



MIXED-USE CORRIDORS OF VIBRANT ACTIVITY

1 BEAUFORT
TOWN
CENTER

MOE'S
southwest grill

E QUEEN ST

Our gateway corridors are hubs for regional mobility, shopping, dining and services that are mixed use in nature and accommodate pedestrians, bicyclists and motor vehicles in a safe and attractive manner.

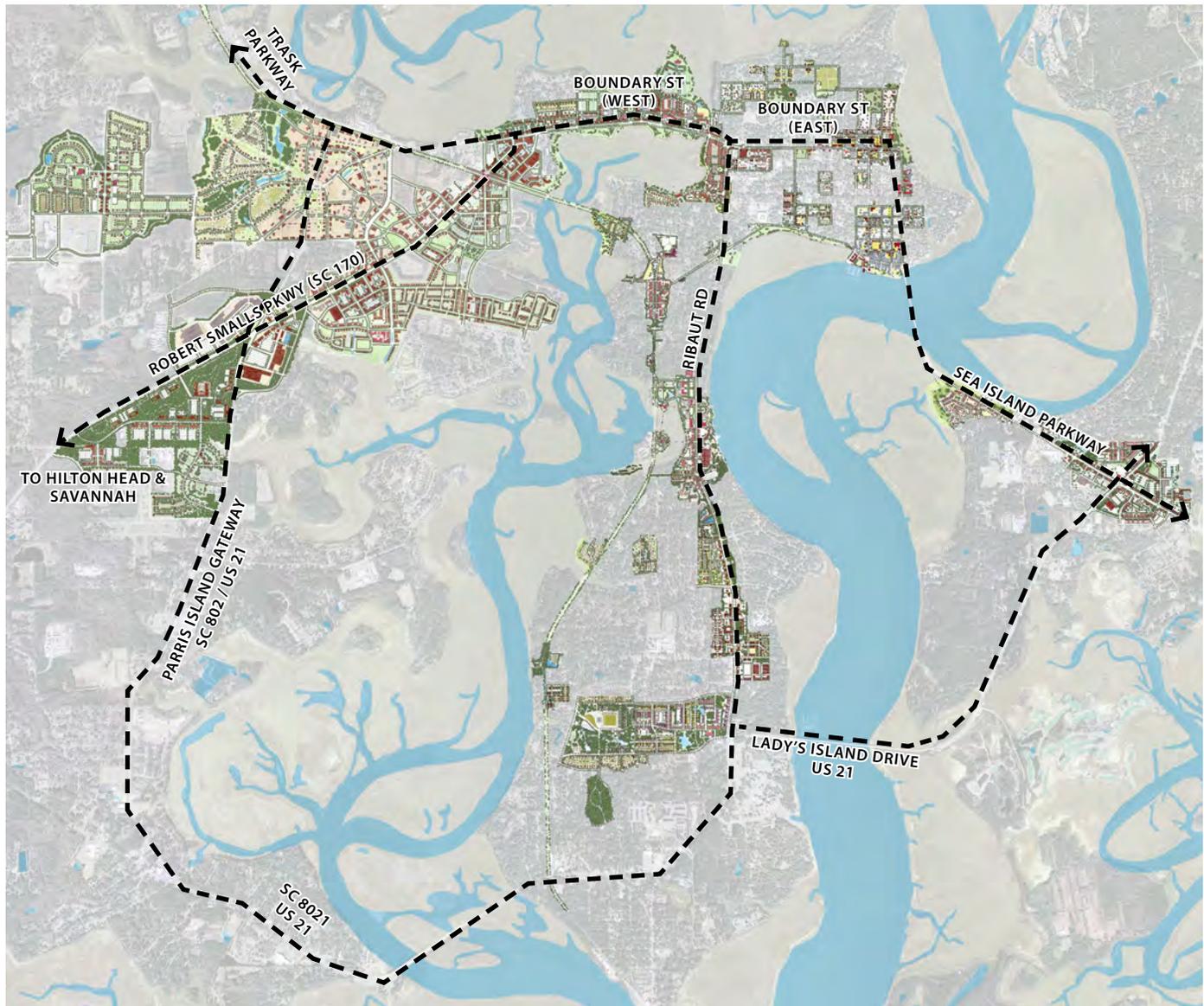
KEY STRATEGIES

- 1:** THE DESIGN OF THE STREETScape ALONG MIXED-USE CORRIDORS SHALL BE SUPPORTIVE OF ITS USE AND FUNCTION
- 2:** THE GROUND FLOOR FRONTAGE OF ALL STRUCTURES IN THE MIXED-USE CORRIDORS SHALL ENCOURAGE PEDESTRIAN ACTIVITY WITH CLEAR ENTRIES FROM THE PUBLIC SIDEWALK, A HIGH PERCENTAGE OF STOREFRONT ALONG THE FRONTAGE, AND AWNINGS/CANOPIES FOR WEATHER PROTECTION
- 3:** PARKING AREAS WILL NOT BE LOCATED ALONG FRONTAGES AND WILL BE INTERCONNECTED WITH ADJACENT PARKING AREAS TO MINIMIZE DRIVEWAYS
- 4:** REGIONAL CORRIDORS SUCH AS TRASK PARKWAY (US 21), BOUNDARY STREET (WEST OF RIBAUT), ROBERT SMALLS PARKWAY (SC 170), RIBAUT ROAD AND SEA ISLAND PARKWAY ARE INTENDED TO PROVIDE OVERALL MOBILITY THROUGH THE COMMUNITY FOR CARS, BICYCLISTS AND PEDESTRIANS
- 5:** WHERE ON-STREET PARKING IS NOT PRACTICAL, AN ENHANCED SIDEWALK, BICYCLE NETWORK, AND MORE SUBSTANTIVE LANDSCAPING WILL BE PURSUED
- 6:** THOUGH SOME CORRIDORS WILL BE AUTO-ORIENTED BY THE NATURE OF THE STREETS AND THE LACK OF PLANNED ON-STREET PARKING, THE CAREFUL PLACEMENT OF BUILDINGS AND RELATED PEDESTRIAN AND BICYCLE FACILITIES IS CRITICAL

KEY INITIATIVES

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▲ OVERVIEW MAP OF MAJOR CORRIDORS

During the Civic Master Plan process the four corridors discussed in this chapter—Ribaut Road, Boundary Street, Robert Smalls Parkway and Sea Island Parkway—were examined as corridors of special significance. Together, these streets make up the primary arterial network that connect the city and the region. They also present some of the most noteworthy opportunities for significant urban development in the city.

All of these streets are typical 20th century urban arterials that were designed with a narrow focus on traffic capacity. This chapter reimagines each of these arterials as mixed-use urban corridors that permit people to walk and bicycle comfortably in the shade of trees along streets lined with

shopfronts, offices, housing, and prominent civic institutions. The street designs and conceptual redevelopment opportunities presented in this chapter have been carefully designed and detailed to balance the needs of pedestrians, bicyclists and automobiles at different points along their corridors.

As civic investment projects, the transformation of these streets will require collaboration between the city and the South Carolina Department of Transportation (SCDOT) in order to make the physical street improvements that will spur new private development and redevelopment projects along these corridors.



▲ RIBAUT ROAD TODAY



▲ BOUNDARY STREET TODAY



▲ ROBERT SMALLS PARKWAY TODAY

Three Lanes vs. Four lanes - The Benefits of Road Diets

Across the country, communities are finding “road diets,” or the reconfiguration of existing streets by reducing vehicular lanes and adding other roadway accommodation, extremely beneficial. The goal of such streetscape modifications is to create a complete street that addresses pedestrians, cyclists and surrounding development, as well as vehicular traffic. There are a variety of factors that determine whether a road diet is an appropriate design solution, including the number of travel lanes, traffic volumes, turn movements, the presence of parallel routes, connections to non-motorized networks, and expectations for the character and intensity of fronting development.

Typical streetscape modifications that may be included in a four-to-three lane road diet include:

- Wider sidewalks
- The incorporation of bike lanes or multi-use paths
- Greater landscape buffer between cars, cyclists and pedestrians
- The addition of on-street parking in neighborhood center areas
- Dedicated transit lanes and shelters
- Pedestrian refuge island at crosswalks
- Planted medians

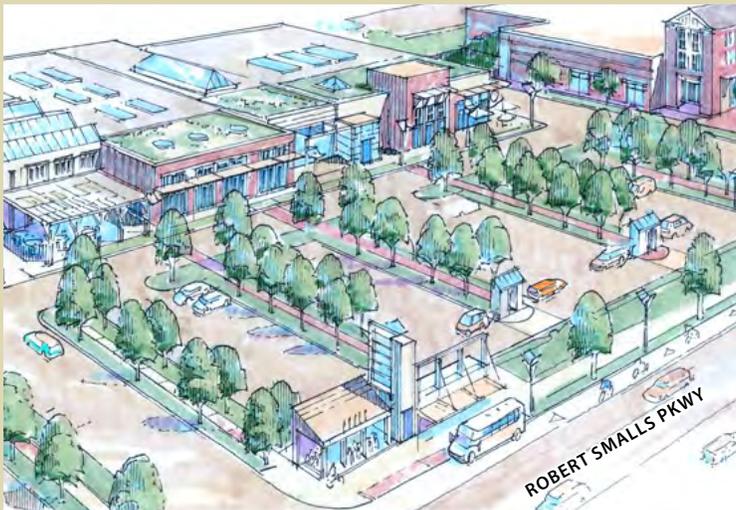
It is not feasible to incorporate all of the streetscape modifications listed above through a one-lane reduction in pavement width. As such, it is important to carefully select and tailor the proposed improvements to the goals of the corridor as a whole, and to individual portions of roadway based upon the expectations for surrounding development.

Many communities across the country are implementing this technique to improve both the operation of the roadway and the character of the surrounding area. When used effectively, benefits of a road diet include:

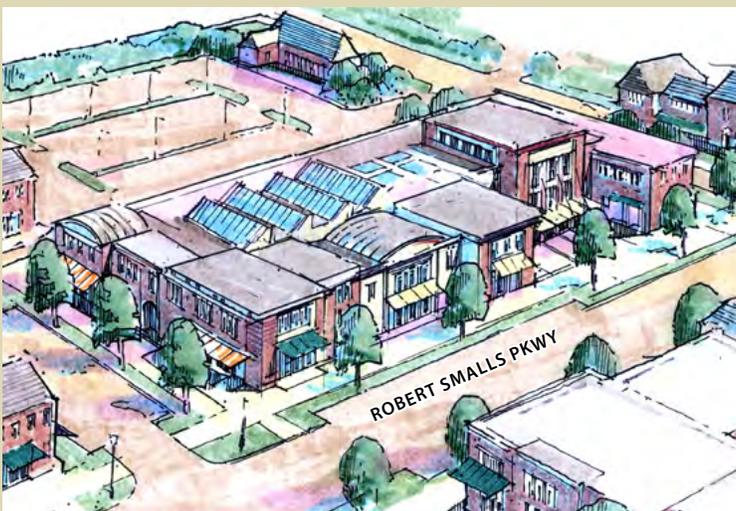
- Greater mobility and accessibility by non-motorized forms of transportation
- Better control of traffic speeds (i.e. traffic calming)
- Greater consistency of traffic movement
- Greater reliability and efficiency of transit
- Improved road safety
- A more attractive and consistent streetscape environment
- Economic development along the corridor



▲ CONCEPTUAL STRIP MALL – EXISTING



▲ CONCEPTUAL STRIP MALL REPAIR – PHASE 1



▲ CONCEPTUAL STRIP MALL REPAIR – PHASE 2

Conceptual Strip Mall Retrofit Example

The series of images at left shows a phased approach to transforming an ordinary auto-oriented strip mall area into a lively neighborhood center; this approach is relevant to all of the corridors discussed in this chapter. The goal of this type of strategic redesign is to allow property owners to gradually build value by increasing curb appeal and attracting new tenants, while also creating new spaces that encourage community gatherings.

- **Existing:** As it stands today, this strip mall is dominated by a treeless, empty parking lot.

- **Phase 1:** The first level of design intervention shows building facade improvements, the addition of shade trees, and new buildings sited at the street edge to engage new sidewalks. On-street parking helps to slow traffic and acts as a buffer to pedestrians from the lanes of car traffic.

- **Phase 2:** The second phase of redevelopment fills the existing parking lot with buildings and moves the parking lot to the rear. Retail, office, and housing uses are all mixed in the same block so that people can live, work, and shop within the same neighborhood. The street is further refined to fully accommodate people walking, bicycling, and driving. Altogether, this block becomes part of a vibrant, memorable community.

8.1 Boundary Street (West of Ribaut Road)

Sector: 4

Project Type: Public | Private

Civic Investment Required: Boundary Street
Streetscape Improvements

See also: 2.9; 3.8; 4.14; 5.2; 8.3; Boundary
Street Master Plan

In accordance with the Boundary Street Master Plan (BSMP) adopted in 2006, the city is seeking to transform Boundary Street (west of Ribaut Road) from a five-lane strip commercial corridor into a complete, compact, and connected, mixed-use district that supports a more walkable, livable, and sustainable community with multi-modal forms of transportation. The overall Boundary Street infrastructure plan combines a number of integrated projects including a realigned intersection of Boundary Street and SC 170, a landscaped median, wide sidewalks, a multi-use path, a new Main Street through Beaufort Plaza, and an extension and improvement of the current Polk/First Street that parallels Boundary Street.

Polk Street serves as integral element in the BSMP by providing a new east-west connection that can be used by the residents who live and work on the north side of Boundary Street. Over time, these improvements are expected to trigger new residentially scaled, mixed-use development to complement the surrounding Polk Village

neighborhood. In turn, Polk Village will see a resurgence in its desirability as it will have waterfront access along the marshes of the Beaufort River and be within walking distance of the vibrant and attractive Boundary Street corridor.

The BSMP also calls for the conversion of existing commercial lots on the south side of the street along the marsh areas into permanently protected open space. In addition, mobility between parcels on the north side will be facilitated by an interconnected series of driveways and a shared access driveway across the frontage to improve on-site circulation and minimize driveway cuts along Boundary Street over time.

This plan proposes that where the Spanish Moss trail intersects the corridor at Beaufort Plaza it is extended to serve the commercial areas along Boundary Street and the neighborhoods to the north. As in the BSMP, a roundabout in front of City Hall will eventually replace the current intersection of Ribaut Road and Boundary Street to help improve overall traffic flow and reduce travel speeds through the area.

Over time, as the area between Neil Road and Ribaut Road continues to redevelop, this plan will help to guide new street connections through the area to form a fine-grained network that accommodate new mixed-use development. This street network will provide choices for those moving through this corridor in a car, on a bike, or walking.

The following two pages show an informational flyer used to update citizens about the Boundary Street project (for west of Ribaut Road).



▲ EXISTING CONDITIONS ALONG BOUNDARY STREET (WEST OF RIBAUT ROAD)

PROJECT INFORMATION

Jean Ribaut Square Montage



Phase 1



Phase 3



Phase 2



Phase 4

RETROFITTING SUBURBIA

WHAT IS TIGER III?

Transportation Investment Generating Economic Recovery (TIGER) is a supplementary discretionary grant program included in the American Recovery and Reinvestment Act of 2009. TIGER grants are awarded to transportation projects that have a significant national or regional impact.

Projects are chosen for their ability to contribute to the long-term economic competitiveness of the nation, improve the condition of existing transportation facilities, increase energy efficiency and reducing greenhouse gas emissions, improve the safety of U.S. transportation facilities and enhance the quality of living and working environments of communities through increased transportation choices and connections. The Department also gives priority to projects that are expected to create and preserve jobs quickly and stimulate increases in economic activity.

Source: USDOT



Boundary Street Redevelopment



Boundary Street Redevelopment



Boundary Street Redevelopment

The Boundary Street Redevelopment District has a unique level of broad regional support from the public and private sectors. Over the last five years, the City of Beaufort and Beaufort County have undertaken an extensive planning, design, engineering, and public financing effort that has led to a completion of a substantial amount of the required plans and construction documents in order to implement this project, including but not limited to the Boundary Street Master Plan, Boundary Street Form-Based Code, Boundary Street Redevelopment Plan, Beaufort County One Percent (1%) Transportation Sales and Use Tax, and related Boundary Street Redevelopment District design and engineering documents.

To date, \$9 million in public sector funding has been committed by Beaufort County as additional funds to implement this project via the citizens of Beaufort County approval of the Beaufort County One Percent (1%) Transportation Sales and Use Tax that included the Boundary Street Redevelopment District as a project within the aforementioned program. The estimated cost to develop and construct this infrastructure project is \$30 million.

The \$12.635 million in TIGER III funds are essential to the retrofit of this suburban, strip corridor into a complete, compact and connected mixed use district that supports a more walkable, livable, and sustainable community with multimodal forms of transportation.



Public Input During Boundary Street Charrette

Boundary Street Regulating Network



▲ INFORMATIONAL FLYER FOR THE BOUNDARY STREET (WEST OF RIBAUT ROAD) PROJECT

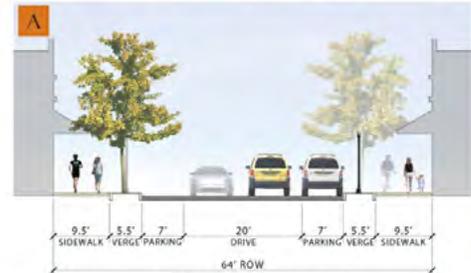
JUNE 1999	Gateway Corridors Redevelopment Plan Established TIF II by City of Beaufort
2006	1% Sales Tax Adopted to Fund Improvements \$13,000,000 Citizen Investment
2006 PRESENT	New Development on Boundary Street \$48,000,000 Private Investment
JUNE 2006	Boundary Street Master Plan Adopted
FEB 2007	Boundary Street Redevelopment District Code Adopted
2008	Traffic Impact Analysis & Feasibility Study
2009	Boundary Street Redevelopment District TIF Overlay Adopted
2010	Beaufort City Hall Complete \$23,000,000 Public Investment
2012	Redevelopment Commission Boundary Street Redevelopment District \$12,635,000 TIGER III Federal Investment
OCT 2012	National Environmental Policy Act (NEPA) Deadline
MARCH 2013	Complete Right of Way Acquisition
JUNE 2013	Boundary Street Construction Begins
JUNE 2013	DOT Fund Obligation Deadline for TIGER III
OCT 2014	Boundary Street Redevelopment District Substantial Construction Work Complete
DEC 2014	Final Project Closeout



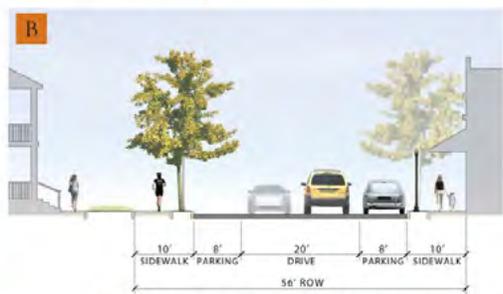
Boundary Street Master Plan

COMPLETE STREETS
For communities to be walkable, streets must be designed with pedestrian comfort and safety as critical goals along with the safe and efficient flow of traffic and other considerations, such as the accommodation of emergency vehicles, parking, utilities, and stormwater. The focus for street design should be on design speed rather than the volume of traffic and the level of service, especially in urban areas.

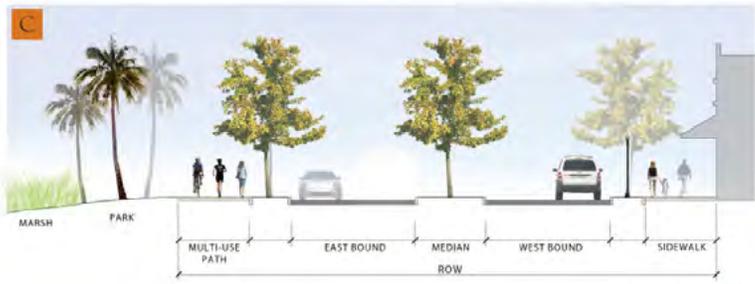
STREET DESIGN
In addition, streets should be typically narrower than conventional streets and arranged in an interconnected, gridded network; intersections should be carefully considered, and such design details as tighter curb radii should be implemented.



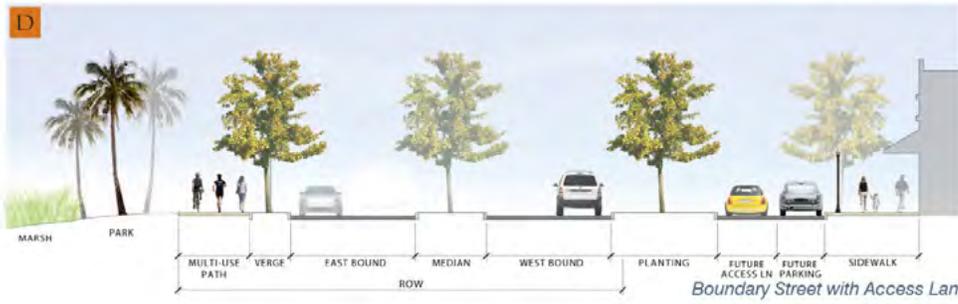
"Main Street" within Beaufort Plaza Shopping Ctr Redevelopment



Parallel Street with Proposed Public Vistas to Marsh



Boundary Street



Boundary Street with Access Lane

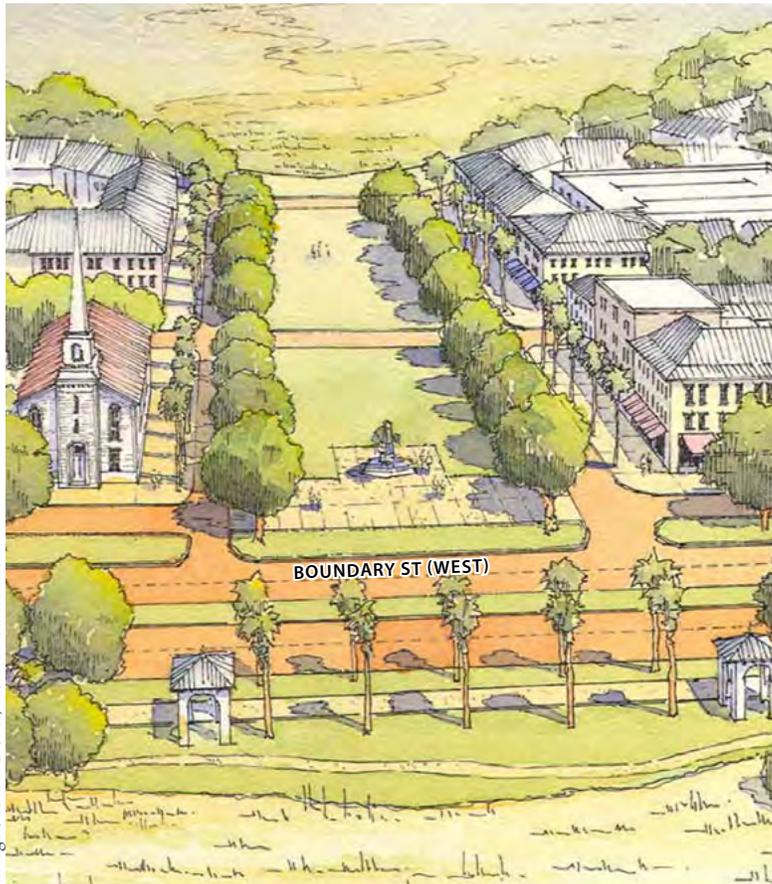


Image Source: Dover, Kohl and Partners

Redevelopment Strategies

The Civic Master Plan recommends redevelopment strategies as set out in the Boundary Street Master Plan, with only minor modifications to account for recent development and planned improvements as a result of the awarded TIGER funds. The intent of this plan is to provide guidance that will create a cohesive environment, even as infill development occurs incrementally over a long period of time.

As in the Boundary Street Master Plan, this plan proposes that large strip centers be redeveloped with mixed-use buildings that are sited to embrace the street edge along the newly created road networks. Parking would be located on the interior of blocks and screened from the public realm by infill buildings.

A primary focus of the redevelopment strategy is to open public access to the waterfront. As such, a continuous marshfront park is proposed along Albergetti Creek to the north, and the small area along the Battery Creek marshes to the south would be preserved as a public park (see 2.9 for details). Civic buildings would be positioned at key points along the corridor, including as an anchoring feature within the proposed Belt Buckle Park.

▲ CONCEPTUAL ILLUSTRATION OF BELT BUCKLE PARK



▲ CONCEPTUAL ILLUSTRATION OF BOUNDARY STREET REDEVELOPMENT

Deanne Drive/Beaufort Plaza

Beaufort Plaza is a parking-dominated strip center at the intersection of Boundary Street and Robert Smalls Parkway. This plan proposes the extension of Deanne Drive across the Spanish Moss Trail and through the existing Beaufort Plaza parking area connecting these areas and creating incremental infill development opportunities. Major tenants would be maintained on the site, redevelopment would reinvigorate the area, and a new streetscape environment would cater to pedestrians and bicyclists as well as automobile users.

