOMAS H JR CAROLYN J	139 SUNSET BOULEVARD	BEAUFORT	SC	29907
R200 15 728 & 774	MOYD-SMITH MARY ELLEN	158 SAMS POINT ROAD	BEAUFORT	SC	29907
R200 15 27A	MULLIGAN WILLIAM L DORIS CAROL	188 SAMS POINT ROAD	BEAUFORT	SC	29907
R200 15 131	NORTHWAY ROBERT G ARLENE L	2 COOSAW RIVER DRIVE	BEAUFORT	SC	29907
R200 15 87H	OAKLEY CRAIG K	POST OFFICE BOX 1174	BEAUFORT	SC	29907
R200 15 693	O'CONNELL DANIEL T SANDRA C	9 OYSTER FACTORY ROAD	BEAUFORT	SC	29907
R200 15 54	O'NEILL M G	7 TABBY POINT LANE	OKATIE	SC	29909
R200 15 269	OQUINN MARLA L	POST OFFICE BOX 1618	BEAUFORT	SC	29901
R200 15 32B	ORAGE ALBERT	166 SAM'S POINT ROAD	BEAUFORT	SC	29907
R200 15 9B	PACHECO PAULO A	20 WALLACE ROAD	BEAUFORT	SC	29907
R200 15 744	PACHECO PAULO A & CUCINELLA EUGENIA B	20 WALLACE ROAD	BEAUFORT	SC	29907
R200 15 28C	PARKER LOUISE	14 ROSEBUD DRIVE	BEAUFORT	SC	29907
R200 15 13B	PEARO ELIZABETH WALLACE	POST OFFICE BOX 2611	KENSINGTON	MD	20891-2611
R200 15 616	PRIESTER GEORGE V	21 WALLACE ROAD	BEAUFORT	SC	29907
R200 15 44	PRINGLE EARNEST JR	5 SHORTS LANDING ROAD	BEAUFORT	SC	29907
R200 15 50	PRINGLE J T DAISY	152 SAMS POINT ROAD	BEAUFORT	SC	29907
R200 15 45	PRINGLE JULIUS D SR (LIFE EST) CAPER	124 SAM'S POINT ROAD	BEAUFORT	SC	29907-9804
R200 15 50A	PRINGLE RAY CAROL	124 SAM'S POINT ROAD	BEAUFORT	SC	29907
R200 15 757-758	PRINGLE THERESA M	POST OFFICE BOX 981	BLUFFTON	SC	29910
R200 15 756	PRINGLE WILLARD	124 SAM'S POINT ROAD	BEAUFORT	SC	29907
R200 15 13H	RANDEL BRYAN P	124 SAM'S POINT ROAD	BEAUFORT	SC	29907
R200 15 664	REEICHEL MARC T TYLER M TRUSTEES	51 WALLACE ROAD	BEAUFORT	SC	29907
R200 15 49	REISELT RONNIE Jr	131 SUNSET BOULEVARD	BEAUFORT	SC	29907
R200 15 655	ROBERTS LAWRENCE R & MARY C	58 SHEPPAROAD ROAD	BEAUFORT	SC	29907
R200 15 691	ROBERTS RICHARD J & ELIZABETH W	137 SUNSET BOULEVARD	BEAUFORT	SC	29907-1417
R200 15 745	RODRIGUEZ JOSE	21 SUNSET BLUFF	BEAUFORT	SC	29907
R200 15 383	ROSS GERALD S	16 WALLACE ROAD	BEAUFORT	SC	29907
		29 BLYTHEWOOD ROAD	BEAUFORT	SC	29907

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from Lady's Island Community Preservation (LICP) & LICP Expanded Home Business Districts to Planned Unit Development (PUD)**

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R200 15 10	SIMMONS GEORGE HEATH & CHILTON GRACE	15 SUNSET BOULEVARD	BEAUFORT	SC	29907
R200 15 52D	SINCLAIR JULIUS B	11 OYSTER FACTORY ROAD	BEAUFORT	SC	29907
R200 15 914	SLONIKER DANIEL & LORA	7 SHORTS LANDING ROAD	BEAUFORT	SC	29907
R200 15 13U	SNYDER ELIZABETH B	187 PLEASANT POINT DRIVE	BEAUFORT	SC	29907
R200 15 13T	SOMMERVILLE DONALD P ANN CROSBY	1509 PIGEON PT ROAD	BEAUFORT	SC	29907
R200 15 31	STONEVY VICTORIA	7 BIG BEN LANE	BEAUFORT	SC	29907
R200 15 238	THOMAS JOSEPH DICKMAN THOMAS CALHOUN	85 AVIAN TRAIL	COLUMBIA	SC	29206
R200 15 92A & 657	TORGENSEN JOHN & MARY ELLEN	133 SUNSET BOULEVARD	BEAUFORT	SC	29907
R200 15 28A	UNITARIAN UNIVERSALIST FELLOWSHIP OF	POST OFFICE BOX 593	BEAUFORT	SC	29901
R200 14 135	WALLACE ALICE	8 ONEAL ROAD	BEAUFORT	SC	29907 2014
R200 15 13 & 130	WALLACE MARTHA M	50 WALLACE ROAD	BEAUFORT	SC	29907-9804
R200 15 12A	WARREN JUANITA ANNETTE BROOKS TRUSTE	1655 ARROWHEAD TRAIL NE	ATLANTA	GA	30345-4115
R200 15 649	WEAVER STERLING A	149 SUNSET BOULEVARD	BEAUFORT	SC	29907
R200 15 28d, 739 & 764	WHITE CALVIN R SR CYNTHIA R	182 SAMS POINT ROAD	BEAUFORT	SC	29907
R200 15 28B	WHITE FREDDIE S	15 FREDDIE'S WHITE	BEAUFORT	SC	29902
R200 15 40D	WILBORN ARVELL JR DARLENE J	154 SAM POINT ROAD	BEAUFORT	SC	29907
R200 15 653	WILLIAMS JAMES MALONE MARTHA WEBB	141 SUNSET BOULEVARD	BEAUFORT	SC	29907
R200 15 32E	WILSON ANTHONY SYLVESTER	38 LITTLE CAPERS ROAD	BEAUFORT	SC	29907
R200 15 878 & 879	WYNN ODELL R Jr & FLOOD MARGARET MARY	199 SAMS POINT ROAD	BEAUFORT	SC	29907