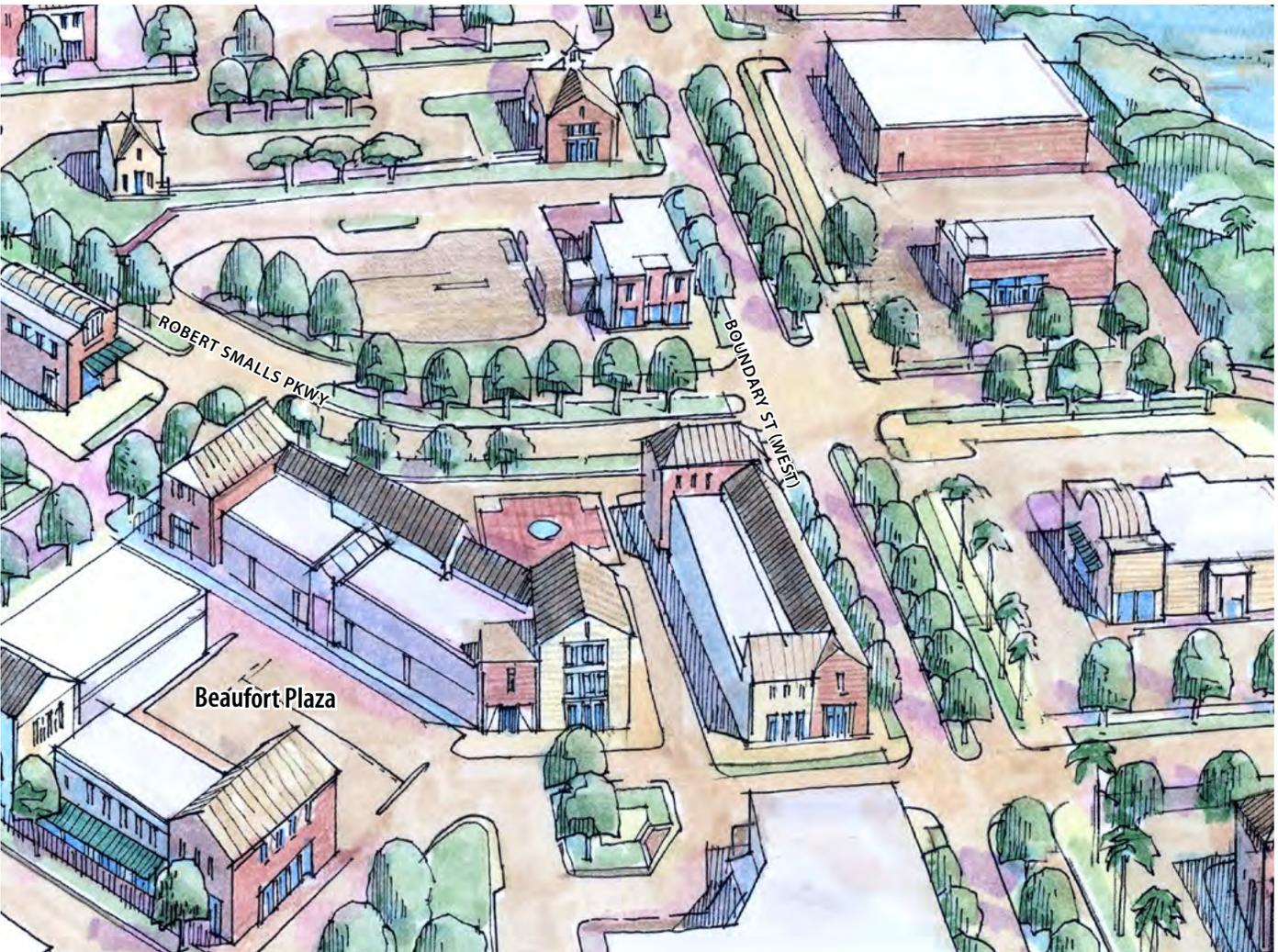
Deanne Drive is the first point where the Spanish Moss Trail crosses a street on the western side of Battery Creek. It is also the point at which the Spanish Moss Trail begins to ascend toward the pedestrian bridge over Robert Smalls Parkway (see 4.14 and 8.3 for details). This plan proposes that Beaufort Plaza serve as a major trailhead, marking the transition point between the commercial intensity of the Beaufort Plaza area to the north of the trail and the primarily residential uses to the south.



▲ CONCEPTUAL PLAN OF BEAUFORT PLAZA REDEVELOPMENT



▲ CONCEPTUAL REDEVELOPMENT ALONG RIBAUT RD CORRIDOR (SOUTH OF ALLISON ROAD)



▲ CONCEPTUAL ILLUSTRATION OF BEAUFORT PLAZA REDEVELOPMENT

8.2 Ribaut Road

Sector: 1, 2 and 3

Project Type: Public | Private

Civic Investment Required: Ribaut Road
Streetscape Improvements

See Also: 7.2; 7.3; 7.4

Streetscape Improvements

Following the same planning model as Boundary Street, this plan suggests a series of alternatives to improve Ribaut Road from its intersection with Boundary Street to the city limits at Mossy Oaks Road in order to improve the conditions for pedestrians and cyclists. Previously, this corridor functioned as US 21 but it has been downgraded with the re-designation of US 21 through Parris Island Gateway. As a result, this is no longer a truck route and GPS devices will help to shift regional traffic through Port Royal to the McTeer Bridge to the south.

The current configuration of the corridor is heavily dominated by vehicular traffic – even though it traverses a residential area – with infrequent sidewalks and no bicycle facilities in the corridor. Additionally, both the Beaufort Memorial Hospital and the Technical College of the Lowcountry have facilities on both sides of Ribaut Road, but it is very challenging for pedestrians to cross the street. The city has long managed speeds in the corridor through an artificial speed limit – one that is set well below the actual design speed. Over time this plan advocates for appropriate lane widths, bicycle and pedestrian facilities, landscaping, and lighting that is appropriate to a mixed-use corridor with a moderate amount of traffic.

Regardless, this plan affirms the city's desire to improve the overall pedestrian and bicyclist mobility in the Ribaut Road corridor, maintain adequate vehicular capacity for both normal operations and emergency access to the hospital facilities, and improve the overall aesthetics for this primary city gateway. These goals will form the basis for a specific corridor plan in the future that will combine the technical capacity and access requirements of the roadway with the long term desires of the community.



▲ EXAMPLE OF A PEDESTRIAN-FRIENDLY, MIXED-USE ENVIRONMENT



▲ PROPOSED IMPROVEMENT AREAS

1 Boundary Street to Bay Street

This portion of Ribaut Road between Boundary Street and Bay Street will serve a wide variety of development contexts, from single family residential, to mixed-use, to the County Government Complex. As such, the pedestrian environment must be improved from its current configuration with sidewalks located directly behind the curb. Planting strips are required to buffer pedestrians from travel lanes. Improved pedestrian crossings at intersections, as well as potential mid-block crossings at the County Government Complex, will enhance overall accessibility and mobility. Finally, with the dedication of additional right-of-way, on-street parking could be incorporated within the street to serve adjacent development and manage travel speeds.

2 Bay Street to Reynolds Street

Between Bay Street and Reynolds Street, fronting development along Ribaut Road is primarily single family residential. The current street configuration includes 4 lanes of fast-moving traffic and sidewalks located at the back of curb—only on one side of the street for most of the corridor—without sufficient buffer from moving vehicles. This configuration is not conducive to the speed of traffic and the pedestrian/bicycle activity that typically occurs along residential streets. Therefore a road diet from 4 travel lanes to 2 travel lanes with a center median/turn lane is highly recommended in this portion of Ribaut. This will improve safety for automobile turning movements into residential driveways and allow right-of-way to be allocated for bike lanes and for planting strips between the sidewalk and travel lanes.

3 Reynolds Street to Allison Road

Two major institutions, Beaufort Memorial Hospital (BMH) and the Technical College of the Lowcountry (TCL), occupy this stretch of Ribaut Road from Reynolds Street to Allison Road. The significant pedestrian activity that these two anchors generate is not adequately supported by the current configuration of Ribaut Road. Planting strips are required to buffer pedestrians from travel lanes. A multi-use path is required along the west side of the corridor to serve the student population of TCL and create a pedestrian/bicycle loop with the nearby Spanish Moss Trail. Improved pedestrian crossings, including mid-block crossings with pedestrian-activated traffic signals at key points create a better connection between BMH and TCL. As these institutions grow and redevelop, it may be appropriate to include slip roads with diagonal on-street parking. This would provide more immediate parking for each institution and transform Ribaut Road into a pedestrian-friendly multi-way boulevard.

4 South of Allison Road

South of the institutional centers of BMH and TCL, Ribaut Road serves a diverse range of development types. The wide right-of-way in this area is currently dedicated almost entirely to the automobile, and must be reconfigured to accommodate pedestrians and bicyclists. Bike lanes are required on both sides of street to serve experienced bicyclists. This bike route will serve a primarily utilitarian function for bicycle commuters and compliment the primarily recreational function of the Spanish Moss Trail on the other side Port Royal peninsula. Planting strips are also required to buffer bicycle and automobile traffic from pedestrians.

Redevelopment Strategies - North Ribaut Road

The Civic Master Plan envisions future development along Ribaut Road to follow the pattern of development set by the new City Hall complex: buildings that front the street with little or no setback from the sidewalk and parking located to the rear of buildings. Framing the street this way provides a more consistent, streetscape environment that encourages walking, accommodates a mix of uses, and increases sidewalk activity.

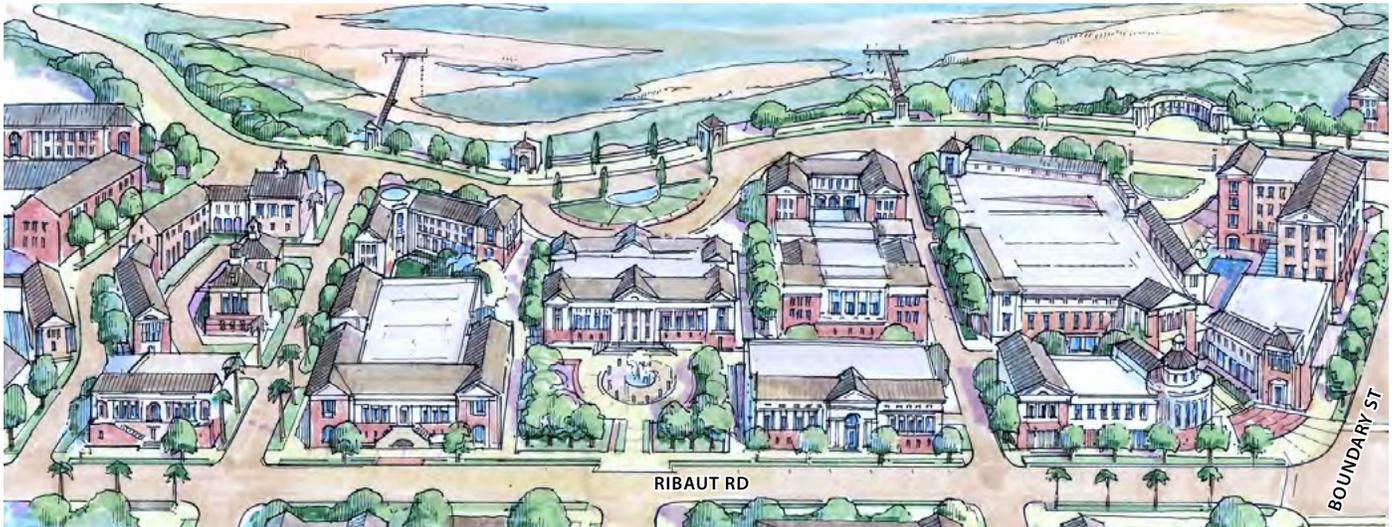
The existing Piggly Wiggly grocery store at the southeast intersection of Ribaut Road and Boundary Street provides essential goods to the surrounding neighborhoods, but meets both Ribaut Road and Boundary Street with a parking lot. The conceptual plan for this area recommends that future redevelopment accommodate growth for the grocery store and introduce commercial infill buildings closer to the street. A proposed roundabout would emphasize the civic significance of the site, provide a prominent gateway feature, and move traffic through the busy intersection at slower, more consistent speeds. Based on the preliminary traffic volumes, the roundabout would likely have two lanes and pedestrian refuge spaces.



▲ CONCEPTUAL ROUNDABOUT AT RIBAUT ROAD & BAY STREET



▲ RIBAUT ROAD (NORTH) CONCEPTUAL CORRIDOR REDEVELOPMENT



▲ CONCEPTUAL REDEVELOPMENT OF COUNTY GOVERNMENT COMPLEX



▲ CONCEPTUAL REDEVELOPMENT OF RIBAUT ROAD AND BOUNDARY STREET

Redevelopment Strategies - South Ribaut Road

South of Allison Road, the illustrative plan proposes a gradual transformation into a walking-friendly neighborhood corridor anchored by a fire station and new housing opportunities. Buildings would be set close to the street with parking areas to the side and rear of the building. Over time, the underutilized parking areas and single-story structures would transition to more economically viable small and medium-sized multi-story buildings. Given the proximity of Allison Road to two major employers—TCL and the Hospital—this area could support higher-density housing. One of the key public investments in this area would be a Fire Station that would serve as the city’s prominent civic building in this area (see 7.4 for details).



▲ RIBAUT ROAD (SOUTH) CONCEPTUAL CORRIDOR REDEVELOPMENT

8.3 Robert Smalls Parkway

Sector: 4

Project Type: Public | Private

Civic Investment Required: Multi-Use Path, Spanish Moss Trail

See Also: 4.14; 5.2

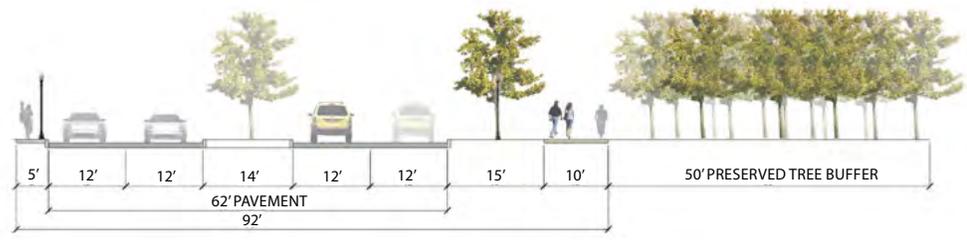
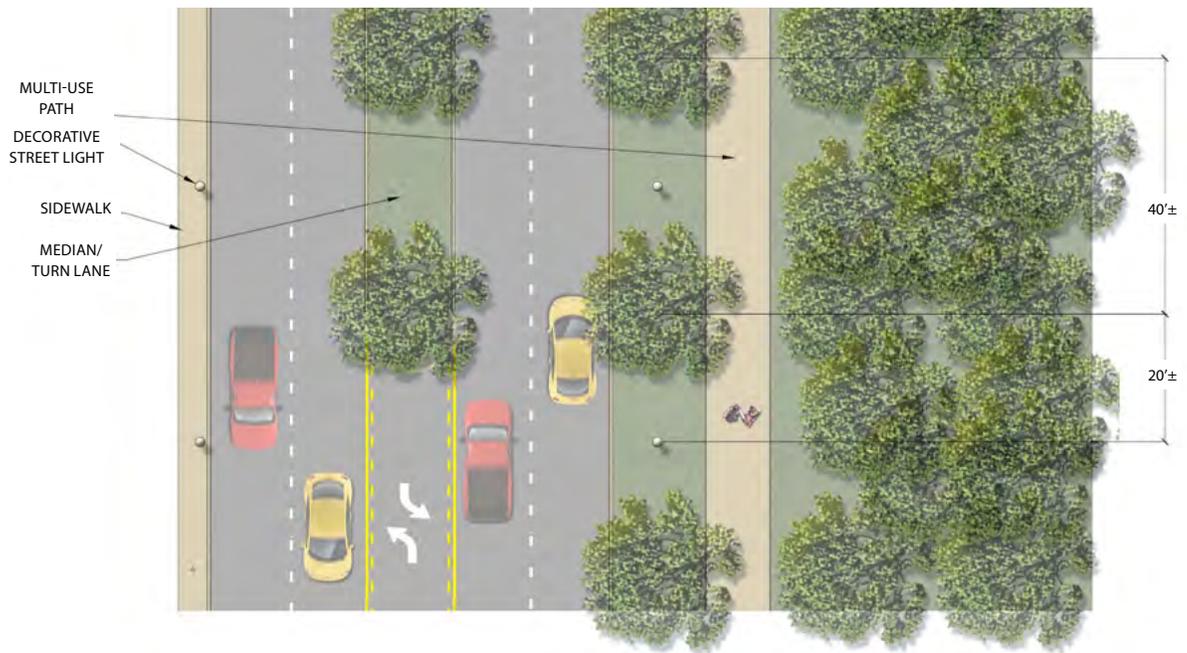
Streetscape Improvements

Robert Smalls Parkway (SC 170) extends southwest from Boundary Street into Beaufort County and provides the most direct connection from Beaufort to Hilton Head Island and Savannah, Georgia. This high speed regional corridor has no fewer than five lanes of vehicular traffic for its entire length. Although sidewalks are included along much of the corridor, there is no planting strip to

separate pedestrians from fast-moving vehicles, and crosswalks are almost nonexistent.

The Civic Master Plan recommends the installation of an off-street multi-use path with a wide landscaped separation from vehicular travel lanes in order to provide a comfortable, safe and beautiful option for pedestrians and cyclists. This multi-use path would connect to the Spanish Moss Trail at Beaufort Plaza (see 8.1 for details), where the path would take a more urban form as a treed sidewalk alongside shop fronts. Moving away from Beaufort Plaza, the path would transition into preserved forest areas alongside Robert Smalls Parkway, retaining the existing mature trees for future generations while allowing development opportunities as well.

Robert Smalls Parkway is the most significant obstacle for users of Spanish Moss Trail to cross because of the speed and volume of traffic. This plan proposes a pedestrian bridge to provide a gateway



▲ CONCEPTUAL ROBERT SMALLS PARKWAY STREETScape IMPROVEMENTS

to the Beaufort Plaza area and allow trail users to easily navigate an otherwise dangerous intersection. (Conceptual illustrations of the pedestrian bridge are below). An alternative paving treatment beneath the bridge would define a long plaza, and mixed-use buildings would activate the new civic space. The combination of the bridge and plaza would transform Robert Smalls Parkway from a potential obstacle into a major trailhead for the Spanish Moss Trail while also serving as a visual gateway into the Boundary Street area and Downtown Beaufort. (See 8.1 for details about how the pedestrian bridge ties into the transformation of Beaufort Plaza).

Redevelopment Strategies

This plan recommends that redevelopment along the Robert Smalls Parkway corridor focus new buildings that line the edges of existing parking lots to face the improved streetscape and multi-use path. This incremental infill would introduce new uses to the area that are currently lacking, especially housing. Civic sites would also be established in key locations – like the intersection of Robert Small Parkway and Parris Island Gateway – to provide focal points for development and encourage greater social interaction. These uses would gradually transform Robert Smalls Parkway into a more attractive, vibrant, mixed-use corridor.



▲ ILLUSTRATION OF PEDESTRIAN BRIDGE OVER ROBERT SMALLS PARKWAY - OPTION A



▲ ILLUSTRATION OF PEDESTRIAN BRIDGE OVER ROBERT SMALLS PARKWAY - OPTION B



▲ CONCEPTUAL ROBERT SMALLS PARKWAY CORRIDOR - CROSS CREEK INFILL ALTERNATIVE



▲ CONCEPTUAL PLAN OF ROBERT SMALLS VILLAGE VISION

A 100-Year Vision: Robert Smalls Village

One function of this Civic Master Plan is to envision Beaufort for generations to come if public investment projects today create both a sense of place and real economic value for the city. In the case of Robert Smalls Parkway, streetscape improvements and a commitment to traditional patterns of development could be the first steps toward a new community at the convergence of Parris Island Gateway and Robert Smalls Parkway.

A focus on developing a fine-grained street network coupled with a combination of preserving natural areas and encouraging infill development along renewed corridors would lay the groundwork for future Beaufortonians to prosper. The illustrative plan at left and the rendering to the right depict a long-term possibility of how Robert Smalls Parkway could one day evolve into Robert Smalls Village.



▲ CONCEPTUAL ROBERT SMALLS PARKWAY CORRIDOR AND BEAUFORT PLAZA



▲ CONCEPTUAL ILLUSTRATION OF ROBERT SMALLS VILLAGE VISION

8.4 Sea Island Parkway and Lady's Island Village Center

Sector: 5
Project Type: Private
Civic Investment Required: None
See Also: 2.7; 6.6

installing planting strips to buffer the sidewalks from vehicular travel lanes. In some areas, on-street parking may be appropriate to serve new development located at the edge of the sidewalk. A series of roundabouts is also proposed to calm traffic through the Lady's Island Village Center and create focal points for redevelopment.

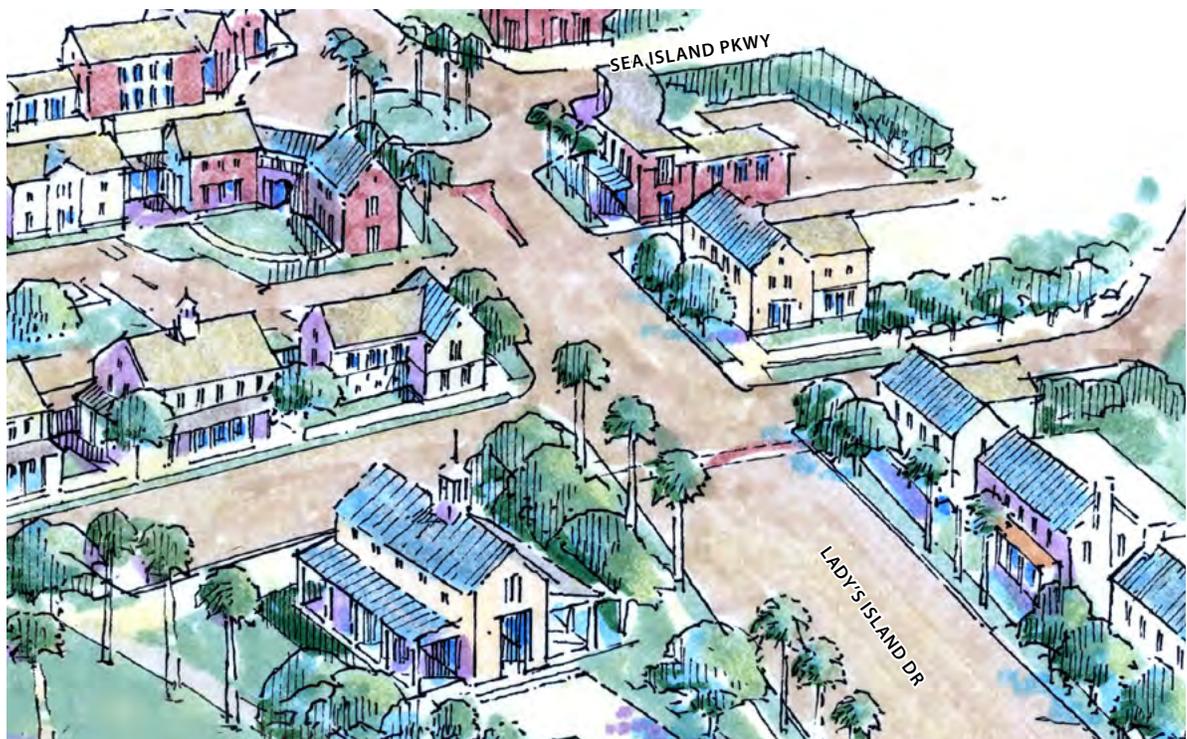
Redevelopment Strategies

The conceptual plan for Lady's Island Village Center focuses on creating a more connected and coherent pattern of circulation and reinforcing the streetscape with walkable development that defines a consistent street edge. Beaufort High School continues to be a prominent, yet secluded civic presence in this area. The proposed plan for the Whitehall area and Hamilton Village seek to complement the mixed-use walkable character of downtown Beaufort by echoing this pattern directly across the Beaufort River. (See 6.6 for more detail about each of these areas.)

Streetscape Improvements

As Carteret Street extends south from downtown Beaufort across the Beaufort River, it becomes Sea Island Parkway, the primary thoroughfare to Lady's Island, St. Helena Island and Hunting Island. Sea Island Parkway is primarily a four-lane rural thoroughfare with scattered sidewalks in the more developed areas near the Beaufort River.

Proposed streetscape improvements are limited to the Lady's Island Village Center area located west of the Beaufort County Airport; the areas beyond this point quickly transition to a rural road with sparse development and little need for additional infrastructure. Along Sea Island Parkway, this plan recommends widening sidewalks and



▲ CONCEPTUAL ILLUSTRATION OF LADY'S ISLAND VILLAGE CENTER



▲ EXISTING STRIP CENTER

Building Redevelopment Strategies

As streetscape improvements create redevelopment opportunities, there will not be one building type or scale that is appropriate for all locations. The two illustrations below show options for how the strip center at left could be redeveloped to create a more pedestrian-friendly destination. The multi-story building option frames the street and allows for vertical mixing of uses: retail on the ground floor and offices or housing above. Multi-story buildings will not be economically viable in all locations, and the one-story building option shows a configuration that establishes a similarly attractive place that may be more appropriate for some locations, such as at mid-block.



▲ ONE-STORY BUILDING OPTION



▲ MULTI-STORY BUILDING OPTION



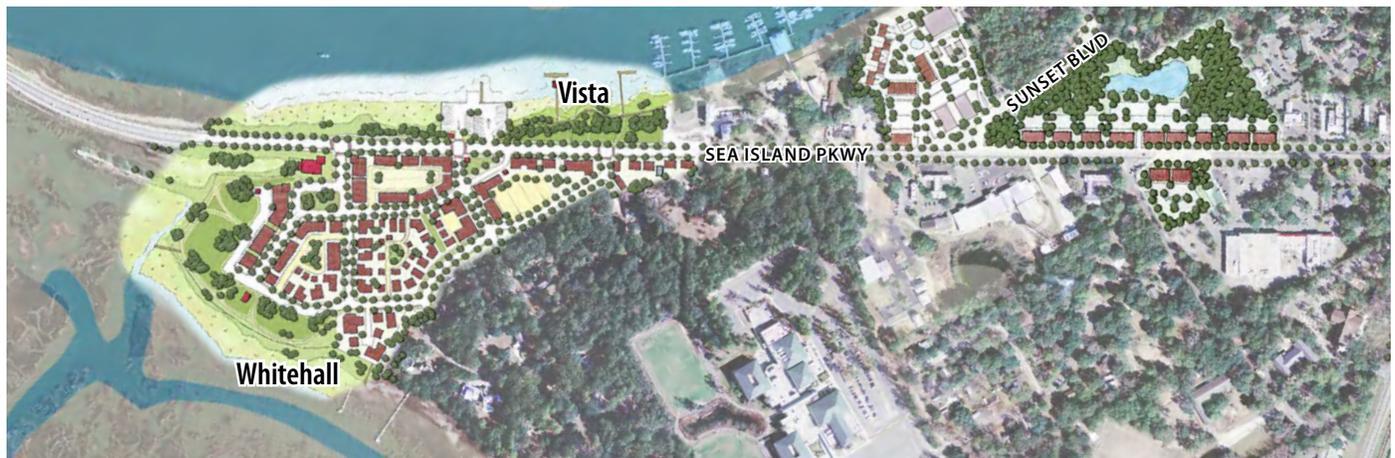
▲ CONCEPTUAL ILLUSTRATION OF LADY'S ISLAND HOUSING INFILL



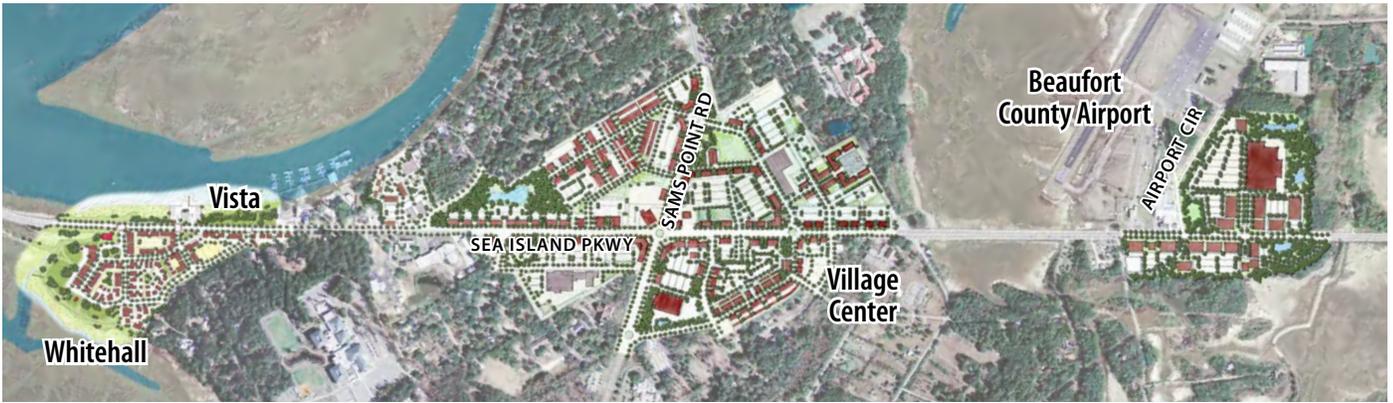
▲ CONCEPTUAL PLAN OF LADY'S ISLAND DEVELOPMENT - PHASE I



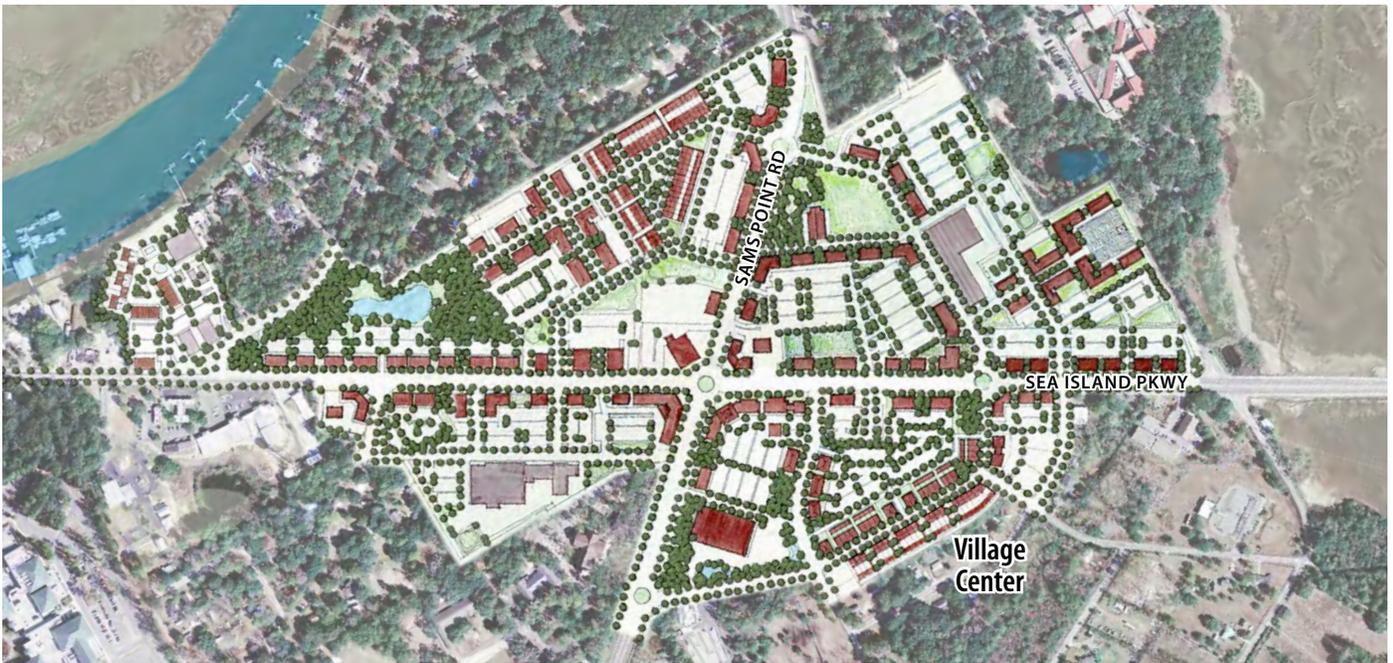
▲ CONCEPTUAL PLAN OF LADY'S ISLAND DEVELOPMENT - PHASE I - VILLAGE CENTER



▲ CONCEPTUAL PLAN OF LADY'S ISLAND DEVELOPMENT - PHASE I - WHITEHALL AND VISTA



▲ CONCEPTUAL PLAN OF LADY'S ISLAND DEVELOPMENT - PHASE 2



▲ CONCEPTUAL PLAN OF LADY'S ISLAND DEVELOPMENT - PHASE 2 - VILLAGE CENTER



▲ CONCEPTUAL PLAN OF LADY'S ISLAND DEVELOPMENT - PHASE 2- AIRPORT JUNCTION PUD



DISTRICTS FOR ECONOMIC DEVELOPMENT ACTIVITY



A strong, vibrant, and healthy economy will be achieved through a successful economic development program in order to ensure the long term success and viability of the City of Beaufort. We must support the continuation and expansion of our primary economic engines – tourism, the military, healthcare, and education – while also seeking to expand opportunities for the arts and the recruitment of creative- and knowledge-based industries.

PRINCIPLES

- 1: THE ATTRACTION AND CREATION OF JOBS THAT EXPORT GOODS AND SERVICES OUTSIDE OF OUR REGION IS CRITICAL TO OUR LONG-TERM ECONOMIC STRATEGY

- 2: JOB CREATION IN BEAUFORT WILL BE BEST SERVED BY RESERVING STRATEGIC SITES AND THROUGH THE ENHANCEMENT OF OUR ENTIRE COMMUNITY TO MAKE US AN ATTRACTIVE LOCATION

- 3: WE WILL CONTINUE TO WORK WITH OUR PARTNERS – THE MILITARY, BEAUFORT MEMORIAL HOSPITAL, TECHNICAL COLLEGE OF THE LOWCOUNTRY, AND THE UNIVERSITY OF SOUTH CAROLINA-BEAUFORT – TO FIND INNOVATIVE WAYS IN WHICH TO LEVERAGE THEIR PRESENCE AND RESOURCES TOWARDS DEVELOPMENT THAT ATTRACTS A SUSTAINABLE JOB BASE

- 4: BEAUFORT WILL SEEK TO OFFER A MYRIAD OF BUILDING AND DEVELOPMENT OPTIONS FOR JOB CREATION – FROM SUBURBAN BUSINESS PARKS AND LOW-SCALE BUILDINGS, TO LARGE-SCALE SITES FOR CAMPUS BUILDINGS, TO MORE URBAN PATTERNS THAT ARE INTEGRATED INTO THE CITY’S WALKABLE, MIXED-USE FABRIC

- 5: PARCELS IDENTIFIED FOR JOB CREATION LOCATED IN THE AIR INSTALLATION COMPATIBLE USE ZONE (AICUZ) WILL BE CONSIDERED FOR LOW-DENSITY, LOW-SCALE COMMERCIAL AND MANUFACTURING OPERATIONS ONLY

KEY INITIATIVES

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9.4 Strategic Opportunity Sites	191



Definitions

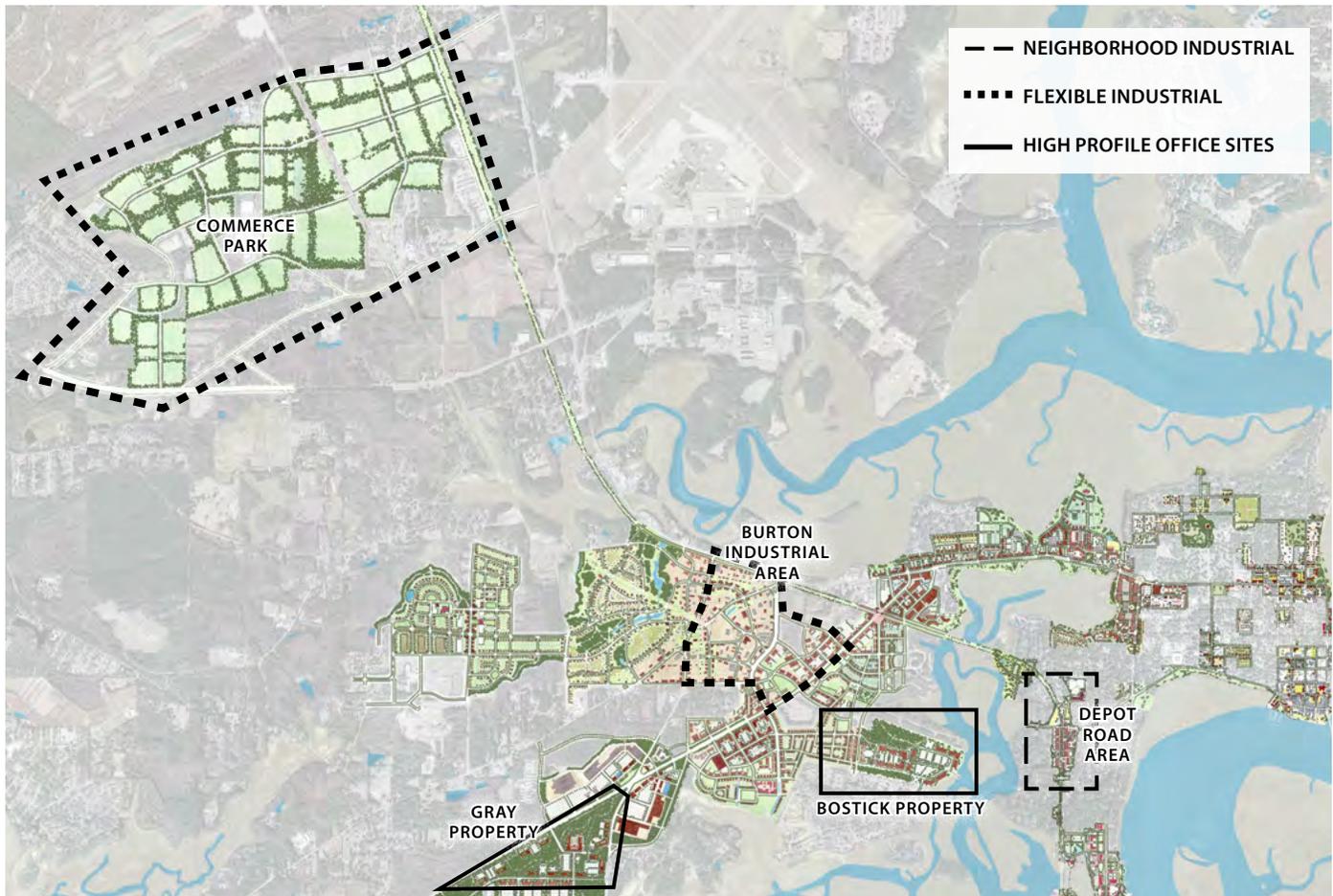
Neighborhood Industrial: The assembly, fabrication, production or processing of goods and materials using processes that ordinarily do not create noise, smoke, fumes, odors, glare, or health or safety hazards outside of the building and are visually undifferentiated from an office building or a residentially-scaled garage. These typically involve the work of artisans, craftspeople or small service operations that are wholly contained within the building but perhaps have a storage/loading bay for smaller shipping and deliveries on single-axle trucks.

Flexible Industrial: A primarily utilitarian structure that accommodates the manufacturing, assembling, finishing, cleaning or developing of any product or commodity. Such sites and buildings typically must accommodate large truck deliveries on a frequent basis and have storage/loading bays. Additionally, they may have external storage for raw materials and various related equipment as well as related office space.

High Profile Office Sites: Locations along major corridors with good visibility and accessibility to attract large office users in a multi-story, large floor plate, corporate office format.

Industrial complexes, manufacturing facilities and employment campuses are important economic engines and job creators. Depending on the expected scale and intensity, these uses may be accommodated within walkable urban settings or within large commercial/industrial complexes. Typically these facilities are most successful near major thoroughfares, within receptive development context, and adjacent to other synergetic uses.

The Civic Master Plan identifies five areas within the City of Beaufort, representing a wide range of development contexts, that are especially well-equipped to serve these employment-based uses: Depot Road Area; Commerce Park; Burton Industrial Area; and properties owned by the Gray and Bostick families.



▲ POTENTIAL ECONOMIC DEVELOPMENT LOCATIONS

9.1 Depot Road Area

Sector: 2

Project Type: Public | Private

Civic Investment Required: Depot Plaza,
Depot Road Streetscape

See Also: 4.9; 5.2

Since the Port Royal Railroad was deactivated in 2006, the Beaufort Depot Area has become a neglected “back of house” district squeezed between residential neighborhoods. Beneath its rough character, the Depot Road Area continues to support a number of successful light industrial and service operations. Additionally, the recent opening of the Spanish Moss Trail, running through the Depot building itself, has already begun to bring a new level of activity to the area with residents of all ages walking, biking, and rolling through daily (see Section 5.2 for details).

Over time, there is an opportunity to build on both the Depot Area’s eclectic industrial past and the Spanish Moss Trail’s new infusion of energy in a manner that supports the immediate neighborhoods and the greater community. Many other aging industrial areas around the country that

are intersected by major trail systems—such as the Swamp Rabbit Trail in Greenville, South Carolina; the South End Rail-Trail in Charlotte, North Carolina; the Tobacco Trail in Durham, North Carolina; the Libba Cotten Bikeway in Carrboro, North Carolina; and the Springwater Corridor in Portland, Oregon—have seen a tremendous level of new life and value creation as a result. For example, recent studies in Greenville, South Carolina have indicated that businesses along the trail have seen a 30-85% increase in sales/revenue (<http://greenvillerec.com/studies-surveys>) directly attributable to the trail’s proximity indicating a substantial economic impact. This fiscal success is connected to the creation of new neighborhood amenities, such as the Swamp Rabbit Café and Grocery. This bakery, coffee shop and purveyor of locally-sourced food re-established a previously abandoned building adjacent to the trail in an older neighborhood two miles from the downtown and created a new hub of activity.

To this end, this plan anticipates similar opportunities and interest in uses and activities in the Depot Area that are complementary to this rapidly-emerging trail corridor and are also compatible and protective of the surrounding neighborhood. One possibility includes the regeneration of the aging—but economically viable—industrial buildings as incubator spaces for small business startups, back office operations, and artists



Image Source: bing.com

▲ EXISTING DEPOT AREA



▲ CONCEPTUAL REDEVELOPMENT PLAN

and craftspeople (e.g. custom woodworkers, cabinet shops, ironworkers and other similar artisans) with small scale fabrication, storage, and sales. Like the Swamp Rabbit Trail and others like it across the country, it is reasonable to foresee demand for small-scale businesses and facilities to support trail users such as bicycle rentals, restrooms, and small grocers/sundries as well as infill housing in a live-work arrangement to take advantage of the trail as a regional amenity. Over time it is also reasonable to expect that demand may also support the construction of new structures that are consistent in scale with the existing buildings.

While these characteristics would make the Depot Area a prime opportunity for redevelopment, this plan does not anticipate major redevelopment in this area until a few key issues are resolved. First, the current haphazard parking configurations, partially addressed by the first phase of trailhead parking, must continue to be resolved. With an active trail through the area, it has become even more important to formalize the area where motor vehicles circulate and park so as not to conflict with trail users and the neighborhood. This discussion with all the stakeholders should begin as soon as possible.

Second, and perhaps most importantly, the current Limited Industrial zoning classification permits a wide variety of manufacturing and industrial uses which are not necessarily conducive to a quiet and prosperous neighborhood (e.g. aviation services, light industrial services, and even waste related services as a Special Exception) and does little in the way of creating an economic and social amenity for both the neighborhood and the community as a whole. The city also recognizes that many of the residents of the neighborhood surrounding the Depot Area have expressed concern about how this vision might impact the quiet nature of the area.

To accommodate this emerging vision, this plan recommends a discussion with Depot Area stakeholders and the community at large with the goal of creating a special zoning district that helps to manage some level of expected change. At a minimum it needs to anticipate trail-generated use and activity, differentiate it from the more conventional industrial areas elsewhere in the city, and ensure compatibility with the peaceful nature of the established neighborhoods. In consultation with stakeholders, the city will convene a group of interested parties within the next twelve months as new sections of the trail are completed.



▲ SWAMP RABBIT CAFE & GROCERY, GREENVILLE, SOUTH CAROLINA

9.2 Commerce Park

Sector: 4

Project Type: Public | Private

Civic Investment Required: Commerce Park
Public Infrastructure Improvements

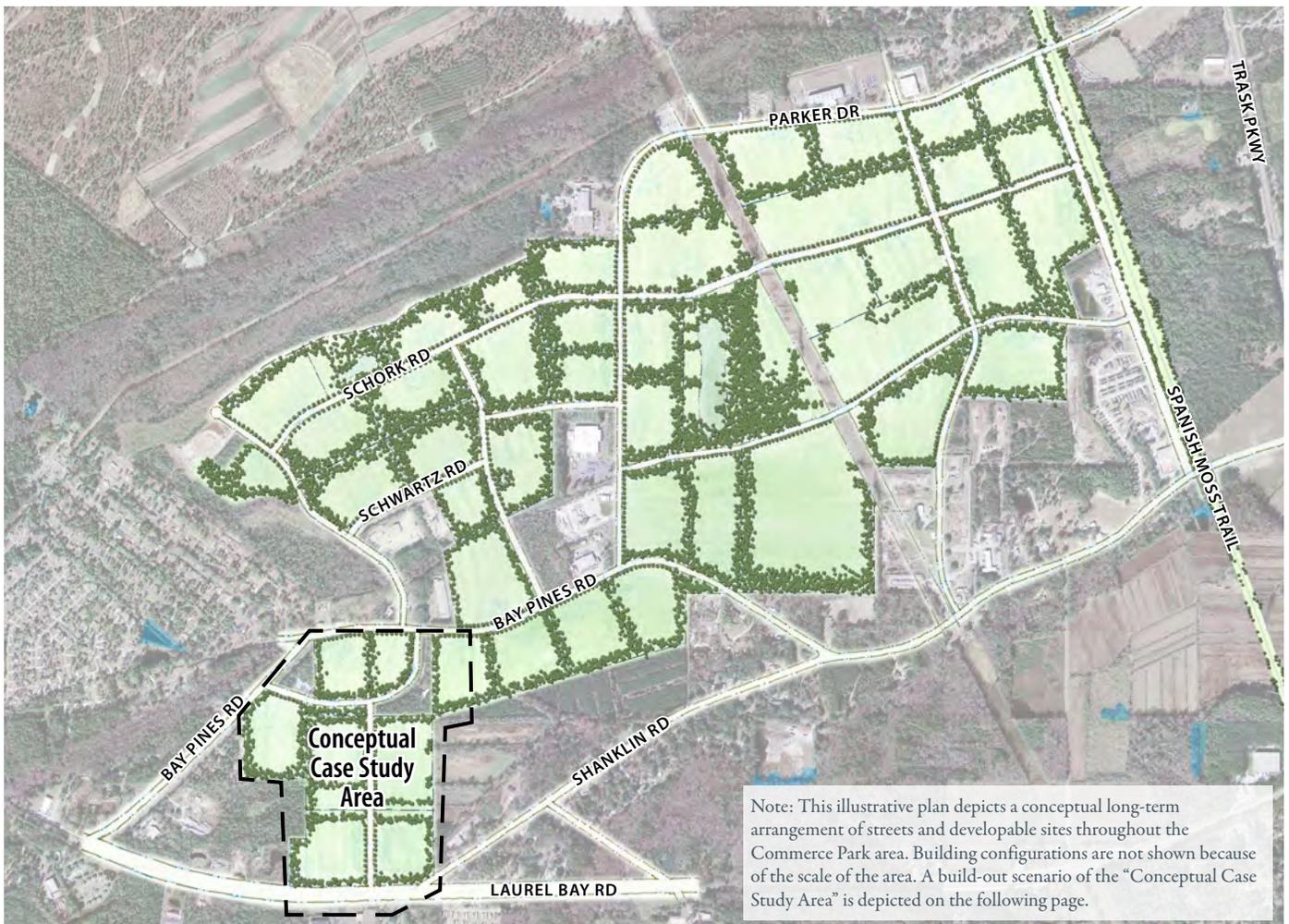
See Also: 7.7

Recognizing that not all development types are able to embody the mixed-use, walkable character that the Civic Master Plan seeks to create throughout the majority of the City of Beaufort, this plan proposes setting aside the existing Commerce Park to accommodate certain important employment centers that are more auto-oriented, require very large buildings, and benefit from an industrial park setting.



Image Source: bing.com

▲ EXISTING CONDITIONS



Note: This illustrative plan depicts a conceptual long-term arrangement of streets and developable sites throughout the Commerce Park area. Building configurations are not shown because of the scale of the area. A build-out scenario of the “Conceptual Case Study Area” is depicted on the following page.

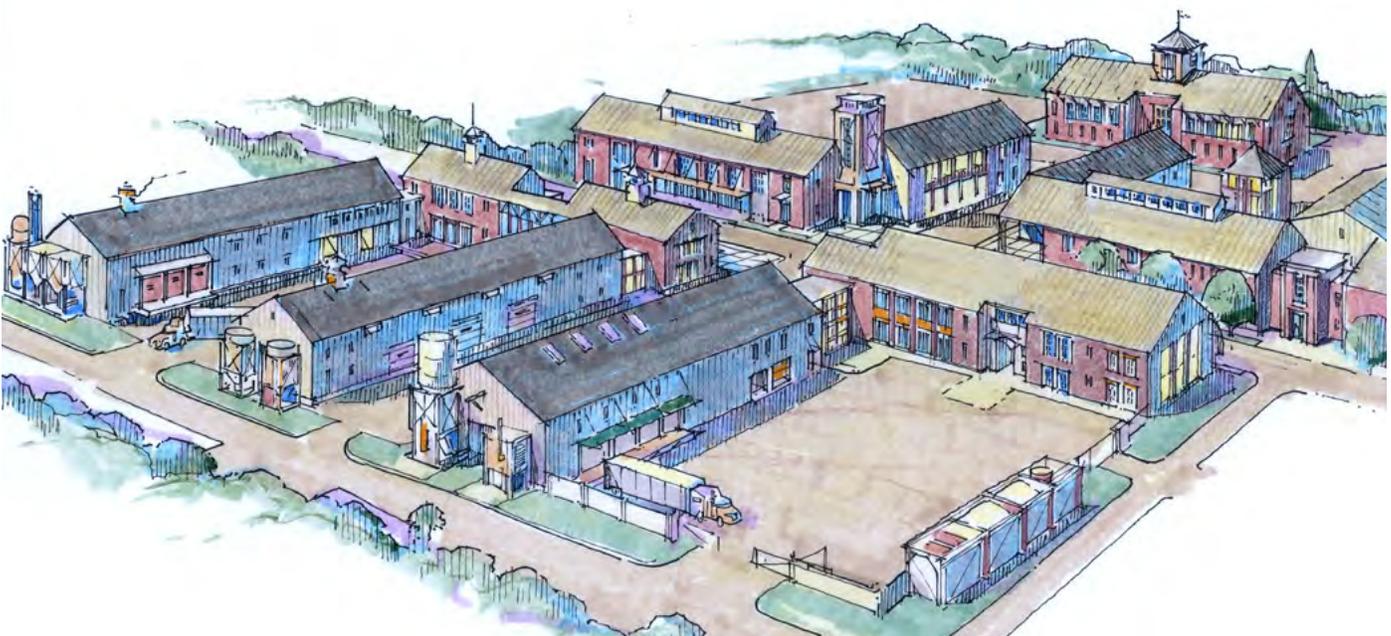
▲ CONCEPTUAL PLAN OF COMMERCE PARK



The Commerce Park is owned by the city, and ideally situated with direct access to US 21. The Civic Master Plan envisions that future Commerce Park development will accommodate large-scale commercial and industrial operations. The conceptual plan for the Commerce Park proposes a variety of lot sizes to be established in order to support a diverse group of tenants. Given the established access routes and large tracts of available land, the Commerce Park is well-suited for larger and more intensive industrial operations that are not necessarily appropriate in other parts of the city.

One limiting factor for development in the Commerce Park area is the presence of the Air Installation Compatible Use Zone (AICUZ) associated with the Beaufort Marine Corps Air Station. Commercial and industrial development is permitted within this zone, but with specific height restrictions and limitations on the number and concentration of people allowed within each facility. Future plans will be in accordance with the AICUZ regulations. (For more information, see Section 7.7).

▲ CONCEPTUAL CASE STUDY OF SMALL-SCALE, MULTI-TENANT, FLEX-INDUSTRIAL BUILDINGS



▲ CONCEPTUAL ILLUSTRATION OF FLEX-INDUSTRIAL DEVELOPMENT

9.3 Burton Industrial Area

Sector: 4

Project Type: Private

Civic Investment Required: None

See Also: 8.3

The Burton Industrial Area is located on the western edge of the City of Beaufort, in the area generally bound by Boundary Street, Robert Smalls Parkway and Parris Island Gateway. Currently, it is a disorganized collection of industrial flex spaces



Image Source: bing.com

▲ EXISTING CONDITIONS



▲ CONCEPTUAL PLAN OF BURTON INDUSTRIAL AREA

9: Districts for Economic Development Activity

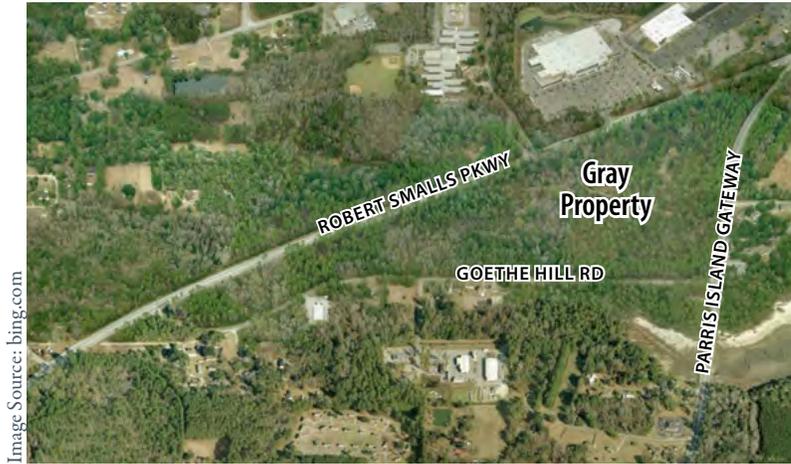


Image Source: bing.com

that lacks a connected street network. The access provided by bordering thoroughfares offer the potential for the continued development of this area as an industrial employment center.

To support future development, the Civic Master Plan proposes the creation of a more coherent and connected street network in this area. Burton Hill Road would become the primary focus for internal development, while Parris Island Gateway would create a prominent external edge for infill development. The scale of buildings would range between the neighborhood-scaled incubator buildings in the Depot Area and the large-scale industrial complexes at Commerce Park. In reality most industrial and manufacturing operations fall within this wide range of building scales, and it is anticipated that the Burton Industrial Area may accommodate a great diversity of uses and building types.



Image Source: bing.com

In contrast to the Commerce Park, the Burton Industrial Area is not limited by the presence of the Air Installation Compatible Use Zone (AICUZ) associated with the Beaufort Marine Corps Air Station, which means that this area offers more potential opportunities for mixed-use development.

▲ EXISTING CONDITIONS



▲ CONCEPTUAL PLAN OF GRAY PROPERTY

9.4 Strategic Opportunity Sites

Sector: 4

Project Type: Private

Civic Investment Required: None

The Gray Property along Robert Smalls Parkway (SC 170) and the Bostick Property adjacent to Battery Creek provide unique opportunities for large employment centers within the city.

Gray Property

The Gray Property is bound by Robert Smalls Parkway, Parris Island Gateway and Goethe Hill Road, and is about 100 acres in size. It has excellent accessibility to and visibility along Robert Smalls Parkway (SC 170).

The conceptual plan for this property includes several major corporate headquarters within an office campus setting and preserves generous amounts of the existing forested land on the site. Tucked amidst the tree canopy, large office buildings connected by a few entrance drives and parking areas, accommodate large numbers of

employees. Multi-use paths weave throughout the office campus and connect to the path along Robert Smalls Parkway and eventually to the Spanish Moss Trail. This non-motorized transportation network encourages employees to commute on foot or bicycle, despite the traditionally auto-oriented building arrangement of the office campus.

Bostick Property

The Bostick Property is an undeveloped peninsula of land located south of Beaufort Plaza on Salem Road, and the eastern edge of the property opens on Battery Creek. While it has excellent views of the creek and tidal marshes, the north and east sides of the property contain wetlands and are unsuitable for development. However, the highland provides an excellent site for development along Battery Creek.

The conceptual plan for this property proposes a large employment campus on the developable portion nearest the marsh. The nearby neighborhoods would offer a variety of housing types that may serve as workforce housing. This arrangement would allow residents to live within easy walking or biking distance to work, while offering the type of controlled office park atmosphere that many employers seek.



▲ CONCEPTUAL PLAN OF BOSTICK PROPERTY



REGULATORY FRAMEWORK



We will update our regulatory framework to support the precise block-by-block development recommendations established in the Civic Master Plan and which has been articulated by its citizens throughout the past decade of planning efforts: to implement the broad vision for a vibrant, sustainable Beaufort with high quality private investment and a robust, interconnected transportation system that accommodates pedestrians, bicyclists, transit and automobiles.

KEY INITIATIVES

10.1 Transect-Based Implementation Strategies .196
10.2 Street Infrastructure Plan205
10.3 Proposed Zoning Changes for the National
Historic Landmark District232



RURAL



URBAN

▲ RURAL-TO-URBAN TRANSECT

Image Source: DPZ/James Wassell

10.1 Transect-Based Implementation Strategies

A new Beaufort Code

One of the priorities for implementation of the “Vision Beaufort: 2009 Comprehensive Plan” (see Section 1.2 for more information) was to update the UDO to utilize more form-based code tools. As a first step, the City of Beaufort entered into a contract with Beaufort County in 2010 as part of a county-wide form-based code effort. The outcomes of this effort provided the overall framework for a future form-based code for the city.

Form-based codes differ from conventional zoning codes by regulating the built environment through a context-sensitive approach to the relationship between the public and private realms, rather than focusing chiefly on the segregation of different types of land use. Whereas the current UDO is organized by a range of single-use zoning districts that are residential, commercial, industrial or special-purpose, form-based codes are organized by the rural-to-urban transect.

The Rural-to-Urban Transect

A transect-based planning method was used throughout the Civic Master Plan process to establish a range of place types for Beaufort (see Section 1.4 for details.) The transect is a method of classifying the natural and built environments as a continuum of conditions, ranging from natural and rural lands to urban centers. When using development regulations to implement a planning vision, the transect groups compatible buildings, public spaces, and infrastructure by scale and intensity of use, while still allowing the development of neighborhoods where residents and workers can walk to work, school, and daily errands.

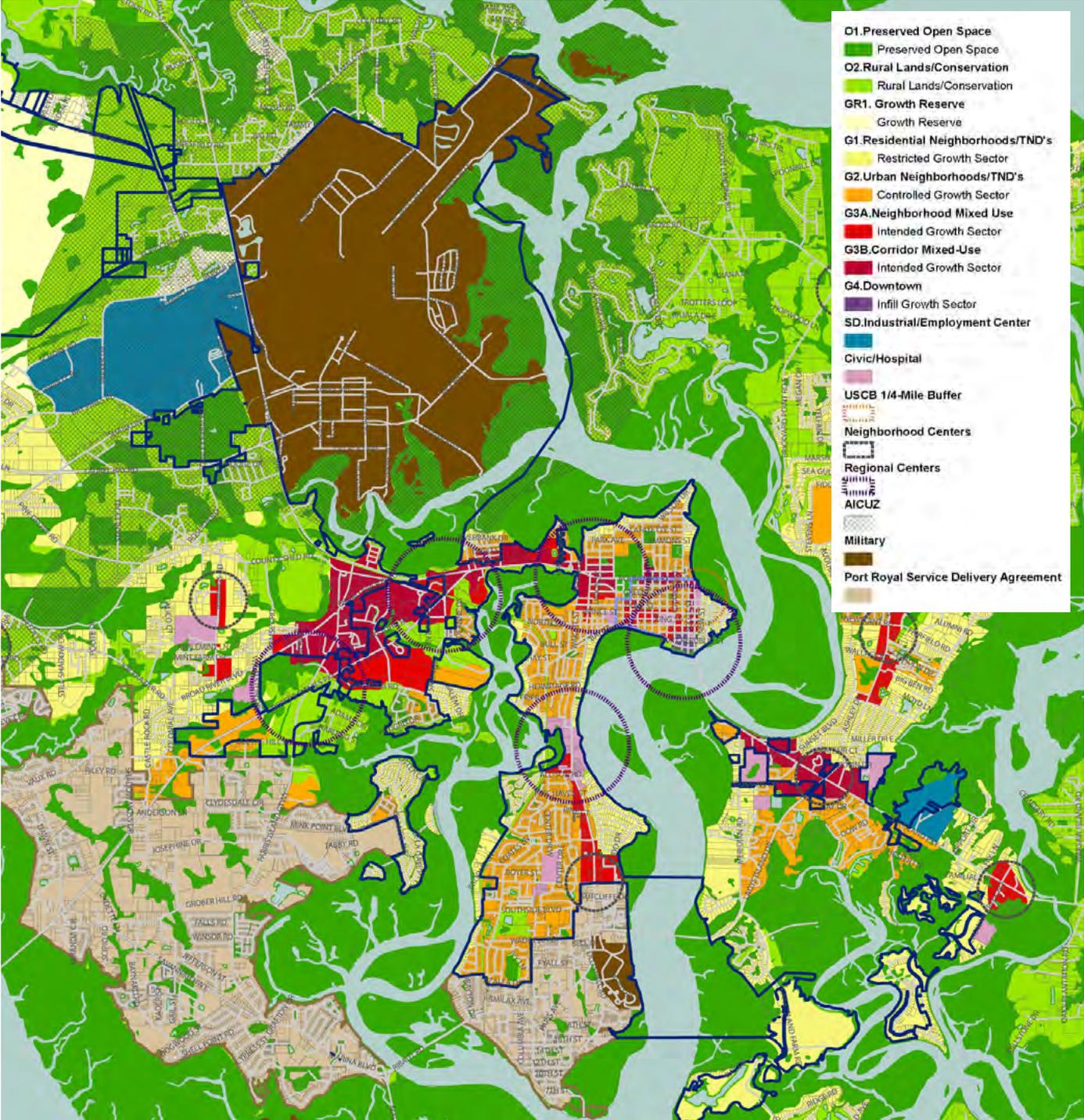
For example, a public space at the edge of the city may be a large regional park with hiking trails, while a public space in the city center may be a more intimate square where markets and events take place. Similarly, a residence near the edge may be on a large lot set back from the street, while a residence in the downtown may be a condominium in a four-story building.

Transect-Based Place Types

The Civic Master Plan establishes the framework for a future form-based code through four levels of development intensity. These levels within the transect range from T1-Natural Preserve at the low end to T5-Downtown Core/Urban Corridor at the high end. In a form-based code, these transect-based place types will translate directly to

form-based districts that regulate the mix of uses, building design, density, height, street design, the design of parks and open space, parking, and other components of the city fabric.

Additionally, these place types translate intuitively to the implementation of the Growth Framework Plan (from the Comprehensive Plan) that identifies areas for conservation and designates different



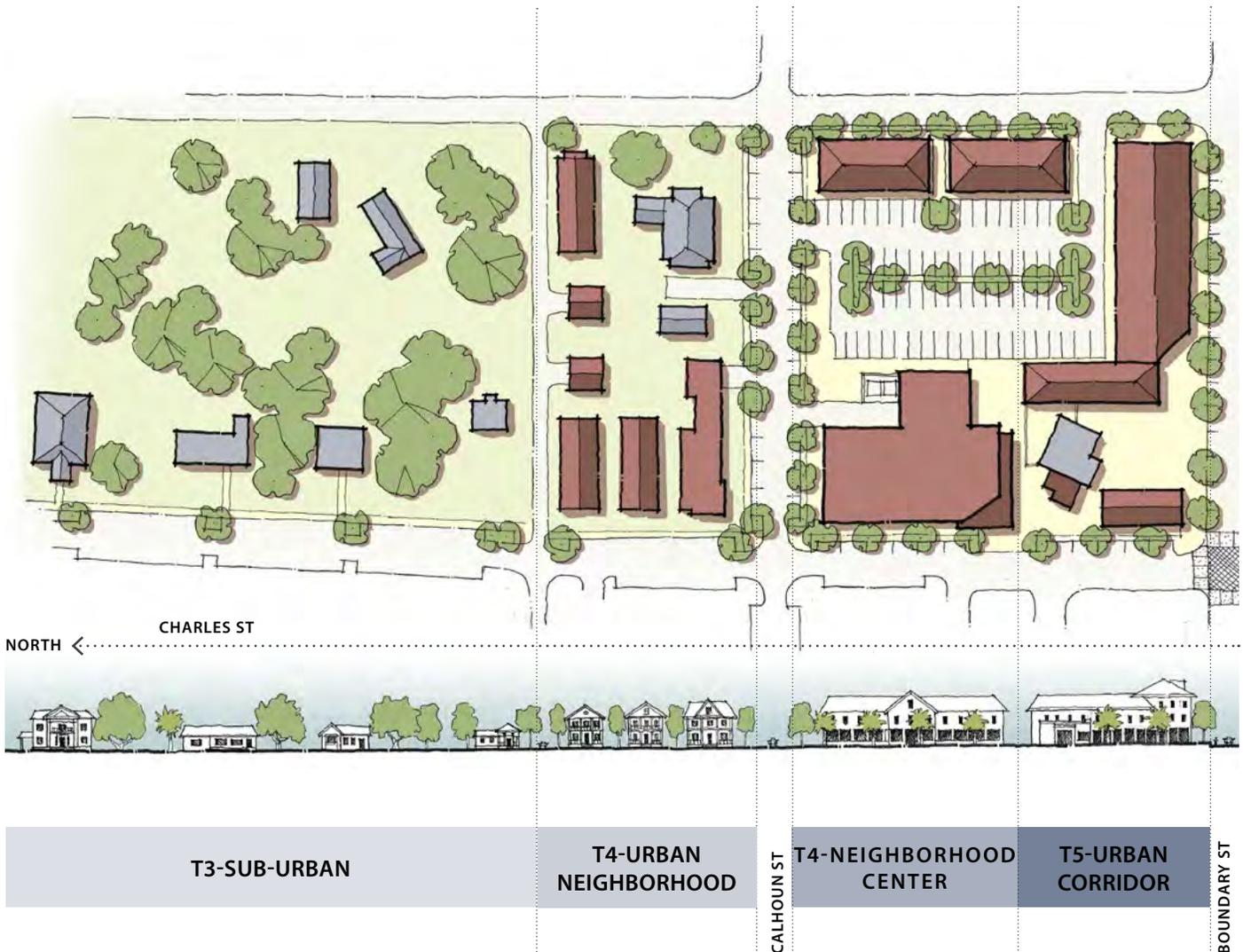
▲ GROWTH FRAMEWORK PLAN (FROM 2009 COMPREHENSIVE PLAN)

types of growth centers. By grouping compatible land uses – like corner stores and schools within residential neighborhoods – rather than strictly separating residential and non-residential areas, the application of transect-based place types will encourage the development of new mixed-use, walkable neighborhoods that complement Beaufort’s historic character.

The diagram along the bottom of these pages provides a sample application of how Transect-based place types can be applied to existing neighborhoods to shape future development. This particular example is taken from the section of Charles Street from Pigeon Point at the west end to Washington Street at the south end. Beginning at the Pigeon Point section, this area exemplifies the sort of lower-density residential development with larger street setbacks and

yards of the T3-Sub-urban place type. Moving south toward Boundary Street, the building height and lot coverage increase, stepping up the transect successively to T4-Urban Neighborhood, T4-Neighborhood Center, and then peaking at T5-Urban Corridor along Boundary Street. Further south beyond Boundary Street, the density steps back down the transect gradually to less-intense urban neighborhood areas, characterized by T4-Neighborhood Center and T4-Urban Neighborhood.

The Charles Street example does not demonstrate the full range of the rural-to-urban transect, but one can imagine that if this example were extended along an axis to the north, the Pigeon Point neighborhood would be generally T3-level development, with the last layer of development—larger lots that overlook the marsh – classified



▲ SAMPLE APPLICATION OF TRANSECT-BASED PLACE TYPES ALONG CHARLES STREET

as T3-Edge. In general, preserved open spaces along the marsh will be classified as T1-Natural Preserve to preclude future development. T2-level development is generally considered rural areas that include working lands and farmhouse-type buildings, but since Beaufort does not have these types of land, the Beaufort Transect skips the T-2 level.

The following pages give detailed descriptions and imagery for each transect-based place type. (For more information about building types, see Section 6.1.)



T1–Natural Preserve

Natural Preserve is intended to preserve areas that contain sensitive habitats, open space, and limited agricultural uses. This place type typically does not contain buildings, with the exception of small civic buildings or interpretive centers.

Examples of Natural Preserve include Pigeon Point Park, Arthur Horne Nature Preserve, and the Spanish Moss Trail.



T2–Rural

Rural areas are made up of lands in an open, cultivated, or sparsely settled state. Planning for future development within the City of Beaufort does not include any rural place types, and inclusion of T2-Rural here is for illustrative purposes only.



Image Source: Beaufort County Planning Department

T3-Edge

The T3-Edge is at the fringe of the city where larger-lot residential areas meet adjacent waterways. While almost exclusively residential, civic and park functions are also complementary to the character within the T3-Edge.

Examples of T3-Edge include the houses and inns facing the water in The Bluff neighborhood and around the perimeter of Battery Point.



T3-Sub-Urban

T3-Sub-Urban is single-family residential in character with less development than other place types within the city. While almost exclusively residential, civic and park functions are also complementary to the character within T3-Sub-Urban place types.

Examples of T3-Sub-Urban include the Royal Oaks and Mossy Oaks neighborhoods.



T3–Neighborhood

T3-Neighborhood is residential in character, and includes a mixture of residential and civic uses. Residential units are an assortment of sizes including cottages, small houses, duplexes, village houses, and bungalow courts.

Examples of T3-Neighborhood include Higginsonville, Jerico Woods, and Dixon Village.



T4–Urban Neighborhood

T4-Urban Neighborhood is a fairly low-intensity, mixed-use district composed primarily of residential development. A wide range of building types exist in T4 Urban Neighborhood including, but not limited to, single-family houses, townhouses, bungalow courts, mansion houses, and live-work units.

Examples of T4-Urban Neighborhood include portions of Bladen Street and Charles Street.



T4–Historic Neighborhood

T4-Historic Neighborhood is a fairly low-intensity place type that encompasses Beaufort’s historic residential neighborhoods. It contains single and two-family houses that are characterized by generous front porches and historic architectural detailing.

Examples of T4-Historic Neighborhood include The Point and The Commons.



T4–Neighborhood Center

T4-Neighborhood Center is a medium-intensity, mixed-use place type composed primarily of attached, mixed-use development. A wide range of building types exist in T4-Neighborhood Center including, but not limited to, mansion houses, apartment buildings, mixed-use buildings.

Examples of T4-Neighborhood Center include portions of Carteret Street and Boundary Street near Bellamy Curve.



T5–Downtown Core

T5-Downtown Core consists of higher density, mixed-use buildings that accommodate retail, offices, and apartments located in the historic downtown area. A tight network of streets lined with shopfronts defines this district as a very walkable area. Buildings are set very close to the street in order to define the public realm.

T5-Downtown Core is exemplified by the downtown area around Bay Street and Carteret Street.



T5–Urban Corridor

T5-Urban Corridor consists of taller, higher density, mixed-use buildings that accommodate retail, offices, and apartments located along primary thoroughfares. A tight network of streets lined with shopfronts defines this district as a highly walkable area. Buildings are set very close to the street in order to define the public realm.

Examples of T5-Urban Corridor include portions of Boundary Street, including Beaufort Town Center.



10.2 Street Infrastructure Plan

See Also: Chapter 5 and Chapter 8

The primary goal of the Street Infrastructure Plan is to create a street network which offers safe and convenient access for all users (pedestrians, bicyclists, drivers and transit riders) of all ages and abilities.

The following pages outline street standards that promote a complete street system by tailoring street design to its intended users and development context. For example, it would be unnecessary and inappropriate to construct a commercial main street in a remote, rural area, just as it would be inappropriate to build a dirt road in the middle of downtown.

Typical Street Sections

During the synoptic survey and charrette processes, the defining characteristics of Beaufort's streets were documented within a wide range of neighborhood types. From these observations, several typical street designs that capture the unique vernacular of Beaufort's streets have been identified.

The Street Infrastructure Plan applies specific expectations for streetscape characteristics to every street segment in the city. Each street type identified on the Street Infrastructure Plan maps corresponds to a street section diagram on

the following pages that establishes the specific standards for that street.

Specific Street Sections

In certain instances, portions of streets have been marked as "Other" on the Street Infrastructure Plan maps. These street segments have been specifically designed through the Civic Master Plan process with details that deviate from the typical street sections to respond to specific environmental, neighborhood, or development opportunities. Each of these specific street sections are included in this chapter following the typical street sections that apply to most streets throughout Beaufort.

In some cases, a specific street section may apply only to a portion of a single street. In other cases, a specific street section may apply to multiple streets. The applicability of each specific street section is detailed in a corresponding table.

Overhead Utilities

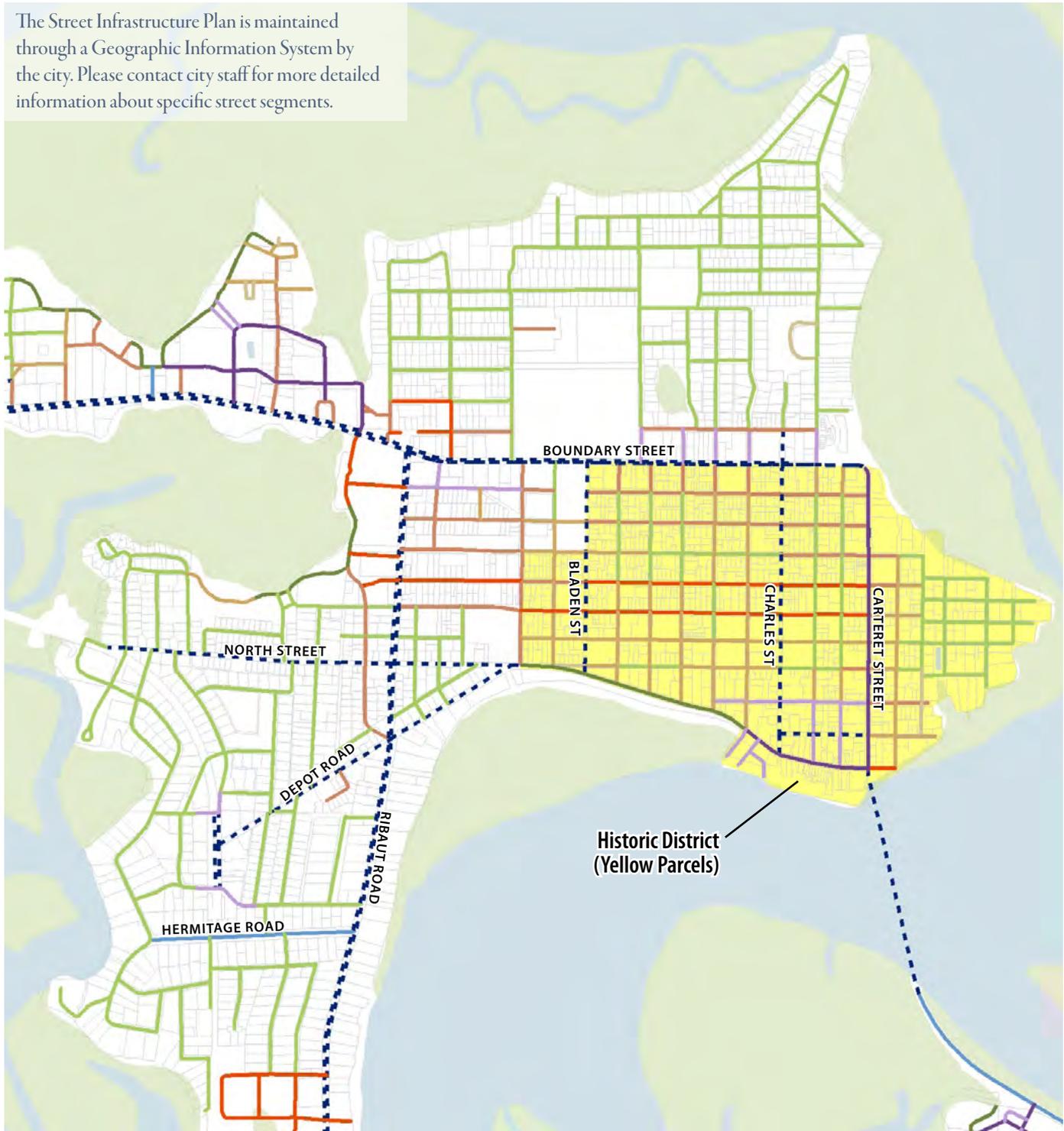
It is the intention of this plan to continue the city's pursuit of the burying of overhead utilities as a part of each street improvement project where funding permits. Where lines cannot be buried, the city will continue to work with the utility providers to replace or eliminate dead lines, unused equipment, and aging poles to improve the overall aesthetics of the entire city.

Military Roads

Some streets have been identified as "Military Roads" in the Street Infrastructure Plan. These streets have been left to the discretion of their associated military authority.



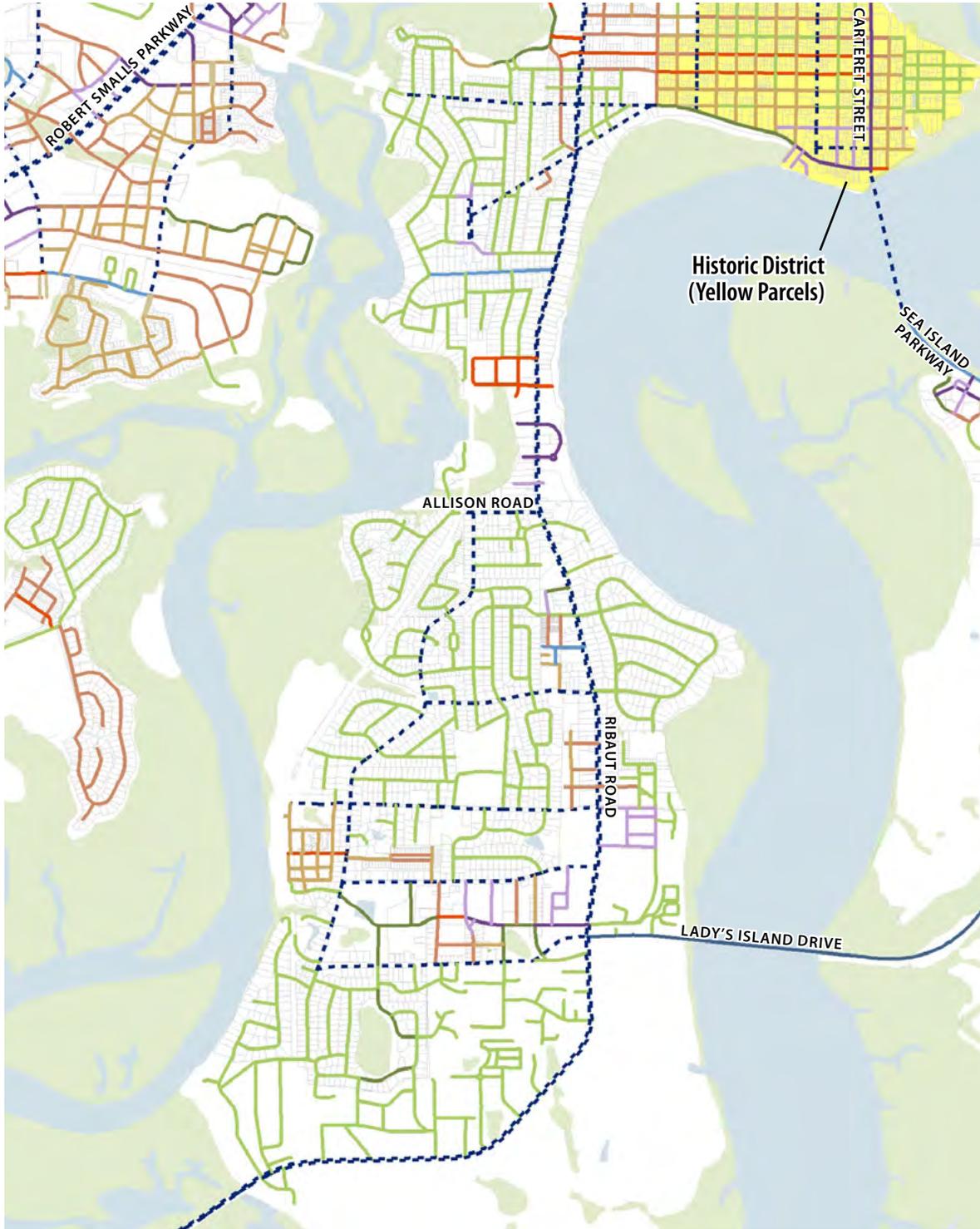
The Street Infrastructure Plan is maintained through a Geographic Information System by the city. Please contact city staff for more detailed information about specific street segments.



▲ STREET INFRASTRUCTURE PLAN SECTOR 1

LEGEND

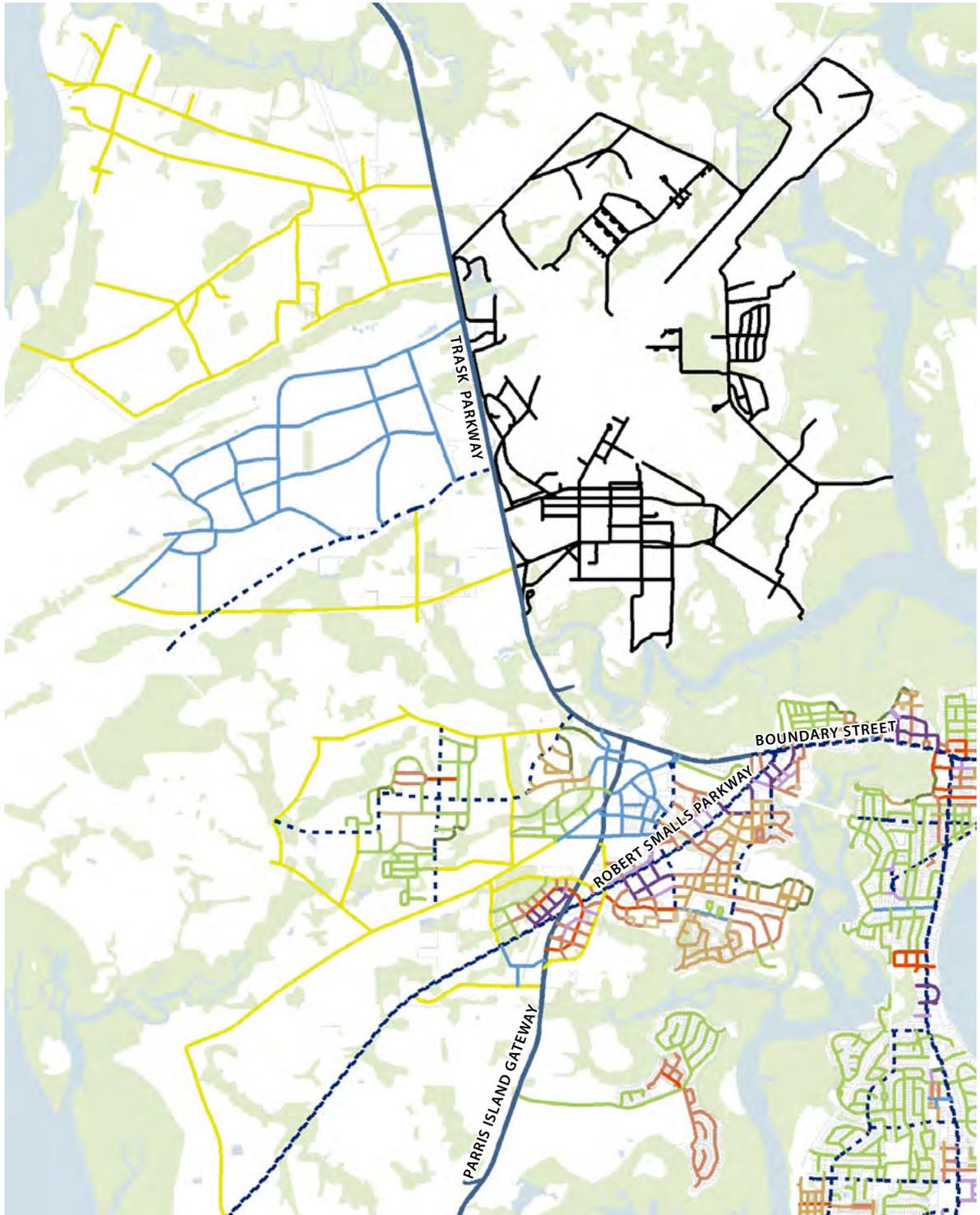
- | | | |
|---------------------------|-----------------------------------|-----------------------------------|
| — Major Thoroughfare | — Avenue | — Parkway |
| — Boulevard | — Neighborhood Street 1 - General | — Low Impact Development Road |
| — Main Street 1 - Primary | — Neighborhood Street 2 - Yield | — Rural Road |
| — Main Street 2 - Limited | — Neighborhood Street 3 - Lane | — Military Roads |
| — Commercial Alley | — Rear Lane | — Other (specific street section) |



▲ STREET INFRASTRUCTURE PLAN SECTORS 2 & 3

LEGEND

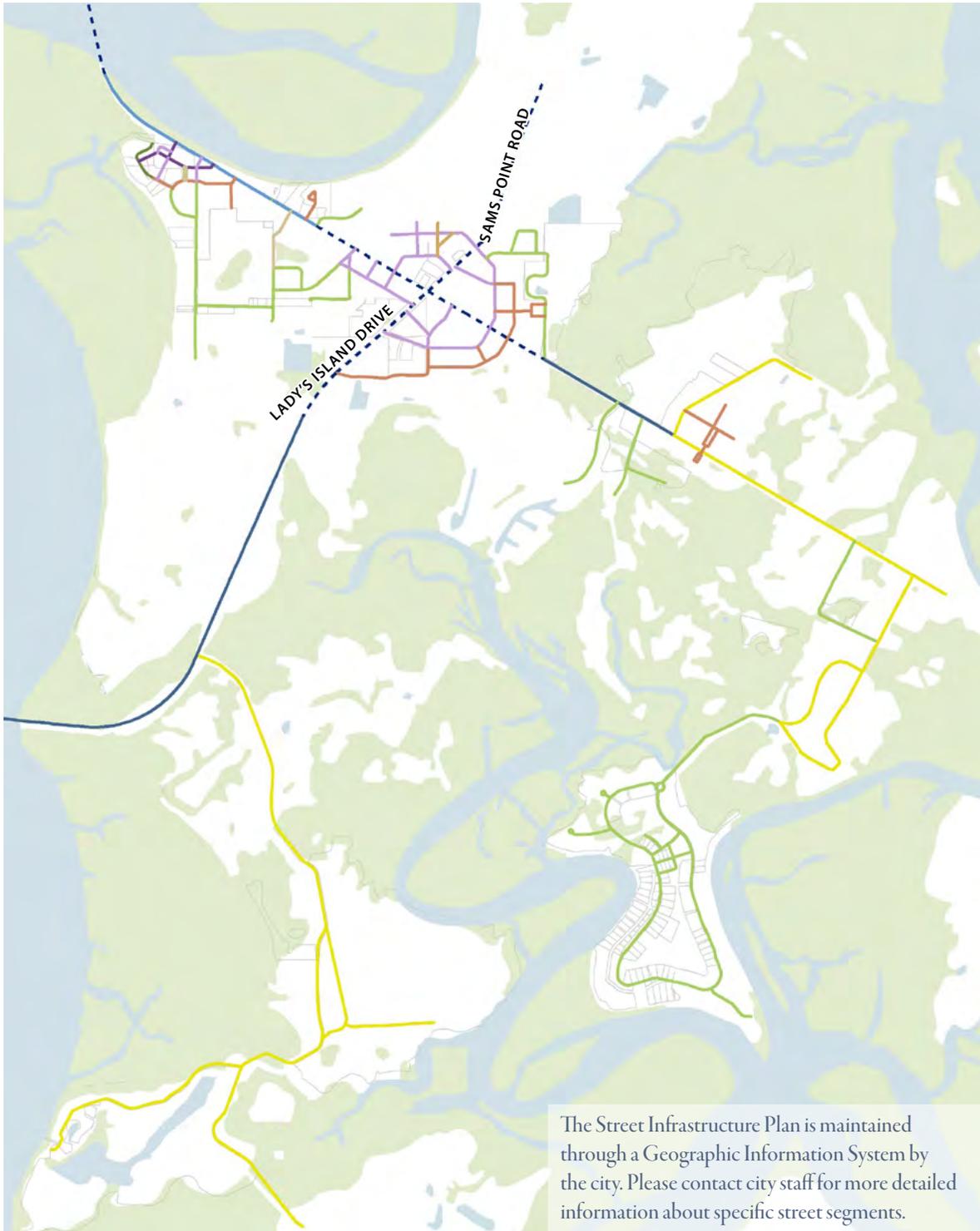
Major Thoroughfare	Avenue	Parkway
Boulevard	Neighborhood Street 1 - General	Low Impact Development Road
Main Street 1 - Primary	Neighborhood Street 2 - Yield	Rural Road
Main Street 2 - Limited	Neighborhood Street 3 - Lane	Military Roads
Commercial Alley	Rear Lane	Other (specific street section)



▲ STREET INFRASTRUCTURE PLAN SECTOR 4

LEGEND

- | | | |
|---------------------------|-----------------------------------|-----------------------------------|
| — Major Thoroughfare | — Avenue | — Parkway |
| — Boulevard | — Neighborhood Street 1 - General | — Low Impact Development Road |
| — Main Street 1 - Primary | — Neighborhood Street 2 - Yield | — Rural Road |
| — Main Street 2 - Limited | — Neighborhood Street 3 - Lane | — Military Roads |
| — Commercial Alley | — Rear Lane | — Other (specific street section) |



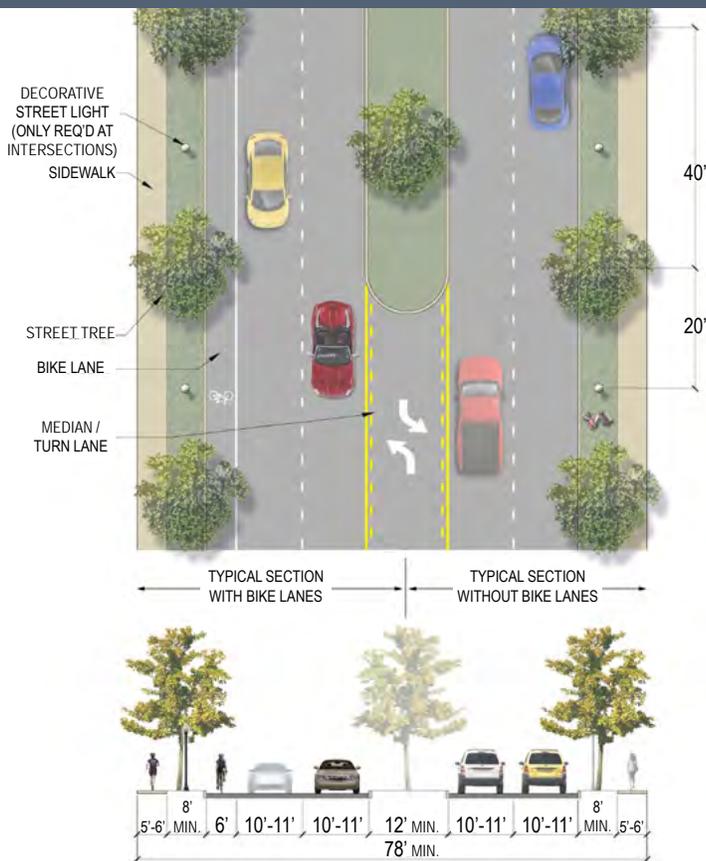
The Street Infrastructure Plan is maintained through a Geographic Information System by the city. Please contact city staff for more detailed information about specific street segments.

▲ STREET INFRASTRUCTURE PLAN SECTOR 5

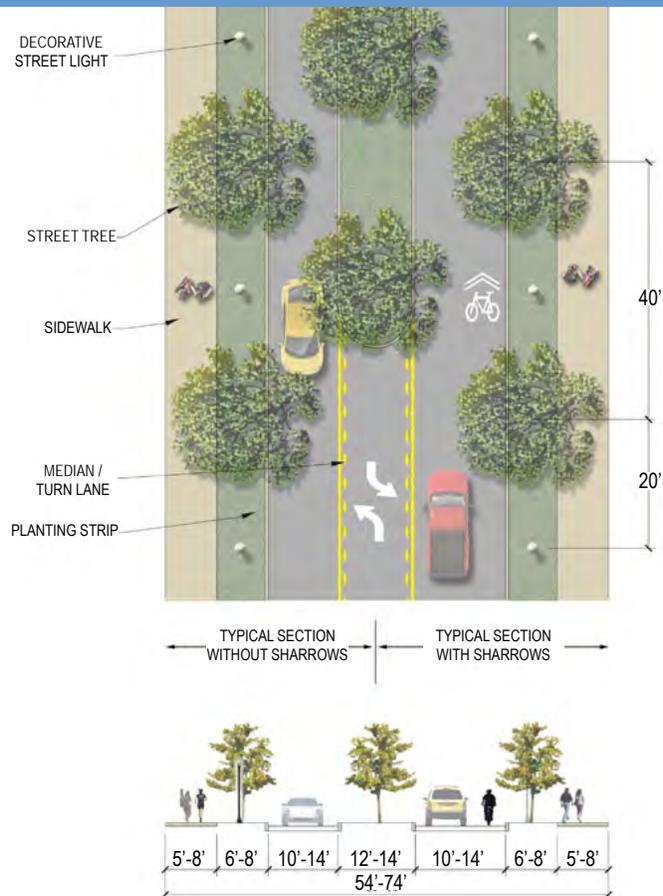
LEGEND

- | | | |
|---------------------------|-----------------------------------|-----------------------------------|
| — Major Thoroughfare | — Avenue | — Parkway |
| — Boulevard | — Neighborhood Street 1 - General | — Low Impact Development Road |
| — Main Street 1 - Primary | — Neighborhood Street 2 - Yield | — Rural Road |
| — Main Street 2 - Limited | — Neighborhood Street 3 - Lane | — Military Roads |
| — Commercial Alley | — Rear Lane | — Other (specific street section) |

Major Thoroughfare (MT: 78 ft +)	
Description	The primary purpose of this street type is to facilitate the movement of cars between regional destinations. As such, the application of this street type is limited to a few major corridors in the rural areas of Beaufort.
Curb Type	Curb or open swale
Right-of-Way Width	78 feet minimum
Traffic Lanes (Pavement Width)	4 lanes (10 to 11 feet each) & Center turn lane/median (12 feet min.)
Movement (Design Speed)	Limited Access (45 MPH)
Parking Lanes (Width)	N/A
Bike Facilities	Bike lanes (6 feet each) as needed
Sidewalk (Width)	2 sides (5 to 6 feet each)
Planter Type (Width)	Planting strip (8 feet minimum) & Planted median (12 feet minimum)
Street Trees	40 feet on-center average
Specific Applicability	See Street Regulating Plan



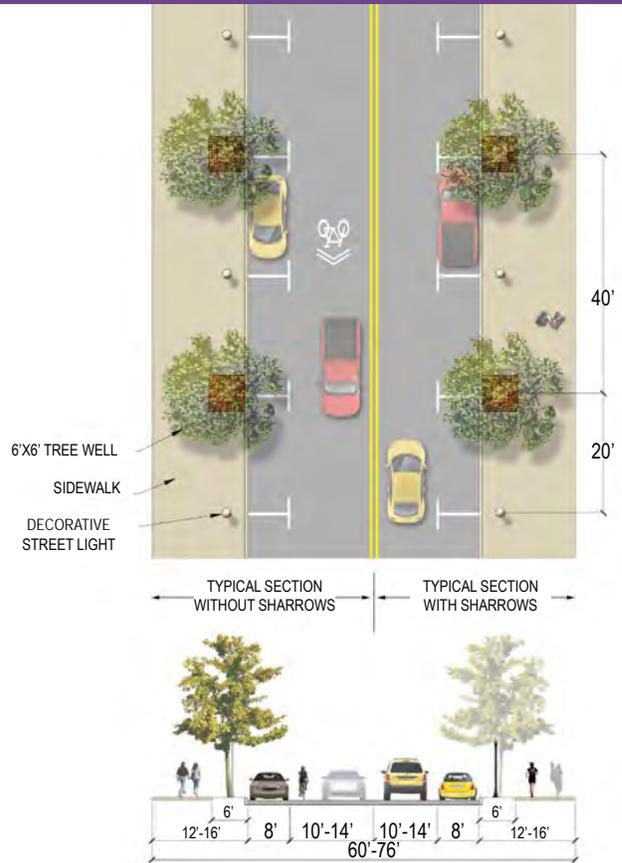
Boulevard (BV: 54 ft - 74 ft)	
Description	This street type is most commonly applied to neighborhood connectors where on-street parking is not necessary and at entrances to residential neighborhoods. It can be adapted to both urban and suburban conditions depending on the access needs of fronting properties. This street type is also appropriate for use in industrial areas and is applied throughout the Commerce Park area of Beaufort.
Curb Type	Curb
Right-of-Way Width	54 to 74 feet
Traffic Lanes (Pavement Width)	2 lanes (10 feet each OR 14 feet each w/ sharrows)
Movement (Design Speed)	Moderate (30 MPH)
Parking Lanes (Width)	N/A
Bike Facilities	Sharrows as needed (14 feet shared lanes)
Sidewalk (Width)	2 sides (5 to 8 feet each)
Planter Type (Width)	Planting strip (6 to 8 feet) & Planted median (12 to 14 feet)
Street Trees	40 feet on-center average in planting strips and median
Specific Applicability	For Heritage Road, the only proposed improvements are the installation of sharrow markings. For other usage of this section, please refer to the Street Regulating Plan.



Main Street 1 - Primary (MS1: 60 ft - 76 ft)

Description	This street type is intended to serve Beaufort's Main Street corridors. These streets are designed to accommodate the highest density of residential and commercial use and the greatest concentration of pedestrian activity.
Curb Type	Curb
Right-of-Way Width	60 to 76 feet
Traffic Lanes (Pavement Width)	2 lanes (10 feet each OR 14 feet each w/ sharrows)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	2 sides parallel parking (8 feet each)
Bike Facilities	Sharrows as needed (14 feet shared lanes)
Sidewalk (Width)	2 sides (12 to 16 feet each)*
Planter Type (Width)	Tree wells (6 feet by 6 feet)*
Street Trees	40 feet on-center average
Specific Applicability	See Street Regulating Plan

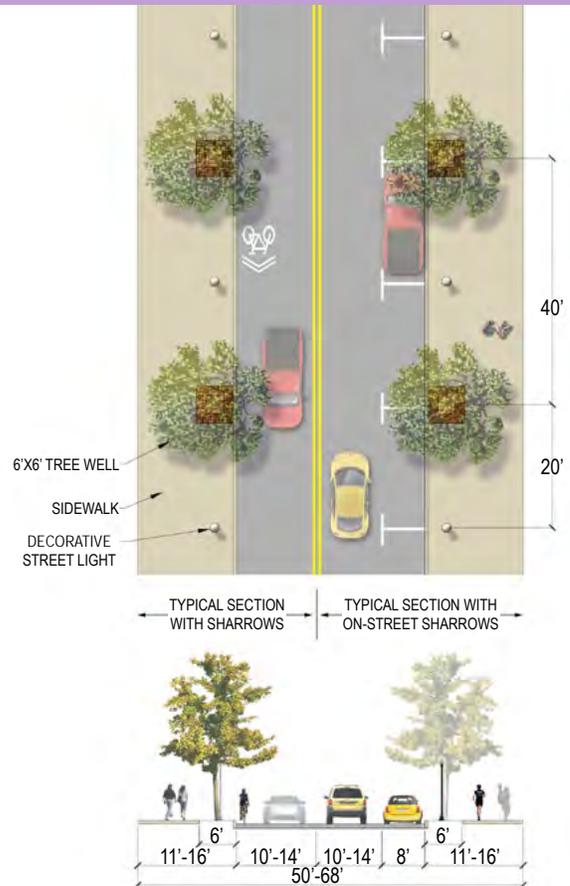
* In the Historic District the sidewalks should be as wide as possible within the existing right-of-way. In some instances, 5 - to 10-foot sidewalks may be the maximum possible within the existing right-of-way. Where the maximum possible sidewalk width is 10 feet or less, street trees in tree wells should not be required.



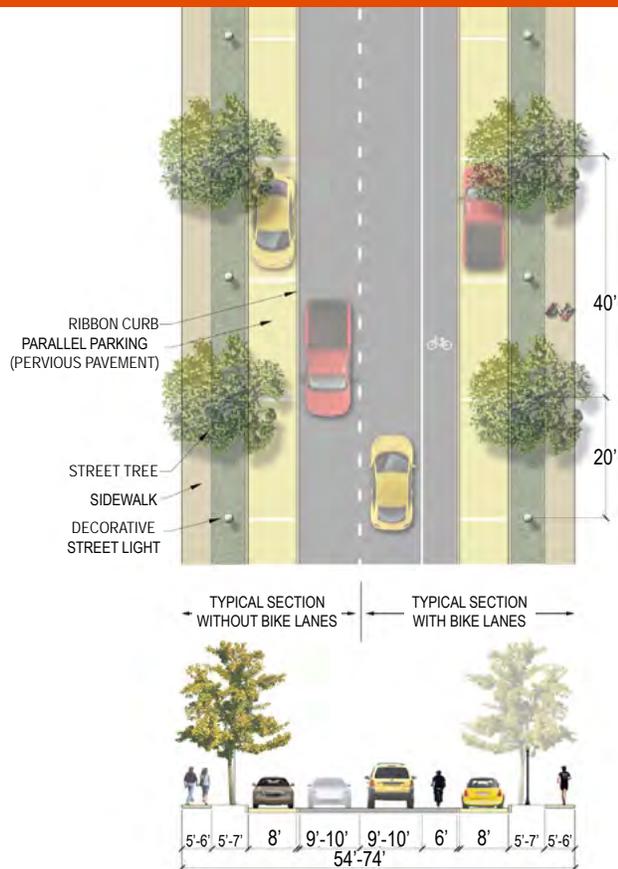
Main Street 2 - Limited (MS2: 50 ft - 68 ft)

Description	This street type is designed for situations where adjacent development calls for a commercial street typology, but the right-of-way is not wide enough to accommodate a Type I Main Street. This street type is also appropriate for the blocks between residential neighborhoods and mixed-use corridors /districts, as they can quickly transition to another street type within the span of a block or less.
Curb Type	Curb
Right-of-Way Width	50 to 68 feet
Traffic Lanes (Pavement Width)	2 lanes (10 feet each OR 14 feet each w/ sharrows)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	1 side parallel parking (8 feet)
Bike Facilities	Sharrows as needed (14 feet shared lanes)
Sidewalk (Width)	2 sides (12 to 16 feet each)*
Planter Type (Width)	Tree wells (6 feet by 6 feet)*
Street Trees	40 feet on-center average
Specific Applicability	See Street Regulating Plan

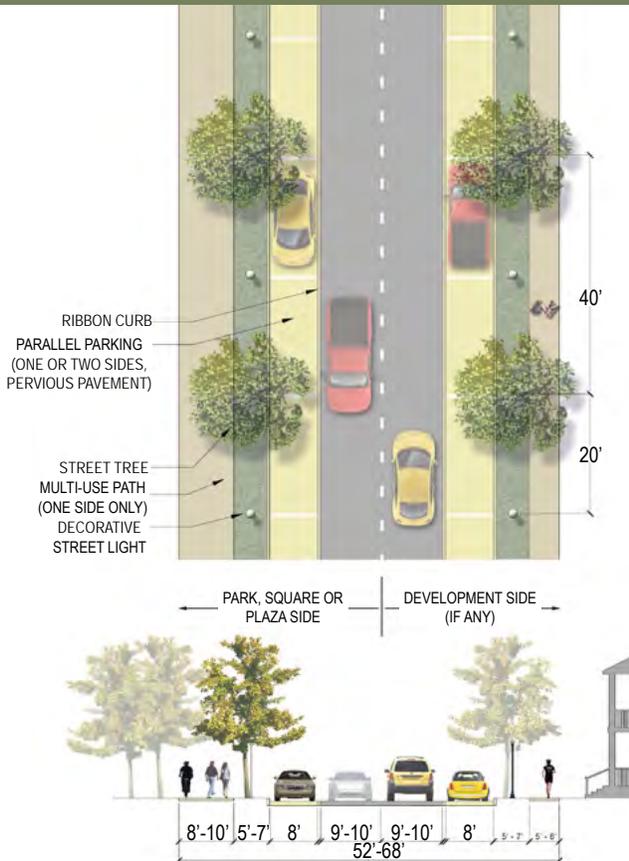
* In the Historic District the sidewalks should be as wide as possible within the existing right-of-way. In some instances, 5 - to 10-foot sidewalks may be the maximum possible within the existing right-of-way. Where the maximum possible sidewalk width is 10 feet or less, street trees in tree wells should not be required.



Avenue (AV: 54 ft - 74 ft)	
Description	Appropriate in both residential and commercial contexts, avenues serve as a primary neighborhood connector, often terminating at prominent buildings or plazas. The on-street parking they provide helps to support activity in neighborhood and employment centers.
Curb Type	Curb
Right-of-Way Width	54 to 74 feet
Traffic Lanes (Pavement Width)	2 lanes (9 to 10 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	2 sides parallel parking (8 feet each) - Pervious pavement preferred
Bike Facilities	Bike lanes (6 feet each) as needed
Sidewalk (Width)	2 sides (5 to 6 feet each)
Planter Type (Width)	Planting strip (5 to 7 feet)
Street Trees	40 feet on-center average
Specific Applicability	See Street Regulating Plan



Parkway (PW: 52 ft - 68ft)	
Description	Parkways are intended to be fronted on at least one side by a park, square, plaza, river or marsh. If such park/natural spaces front only one side of the street, Parkway are suitable to support a broad range of development types on the opposite side, including residential, commercial, mixed-use and civic buildings. Parkway accommodate pedestrians and bicyclists through a multi-use path located on any one side of the street which is fronted by a park/natural space. Where right-of-way is sufficient, Parkway provide parallel parking on both sides of the street. If the right-of-way is constrained, parallel parking is required on one side of the street only, preferably on the side of the street with fronting development.
Curb Type	Curb
Right-of-Way Width	52 to 68 feet
Traffic Lanes (Pavement Width)	2 lanes (9 to 10 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	2 sides parallel parking (8 feet each) where R.O.W is sufficient, OR 1 side parallel parking (8 feet, preferably on development side). Pervious pavement preferred for all parallel parking
Bike Facilities	Multi-use path, must be on park side
Sidewalk (Width)	2 sides (5 to 8 feet, and 8 to 10 feet multi-use path)
Planter Type (Width)	Planting strip (5 to 7 feet each)
Street Trees	40 feet on-center average
Specific Applicability	See Street Regulating Plan



**Neighborhood St. 1 - General
(NS1: 50 ft - 62 ft)**

Description

Type 1 Neighborhood Streets are the most common urban street type in Beaufort. These versatile streets are typically unmarked in residential neighborhoods to allow for informal parking, but can be striped with bike lanes and/or a lane of on-street parking as necessary in more urban situations. As Beaufort's neighborhoods begin to fill in and intensify over time, this street type with its formal curbs and sidewalks may be applied as the logical next step in urbanization from Low Impact Development roads with their open swale drainage.

Curb Type	Curb
Right-of-Way Width	50 to 62 feet
Traffic Lanes (Pavement Width)	2 lanes (30 to 36 feet total) - Marked or Unmarked
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	Informal - 1 side only if bike lane is included
Bike Facilities	Signed route or bike lane (5 feet) as needed
Sidewalk (Width)	2 sides (5 to 6 feet each)
Planter Type (Width)	Planting strip (5 to 7 feet)
Street Trees	40 feet on-center average
Specific Applicability	See Street Regulating Plan

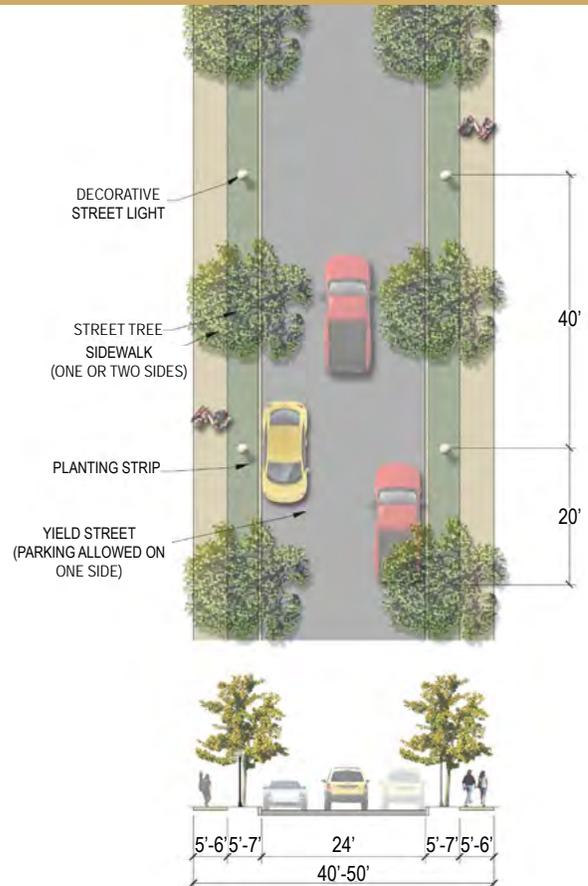


**Neighborhood St. 2 - Yield
(NS2: 40 ft - 50 ft)**

Description

Type 2 Neighborhood Streets are primarily intended to serve single-family structures in residential neighborhoods. The traveled way for cars is slightly narrower than a Type 1 Neighborhood Street, calming traffic and allowing for a yield flow of cars between vehicles parked on the street.

Curb Type	None
Right-of-Way Width	40 to 50 feet
Traffic Lanes (Pavement Width)	2 lanes (24 feet total) - Unmarked
Movement (Design Speed)	Yield Flow (25 MPH)
Parking Lanes (Width)	Informal - parking allowed on one side
Bike Facilities	Signed route as needed
Sidewalk (Width)	1 or 2 sides (5 to 6 feet each)*
Planter Type (Width)	Planting strip (5 to 7 feet)
Street Trees	40 feet on-center typical but can vary to accommodate existing trees where possible
Specific Applicability	See Street Regulating Plan



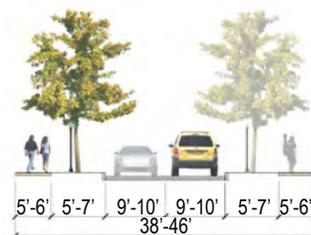
* In the Historic District sidewalks should only be constructed as needed.

Neighborhood St. 3 - Lane (NS3: 38 ft - 46 ft)

Description

Type 3 Neighborhood Streets are small residential lanes intended to provide direct access to the front of a limited number of single-family structures. This street type should be used in situations where the right-of-way is too constrained for other typical street types, and is ideally paired with a Rear Lane that provides off-street parking access to the rear of lots.

Curb Type	Curb
Right-of-Way Width	38 to 46 feet
Traffic Lanes (Pavement Width)	2 lanes (9 to 10 feet each) - Unmarked
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	N/A
Bike Facilities	Signed route as needed
Sidewalk (Width)	1 or 2 sides (5 to 6 feet each)
Planter Type (Width)	Planting strip (5 to 7 feet) - May be waived if R.O.W. is not sufficient
Street Trees	40 feet on-center average
Specific Applicability	See Street Regulating Plan

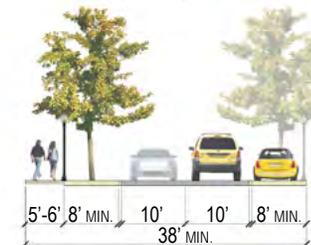
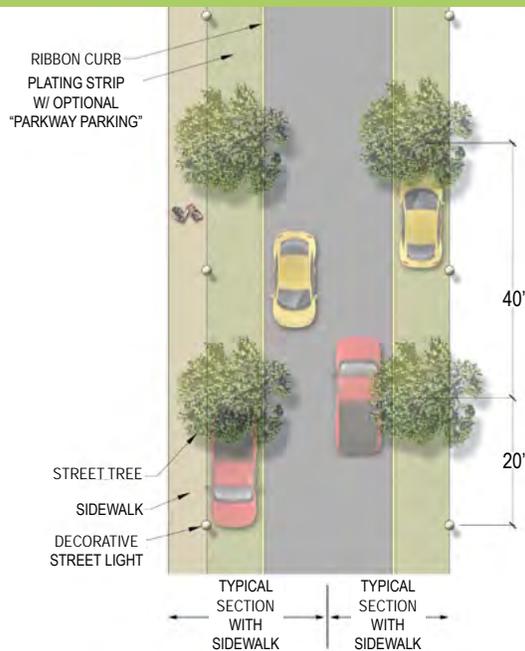


Low Impact Development Rd. (LID: 38 ft +)

Description

In areas where a formal curb and sidewalk treatment is not needed to support adjacent development, this street type provides adequate infrastructure while allowing for the passive infiltration of stormwater at the street edge. In addition to providing drainage, the planting strip area may be reinforced to allow for on-street "parkway parking" between required street trees. Sidewalks should be provided as necessary where the right-of-way is wide enough to accommodate them.

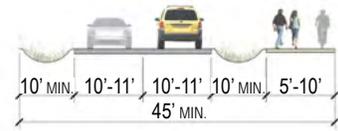
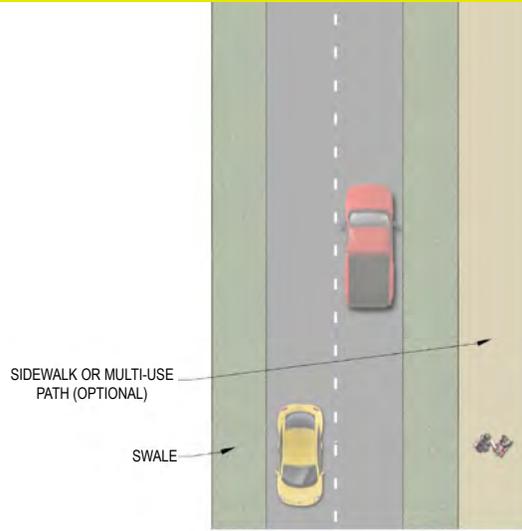
Curb Type	None
Right-of-Way Width	38 feet minimum*
Traffic Lanes (Pavement Width)	2 lanes (10 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	Optional "parkway parking" w/in planting strip area (8 feet each side)
Bike Facilities	Signed route as needed
Sidewalk (Width)	Optional (5 to 6 feet each)
Planter Type (Width)	Planting strip (8 feet minimum - may be used for "parkway parking")*
Street Trees	40 feet on-center average
Specific Applicability	See Street Regulating Plan



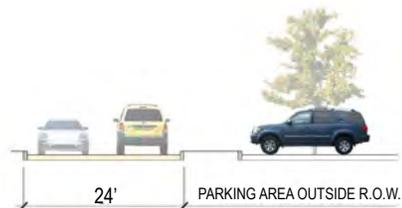
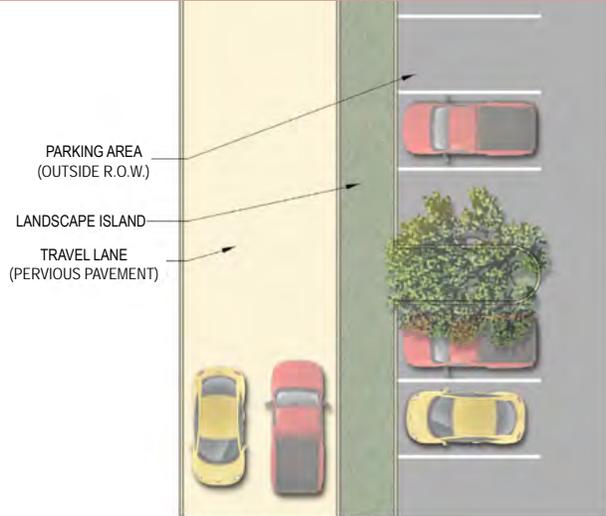
* In the Historic District the minimum permitted right-of-way width should be decreased as necessary, by eliminating a planting strip from one side of the street, to accommodate existing corridors that are particularly narrow. (Example: King Street)



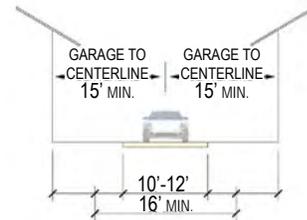
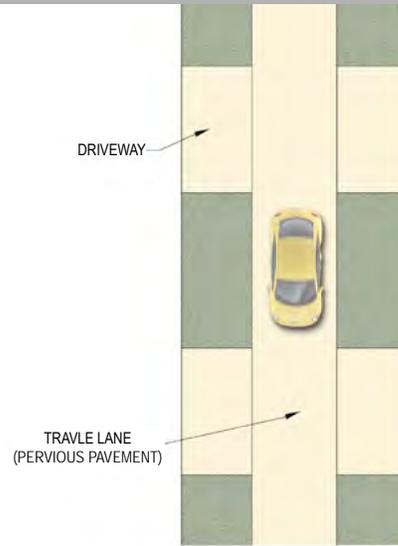
Rural Road (RR: 45 ft +)	
Description	This street type is widely applied throughout Beaufort's rural areas in instances where adjacent development does not require the support of substantial infrastructure. As such, the Rural Road street type includes only limited pedestrian accommodation and open swale drainage.
Curb Type	Open swale
Right-of-Way Width	45 feet minimum
Traffic Lanes (Pavement Width)	2 lanes (10 to 11 feet each) - May include center turn lane
Movement (Design Speed)	Moderate (35 MPH)
Parking Lanes (Width)	N/A
Bike Facilities	Multi-use path - Optional
Sidewalk (Width)	1 side (5 feet or 10 feet with multi-use path)
Planter Type (Width)	Planting strip (10 feet minimum)
Street Trees	N/A
Specific Applicability	See Street Regulating Plan

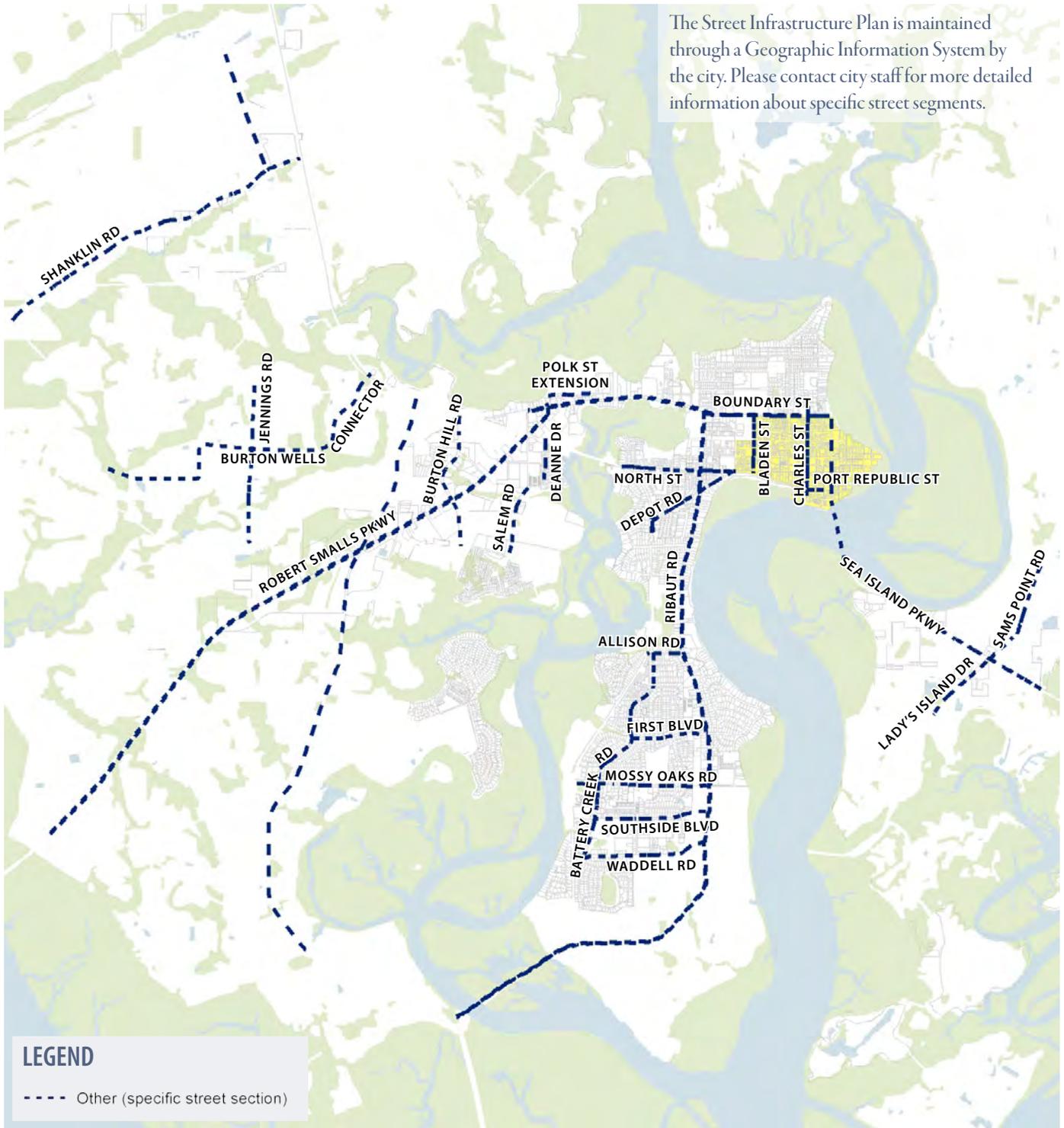


Commercial Alley (CA: 24 ft)	
Description	This street type is intended to be privately maintained and is used most frequently as an access lane for off-street parking and loading areas. Although primary building entrances should always be located along the major fronting street and any large off-street parking areas, in certain situations adjacent structures may include secondary/service entrances that allow direct access from rear commercial alleys.
Curb Type	None
Right-of-Way Width	24 feet
Traffic Lanes (Pavement Width)	2 lanes (24 feet) - Pervious pavement preferred
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	N/A
Bike Facilities	N/A
Sidewalk (Width)	N/A
Planter Type (Width)	Parking area landscape islands (varied width)
Street Trees	Located within parking area landscape islands
Specific Applicability	See Street Regulating Plan



Rear Lane (RL: 16 ft)	
Description	This street type is intended to be privately maintained and provide indirect, limited access to the rear of residential properties. It is not intended to accommodate through traffic but may accommodate city services such as garbage and recycling collection. Utilities, either above ground or underground, may be located in Rear Lanes to provide service connections to adjacent properties.
Curb Type	None
Right-of-Way Width	20 feet minimum
Traffic Lanes (Pavement Width)	1 lane (10 to 12 feet) - Pervious Pavement
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	N/A
Bike Facilities	N/A
Sidewalk (Width)	N/A
Planter Type (Width)	N/A
Street Trees	N/A
Specific Applicability	See Street Regulating Plan



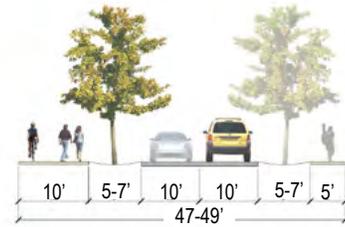
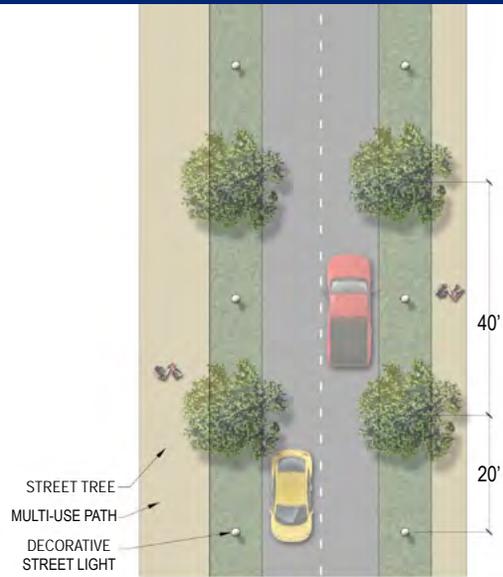


▲ STREET INFRASTRUCTURE PLAN - OTHER STREETS WITH SPECIFIC SECTIONS

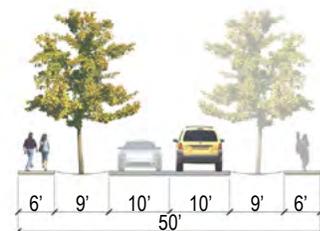
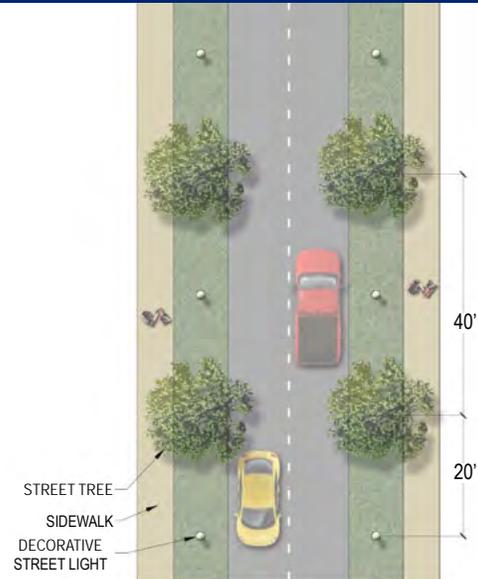
Specific Street Sections

All of the street segments marked as “Other” in the Street Regulating Plan (illustrated in the map above) have been assigned one of the specific street sections on the following pages. These street sections were carefully designed during the Civic Master Plan process to address unique site-specific considerations. As such, these street sections are specifically tailored to a unique development context and deviate from the standards enumerated in the typical street sections. In some cases, a specific street section may apply only to a portion of a single street. In other cases, a specific street section may apply to multiple streets. The applicability of each specific street section is enumerated in its corresponding table.

Allison Rd. (ST: 47-49 ft)	
Specific Applicability	• Allison Road - entire length
Curb Type	Curb
Right-of-Way Width	47-49 feet
Traffic Lanes (Pavement Width)	2 lanes (10 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	N/A
Bike Facilities	Multi-use path primarily on south side
Sidewalk (Width)	2 sides (5 feet on north side, 10 feet multi-use path primarily on south side)
Planter Type (Width)	Planting strip (5-7 feet)
Street Trees	40 feet on-center average
Phasing	Phase 1: South side of travel-way Phase 2: North side of travel-way

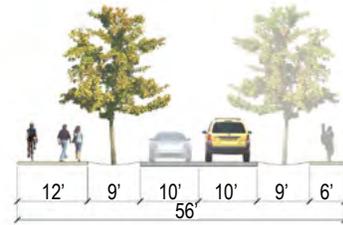
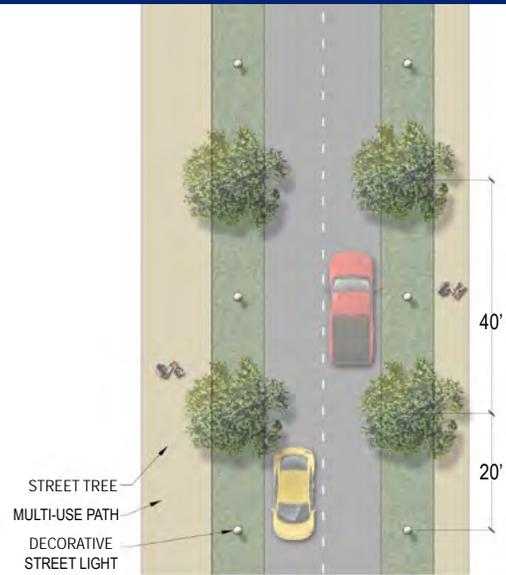


Battery Creek Rd. A (RD: 50 ft)	
Specific Applicability	• Battery Creek Road from Allison Road to First Boulevard
Curb Type	Open swale
Right-of-Way Width	50 feet
Traffic Lanes (Pavement Width)	2 lanes (10 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	N/A
Bike Facilities	N/A
Sidewalk (Width)	2 sides (6 feet each)
Planter Type (Width)	Planting strip (9 feet)
Street Trees	40 feet on-center average



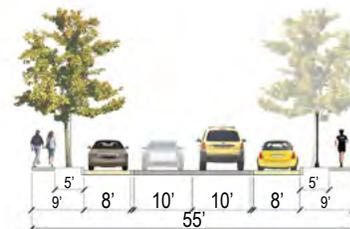
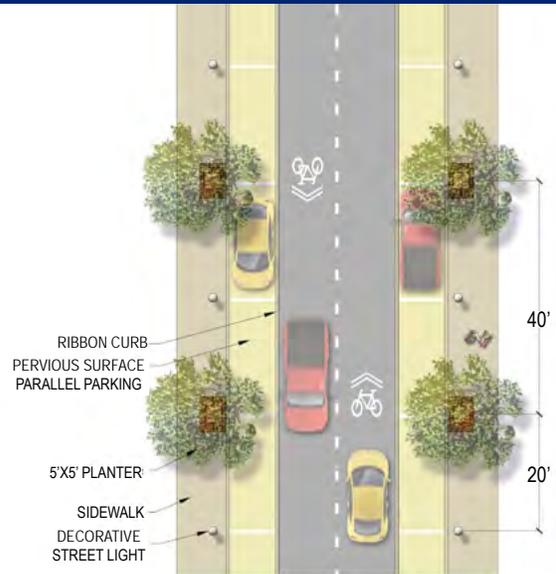
**Battery Creek Rd. B
(RD: 56 ft)**

Specific Applicability	• Battery Creek Road from First Boulevard to Waddell Road
Curb Type	Curb
Right-of-Way Width	56 feet
Traffic Lanes (Pavement Width)	2 lanes (10 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	N/A
Bike Facilities	Multi-use path, West side only
Sidewalk (Width)	2 sides (6 feet on East side, 12 feet multi-use path on West side)
Planter Type (Width)	Planting strip (9 feet)
Street Trees	40 feet on-center average



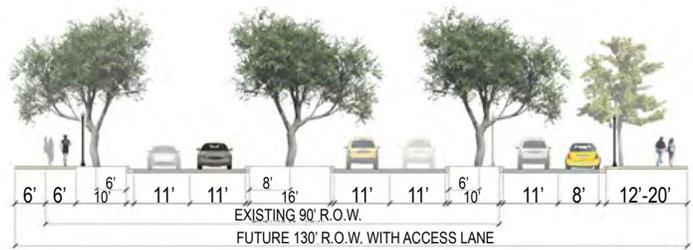
**Bladen St. & Charles St. B
(ST: 55 ft)**

Specific Applicability	• Bladen Street - entire length • Charles Street from Duke Street to Calhoun Street
Curb Type	Curb
Right-of-Way Width	55 feet
Traffic Lanes (Pavement Width)	2 lanes (10 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	2 sides parallel parking (8 feet each) - Pervious pavement
Bike Facilities	Sharrows
Sidewalk (Width)	2 sides (9 feet each)
Planter Type (Width)	Planters (5 feet by 5 feet)
Street Trees	40 feet on-center average



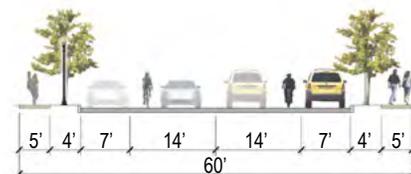
Boundary St. 1 - West of Ribaut Rd. (BLVD: 90 ft - 130 ft)	
Specific Applicability	• Boundary Street Context Zone 1 (West of Ribaut Road)
Curb Type	Curb
Right-of-Way Width	90 feet for current improvements, 130 feet for future improvements
Traffic Lanes (Pavement Width)	4 lanes (11 feet each) and 1 future access lane (11 feet)
Movement (Design Speed)	Moderate (35 MPH)
Parking Lanes (Width)	1 side future parallel parking (8 feet)
Bike Facilities	N/A
Sidewalk (Width)	1 side (6 feet) for current improvements 2 sides (12 to 20 feet) for future improvements
Planter Type (Width)	Planting strip (10 feet) & Planted median (16 feet)
Street Trees	40 feet on-center average

STREET TREE
SIDEWALK FUTURE
SIDEWALK DECORATIVE
STREET LIGHT



Boundary St. 2A - East of Ribaut Road (ST: 60 ft)	
Specific Applicability	• Boundary Street (from Ribaut Road east to Carteret Street) - intended to be applied as a preliminary Phase 1 condition in the redevelopment of the Boundary Street corridor. The final phase condition is illustrated in "Boundary St. A2" below.
Curb Type	Curb
Right-of-Way Width	60 feet
Traffic Lanes (Pavement Width)	2 lanes (14 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	2 sides parallel parking (7 feet each)
Bike Facilities	Sharrows (14 feet shared lanes)
Sidewalk (Width)	2 sides (5 feet each)
Planter Type (Width)	Planting strip (4 feet)
Street Trees	40 feet on-center average

DECORATIVE STREET LIGHT
SIDEWALK
STREET TREE
PLANTING STRIP
PARALLEL PARKING

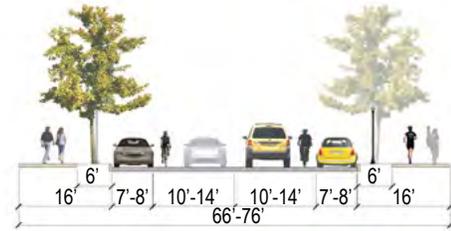
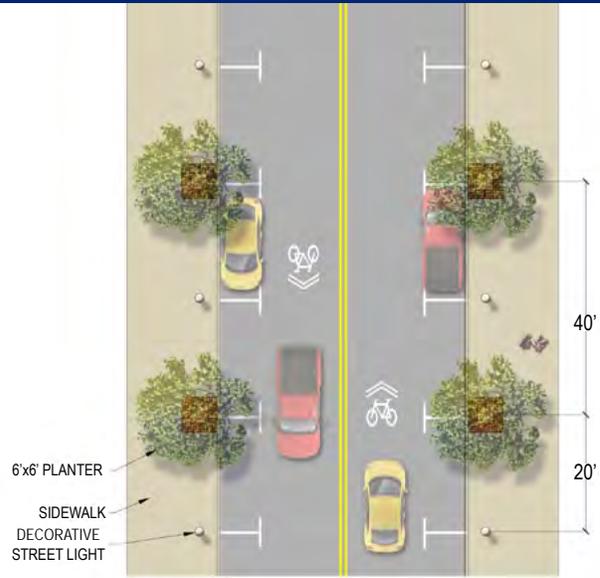


**Boundary St. 2B -
East of Ribaut Road
(ST: 66 ft - 76 ft)**

Specific Applicability

• Boundary Street (from Ribaut Road to Carteret Street) - intended to be applied as a final Phase 2 condition in the redevelopment of the Boundary Street corridor. The preliminary phase condition is illustrated in "Boundary St. 2A" above.

Curb Type	Curb
Right-of-Way Width	66 to 76 feet
Traffic Lanes (Pavement Width)	2 lanes (10 to 14 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	2 sides parallel parking (7 to 8 feet each)
Bike Facilities	Sharrows (10 to 14 feet shared lanes)
Sidewalk (Width)	2 sides (16 feet each)
Planter Type (Width)	Tree wells (6 feet by 6 feet)
Street Trees	40 feet on-center average

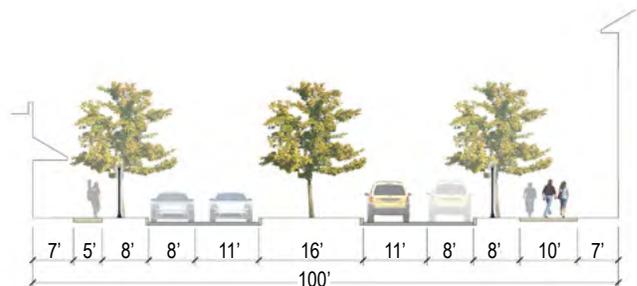


**Burton Hill Rd.
(BLVD: 100 ft)**

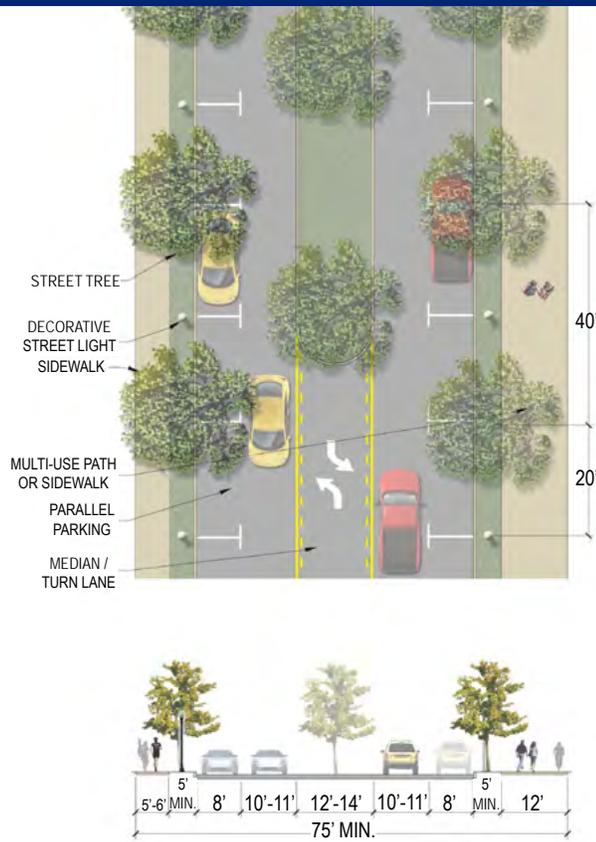
Specific Applicability

• Burton Hill Road - entire length

Curb Type	Curb
Right-of-Way Width	100 feet
Traffic Lanes (Pavement Width)	2 lanes (11 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	2 sides parallel parking (8 feet)
Bike Facilities	Multi-use path
Sidewalk (Width)	Sidewalk on west side (5 feet), Multi-use path on east side (10 feet)
Planter Type (Width)	Planting strip (8 feet) & Planted median (16 feet)
Street Trees	40 feet on-center average



Burton Wells Connector (BLVD: 75 ft +)	
Specific Applicability	• Future Burton Wells connector from Burton Wells Park (extending from Burton Wells Drive) to Boundary Street
Curb Type	Curb
Right-of-Way Width	75 feet minimum
Traffic Lanes (Pavement Width)	2 lanes (10 to 11 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	2 sides parallel parking (8 feet)
Bike Facilities	Multi-use path
Sidewalk (Width)	Sidewalk on south side (5 to 6 feet), Multi-use path on north side (12 feet)
Planter Type (Width)	Planting strip (5 feet minimum) & Planted median (12 to 14 feet)
Street Trees	40 feet on-center average

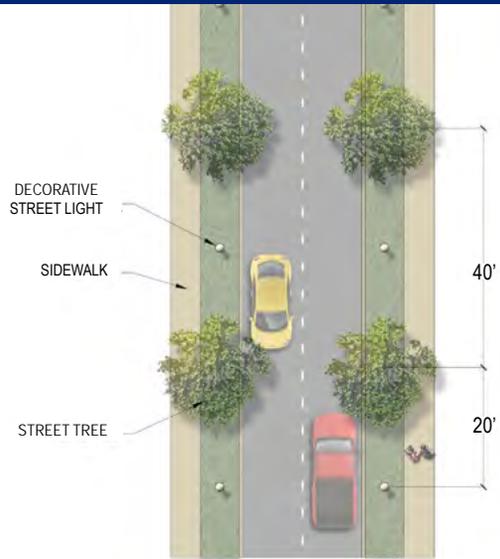


Charles St. A (ST: 45 ft)	
Specific Applicability	• Charles Street from Bay Street to Duke Street (See also: Bladen St. / Charles St. B)
Curb Type	None
Right-of-Way Width	45 feet
Traffic Lanes (Pavement Width)	2 lanes (10 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	1 side parallel parking (8 feet)
Bike Facilities	N/A
Sidewalk (Width)	2 sides (9 feet each)
Planter Type (Width)	Planters (4 feet by 5 feet)
Street Trees	40 feet on-center average



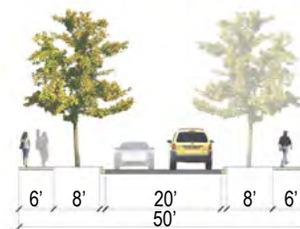
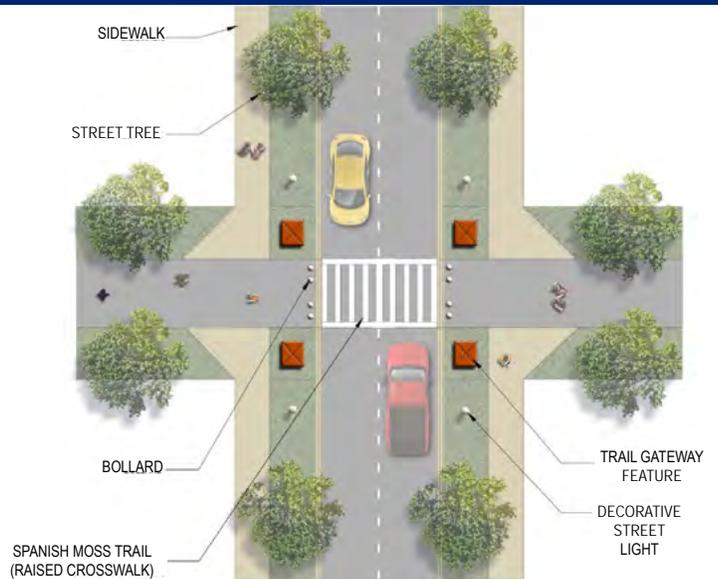
**Deanne Dr. A
(ST: 36 ft - 46 ft)**

Specific Applicability	• Deanne Drive - existing street & future connection to Beaufort Plaza across the Spanish Moss Trail
Curb Type	Curb
Right-of-Way Width	36 to 46 feet
Traffic Lanes (Pavement Width)	2 lanes (9 to 10 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	N/A
Bike Facilities	N/A
Sidewalk (Width)	2 sides (4 to 6 feet each)
Planter Type (Width)	Planting strip (5 to 7 feet)
Street Trees	40 feet on-center average



**Deanne Dr. B
(ST: 50 ft)**

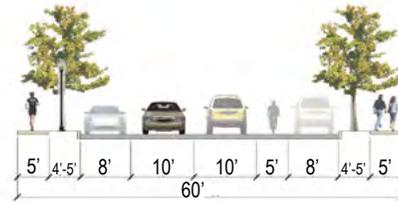
Specific Applicability	• Deanne Drive - at future intersection with Spanish Moss Trail
Curb Type	Curb
Right-of-Way Width	50 feet
Traffic Lanes (Pavement Width)	2 lanes (10 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	N/A
Bike Facilities	N/A
Sidewalk (Width)	2 sides (6 feet each)
Planter Type (Width)	Planting strip (8 feet)
Street Trees	40 feet on-center average



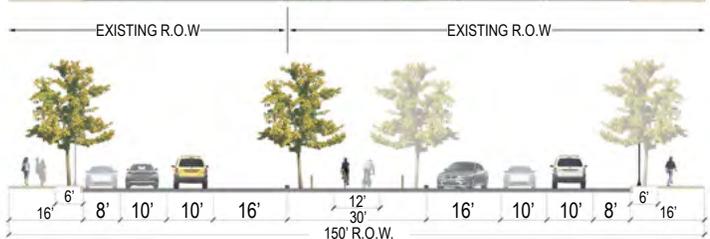
DEANNE DRIVE AT SPANISH MOSS TRAIL

10: Regulatory Framework

Depot Rd. B (ST: 60 ft)	
Specific Applicability	<ul style="list-style-type: none"> Depot Road from Burroughs Avenue to Depot Road redevelopment area (See also: North St. / Depot Rd. A)
Curb Type	Curb
Right-of-Way Width	60 feet
Traffic Lanes (Pavement Width)	2 lanes (10 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	2 sides parallel parking (8 feet each)
Bike Facilities	1 side (5 feet) - South side
Sidewalk (Width)	2 sides (5 feet each)
Planter Type (Width)	Planting strip (4 to 5 feet)
Street Trees	40 feet on-center average

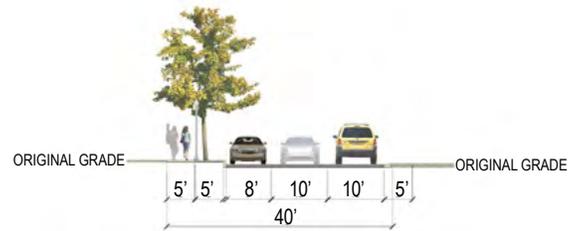
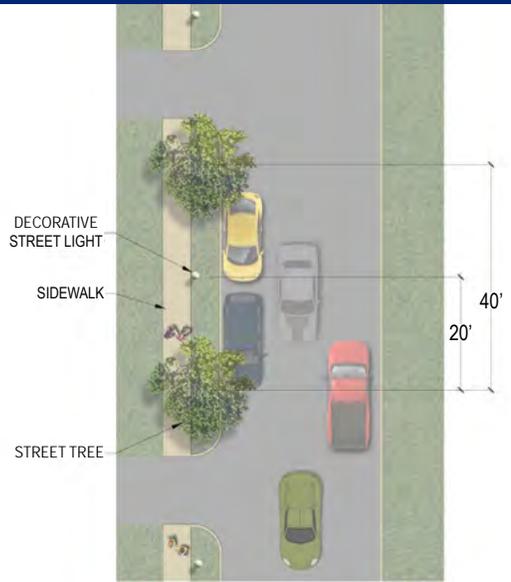


Depot Rd. C (ST: 90 ft - 150 ft)	
Specific Applicability	<ul style="list-style-type: none"> Depot Road redevelopment area (Hay Street to Middleton Street) - intended to be built in phases beginning with the existing 90-foot right-of-way and eventually replicating on the west side of the Spanish Moss Trail within an expanded 150-foot right-of-way (See also: North St. / Depot Rd. A)
Curb Type	Normal curb on development side, flush curb on trail side*
Right-of-Way Width	90 feet, expanding to 150 feet
Traffic Lanes (Pavement Width)	2 lanes, expanding to 4 lanes (10 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	Parallel parking (7 feet) on development side, 30 degree diagonal parking (16 feet) on trail side
Bike Facilities	Spanish Moss Trail
Sidewalk (Width)	Development side (16 feet each), Spanish Moss Trail (12 feet)
Planter Type (Width)	Tree wells (6 feet by 6 feet)
Street Trees	40 feet on-center average



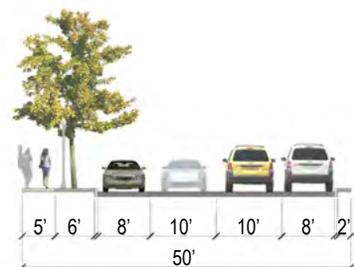
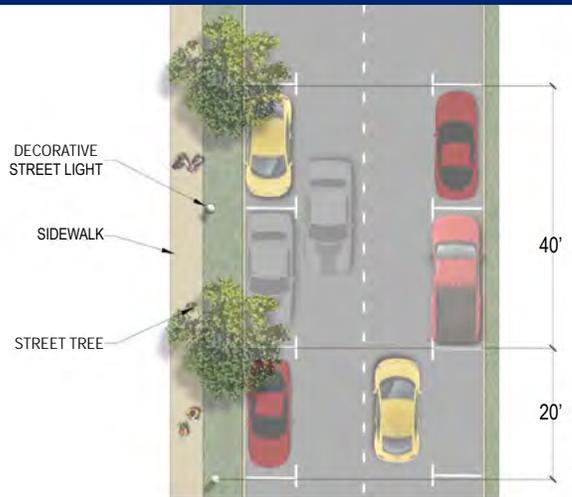
**First St. A
(ST: 40 ft)**

Specific Applicability	• First Street from Westview Avenue to Hogarth Street
Curb Type	Normal curb on North side, Roll curb on South side
Right-of-Way Width	40 feet
Traffic Lanes (Pavement Width)	2 lanes (10 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	North side only parallel parking (8 feet)
Bike Facilities	N/A
Sidewalk (Width)	North side only (5 feet)
Planter Type (Width)	Planting strip (5 feet)
Street Trees	North side only, 40 feet on-center average

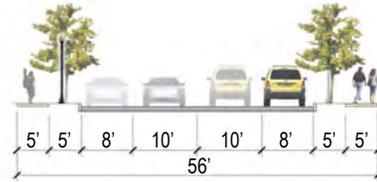
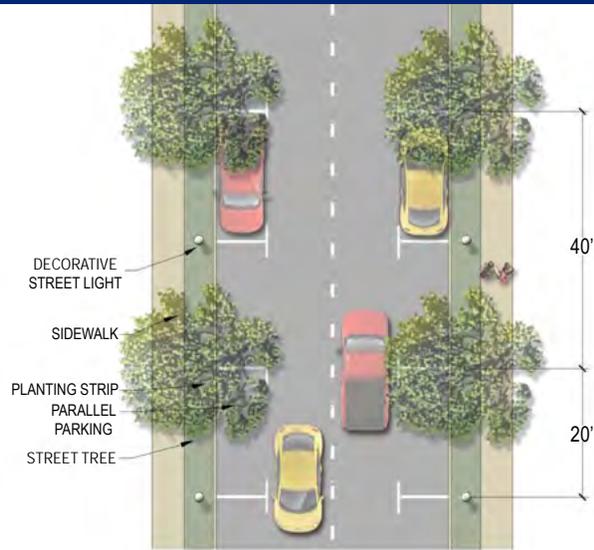


**First St. B
(ST: 50 ft)**

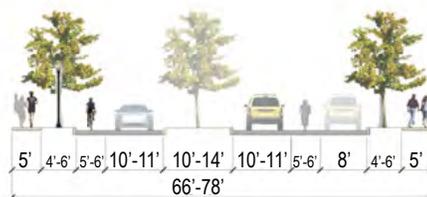
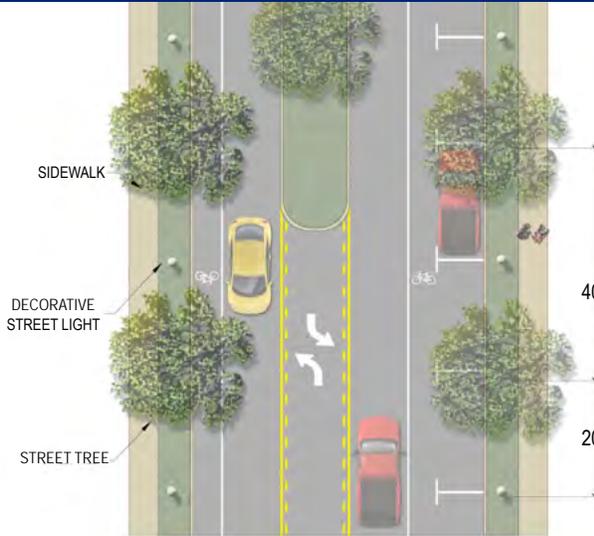
Specific Applicability	• First Street future extension from Boundary Street to Westview Avenue
Curb Type	Curb
Right-of-Way Width	50 feet
Traffic Lanes (Pavement Width)	2 lanes (10 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	2 sides parallel parking (8 feet each)
Bike Facilities	N/A
Sidewalk (Width)	North side only (5 feet)
Planter Type (Width)	Planting strip (6 feet on North side, 2 feet on South side)
Street Trees	North side only, 40 feet on-center average



Jennings Rd. B (ST: 56 ft)	
Specific Applicability	<ul style="list-style-type: none"> Jennings Road - intended to be used in the proposed Jennings Road neighborhood center near Battery Creek High School
Curb Type	Curb
Right-of-Way Width	56 feet
Traffic Lanes (Pavement Width)	2 lanes (10 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	2 sides parallel parking (8 feet each)
Bike Facilities	N/A
Sidewalk (Width)	2 sides (5 feet each)
Planter Type (Width)	Planting strip (5 feet)
Street Trees	40 feet on-center average

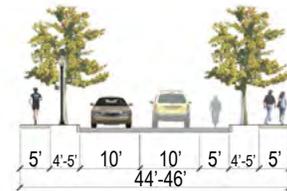


Mossy Oaks Rd. (BLVD: 66 ft - 78 ft)	
Specific Applicability	<ul style="list-style-type: none"> Mossy Oaks Road from Ribaut Road to Battery Creek Road
Curb Type	Curb
Right-of-Way Width	66 to 78 feet
Traffic Lanes (Pavement Width)	2 lanes (10 to 11 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	1 sides parallel parking (8 feet)
Bike Facilities	Bike lanes
Sidewalk (Width)	2 sides (5 feet each)
Planter Type (Width)	Planting strip (4 to 6 feet)
Street Trees	40 feet on-center average



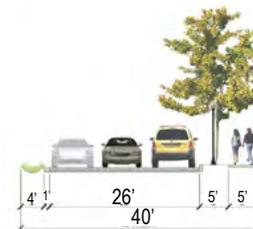
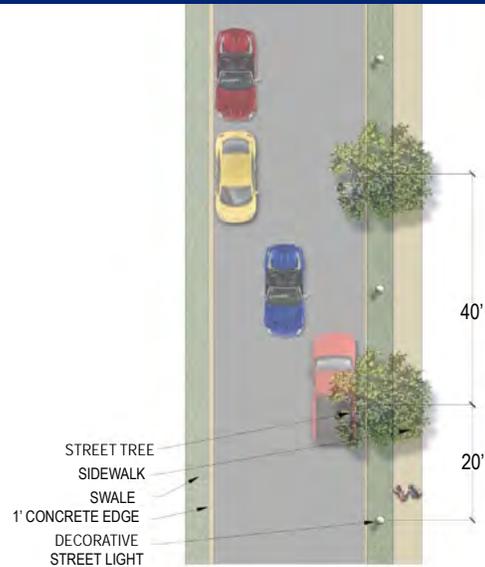
**North St. & Depot Rd. A
(ST: 44 ft - 46 ft)**

Specific Applicability	<ul style="list-style-type: none"> • North Street - entire length • Depot Street from Ribaut Road to Burroughs Avenue
Curb Type	Curb
Right-of-Way Width	44 to 46 feet
Traffic Lanes (Pavement Width)	2 lanes (10 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	N/A
Bike Facilities	1 side (5 feet) - South side of Depot Rd. and North side of North St.
Sidewalk (Width)	2 sides (5 feet each)
Planter Type (Width)	Planting strip (4 to 5 feet)
Street Trees	40 feet on-center average

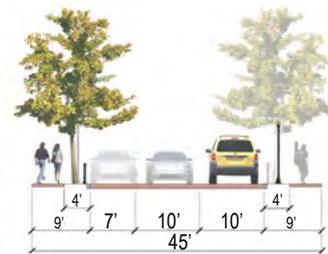
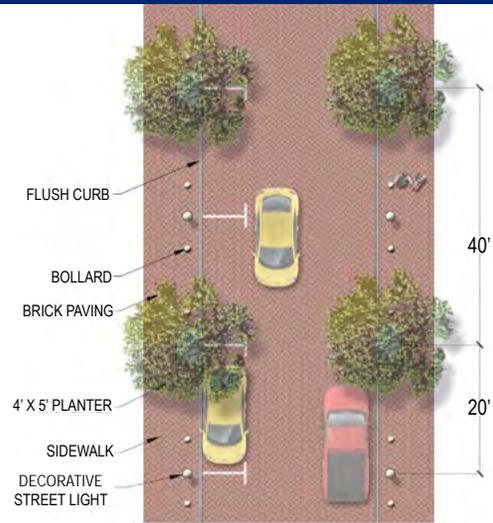


**Polk St. Extension
(ST: 40 ft)**

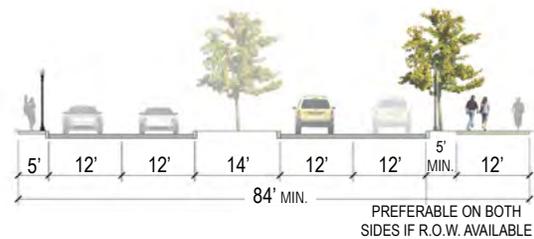
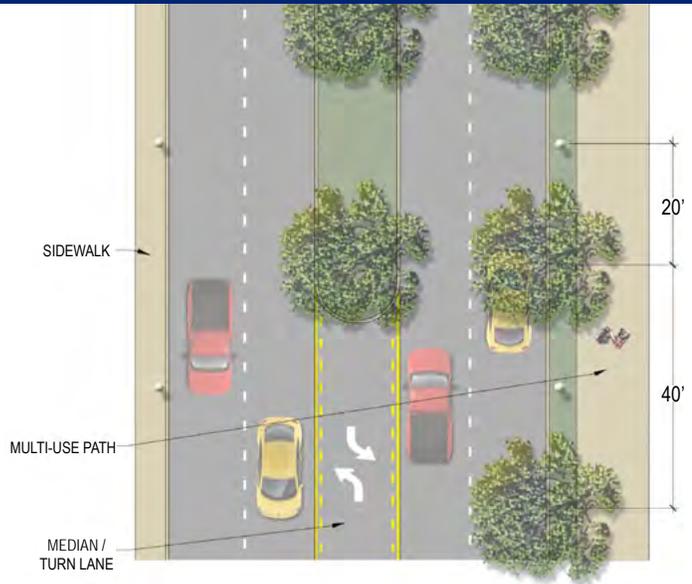
Specific Applicability	• Polk Street Extension connecting to Robert Smalls Parkway across Boundary Street
Curb Type	Curb/1' Concrete edge with small swale
Right-of-Way Width	40 feet
Traffic Lanes (Pavement Width)	2 lanes (9 feet each)
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	1 side parallel parking (8 feet)
Bike Facilities	N/A
Sidewalk (Width)	1 sides (5 feet)
Planter Type (Width)	Planting Strip (5 feet)
Street Trees	40 feet on-center average

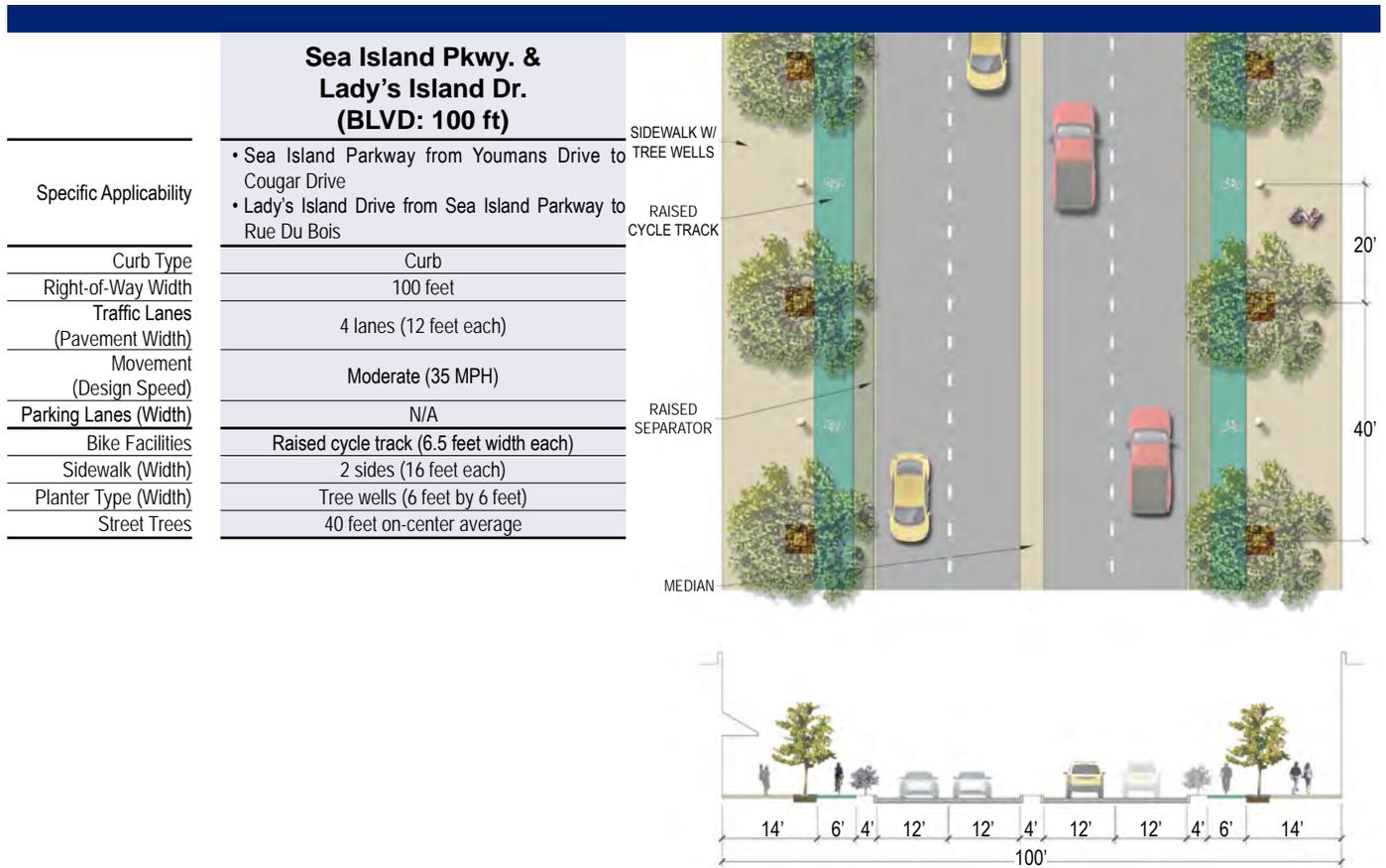
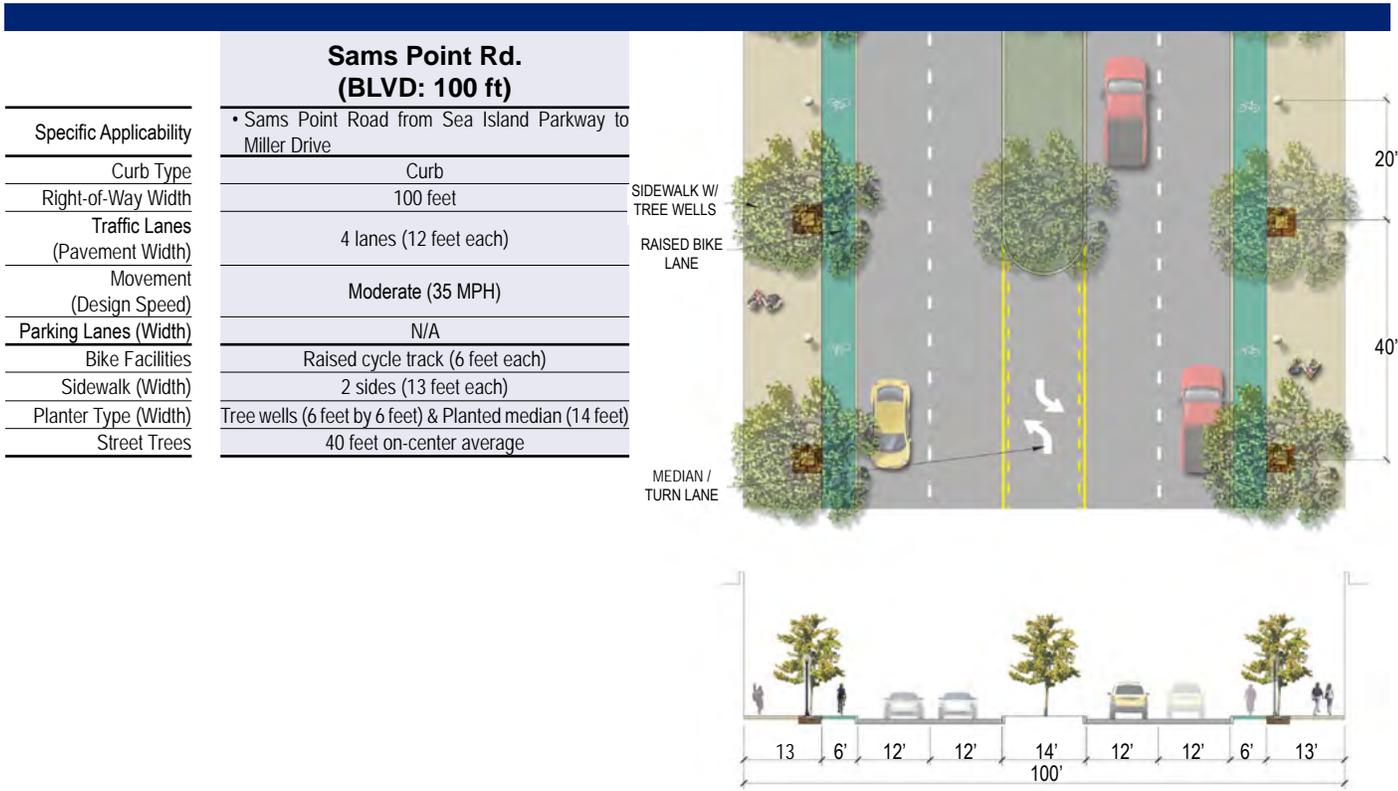


Port Republic St. (ST: 45 ft)	
Specific Applicability	• Port Republic Street from Charles Street to Carteret Street
Curb Type	Flush curb
Right-of-Way Width	45 feet
Traffic Lanes (Pavement Width)	2 lanes (10 feet each) - brick pavers
Movement (Design Speed)	Slow (25 MPH)
Parking Lanes (Width)	1 side parallel parking (7 feet)
Bike Facilities	N/A
Sidewalk (Width)	2 sides (9 feet each) - brick pavers
Planter Type (Width)	Tree wells (4 feet by 5 feet)
Street Trees	40 feet on-center average



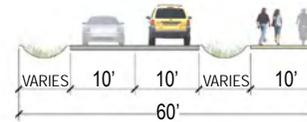
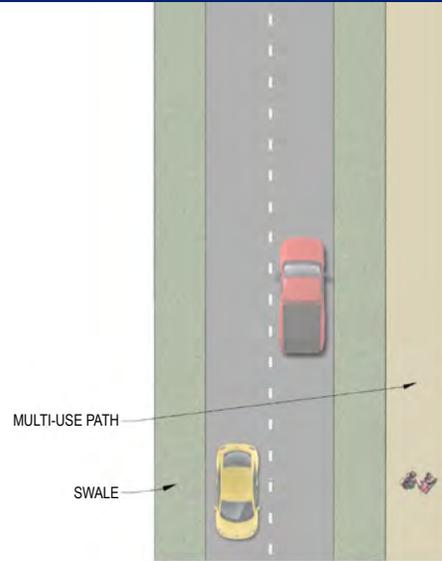
Robert Smalls Pkwy. (BLVD: 84 ft +)	
Specific Applicability	• Robert Smalls Parkway - entire length
Curb Type	Curb
Right-of-Way Width	84 feet minimum
Traffic Lanes (Pavement Width)	4 lanes (12 feet each)
Movement (Design Speed)	Limited Access (45 MPH)
Parking Lanes (Width)	N/A
Bike Facilities	Multi-use path
Sidewalk (Width)	2 sides (5 feet and 12 feet multi-use path)
Planter Type (Width)	Planting strip (5 feet minimum) & Planted median (14 feet)
Street Trees	40 feet on-center average



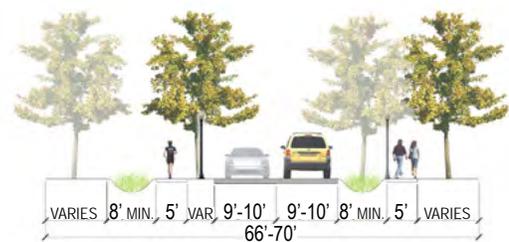
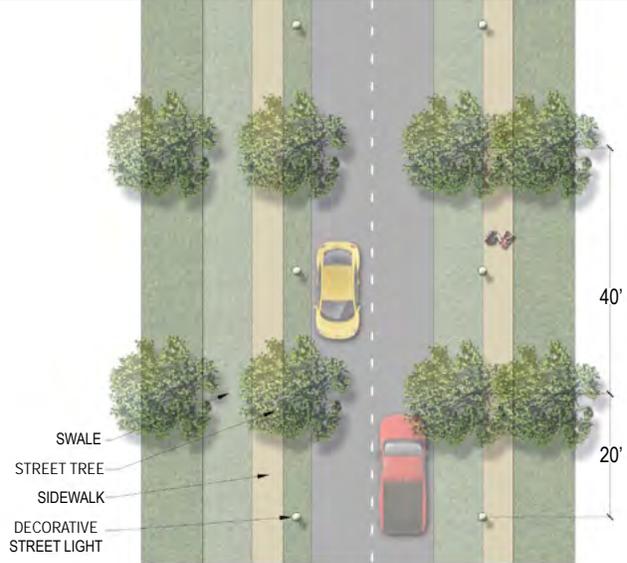




Shanklin Rd. & Salem Rd. (RD: 60 ft)	
Specific Applicability	<ul style="list-style-type: none"> Shanklin Road - entire length Salem Road - intended to be used along undevelopable land South of Moss Street
Curb Type	Open swale
Right-of-Way Width	60 feet
Traffic Lanes (Pavement Width)	2 lanes (10 feet each)
Movement (Design Speed)	Moderate (35 MPH)
Parking Lanes (Width)	N/A
Bike Facilities	Multi-use path, East side only
Sidewalk (Width)	1 sides (10 feet multi-use path on East side)
Planter Type (Width)	Planting strip (varied width)
Street Trees	N/A

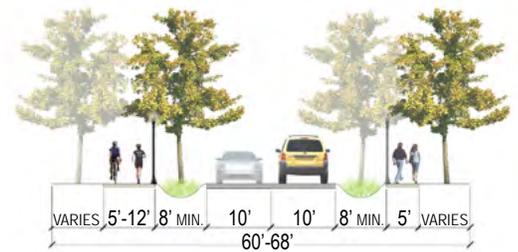
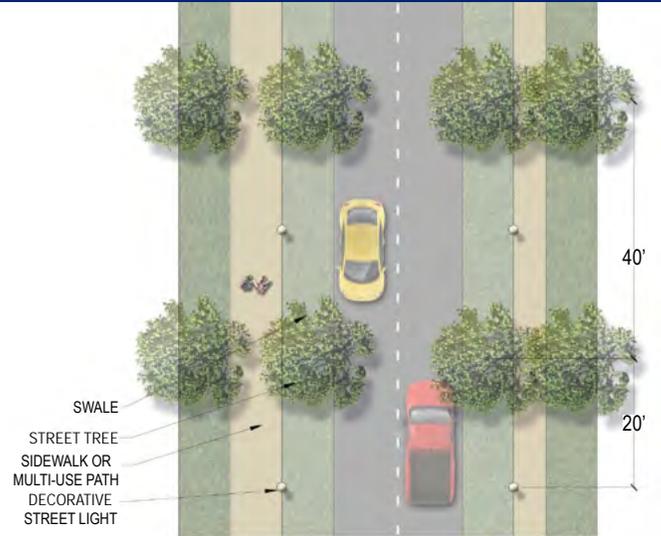


Southside Blvd. & First Blvd. (ST: 66 ft - 70 ft)	
Specific Applicability	<ul style="list-style-type: none"> Southside Boulevard - entire length First Boulevard - entire length
Curb Type	Open swale
Right-of-Way Width	66 to 70 feet
Traffic Lanes (Pavement Width)	2 lanes (9 to 10 feet each)
Movement (Design Speed)	Moderate (35 MPH)
Parking Lanes (Width)	N/A
Bike Facilities	N/A
Sidewalk (Width)	2 sides (5 feet each)
Planter Type (Width)	Planting strip (varied width)
Street Trees	40 feet on-center average

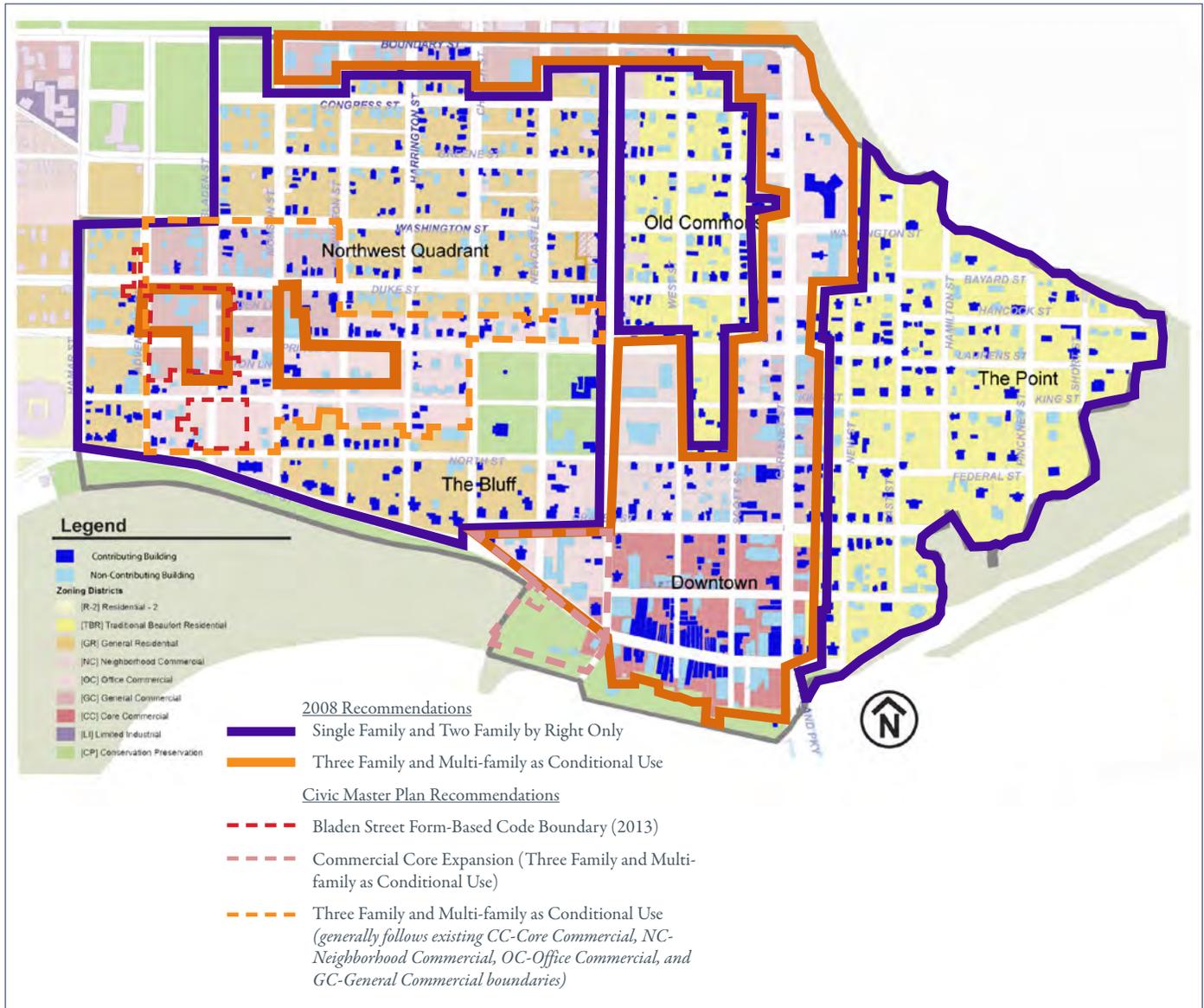


**Waddell Rd. & Jennings Rd. A
(RD: 60 ft - 68 ft)**

Specific Applicability	<ul style="list-style-type: none"> Waddell Road - entire length Jennings Road - intended to be used along the portions of Jennings Road NOT located in the Jennings Road neighborhood center near Battery Creek High School
Curb Type	Open swale
Right-of-Way Width	60 to 68 feet
Traffic Lanes (Pavement Width)	2 lanes (10 feet each)
Movement (Design Speed)	Moderate (35 MPH)
Parking Lanes (Width)	N/A
Bike Facilities	Multi-use path, North side of Waddell Road only
Sidewalk (Width)	2 sides (5 feet and 12 feet multi-use path on Waddell Road, 5 feet each on Jennings Road)
Planter Type (Width)	Planting strip (8 feet minimum)
Street Trees	N/A



10.3 Proposed Zoning Changes for the National Historic Landmark District



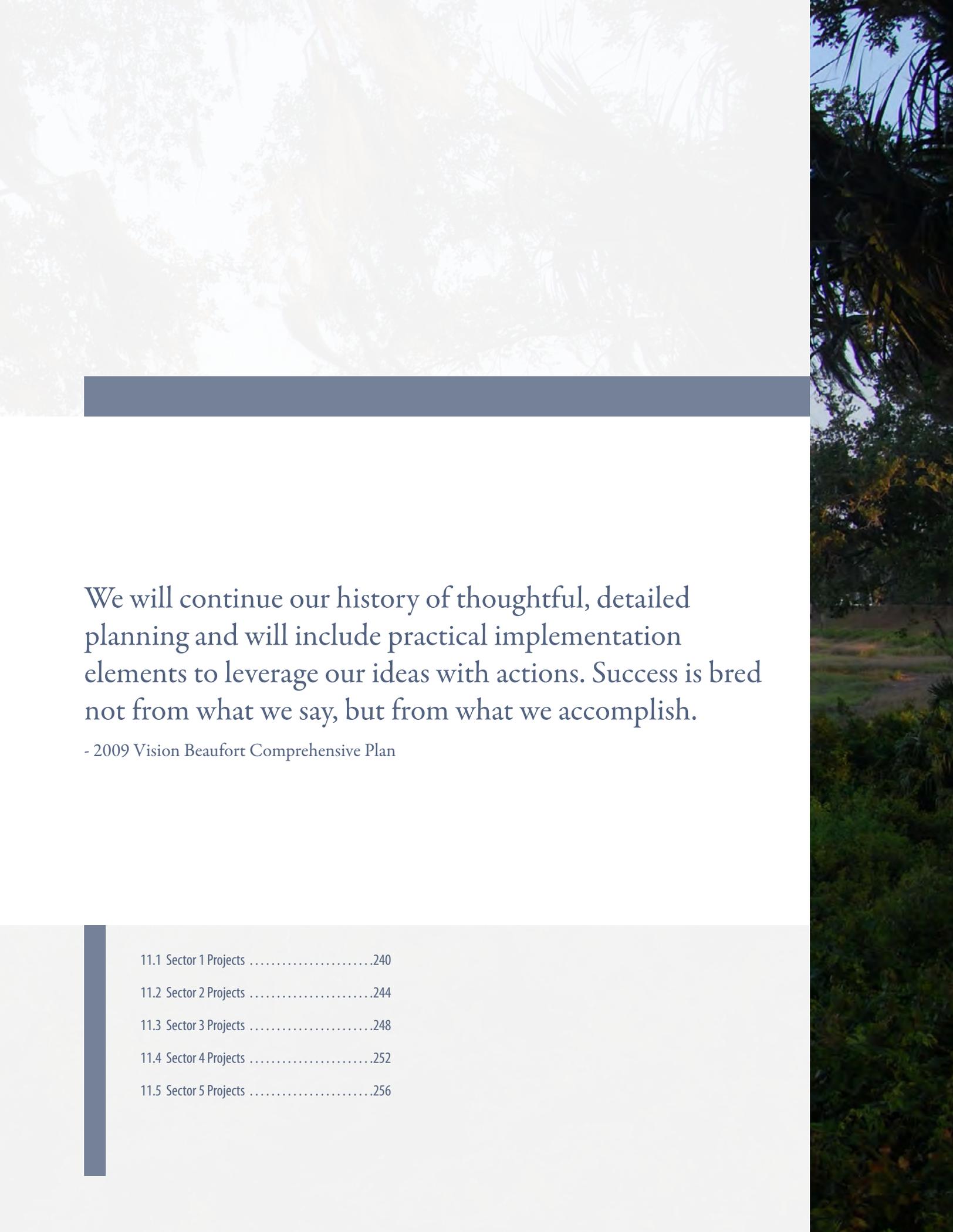
Base Map Created by Lord Aeck Sargent

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PROJECT IMPLEMENTATION

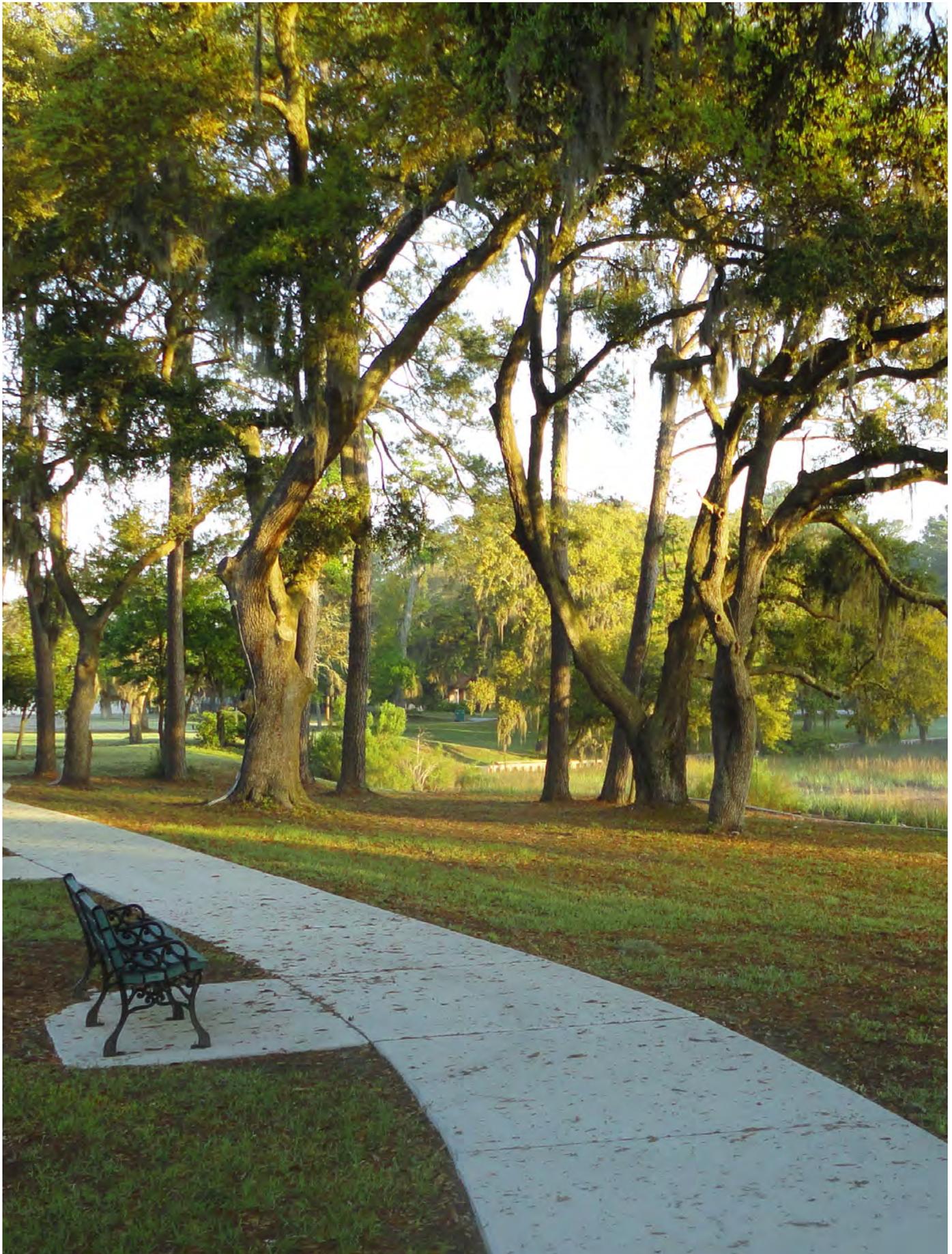




We will continue our history of thoughtful, detailed planning and will include practical implementation elements to leverage our ideas with actions. Success is bred not from what we say, but from what we accomplish.

- 2009 Vision Beaufort Comprehensive Plan

11.1 Sector 1 Projects	240
11.2 Sector 2 Projects	244
11.3 Sector 3 Projects	248
11.4 Sector 4 Projects	252
11.5 Sector 5 Projects	256



In order to implement the recommendations in the Civic Master Plan, the City of Beaufort's Redevelopment Commission will prioritize civic infrastructure projects and seek partnerships with private organizations ready to invest in the future of the city. These organizations may include both developers interested in expanding the city's economic base, as well as the non-profit agencies whose daily mission is to protect natural habitats, preserve historic neighborhoods, and improve social services.

Each section of this chapter compiles both the proposed infrastructure projects and corresponding development opportunities proposed in the Civic Master Plan by sector, and provides basic elements of the project – such as the site size and magnitude of cost – as well as a reference to the section(s) in this plan where the project is detailed. Where project costs are known or can be reasonably estimated they are so noted. Otherwise, investments are estimated on an order of magnitude scale as follows:

\$ - \$0 to \$250,000

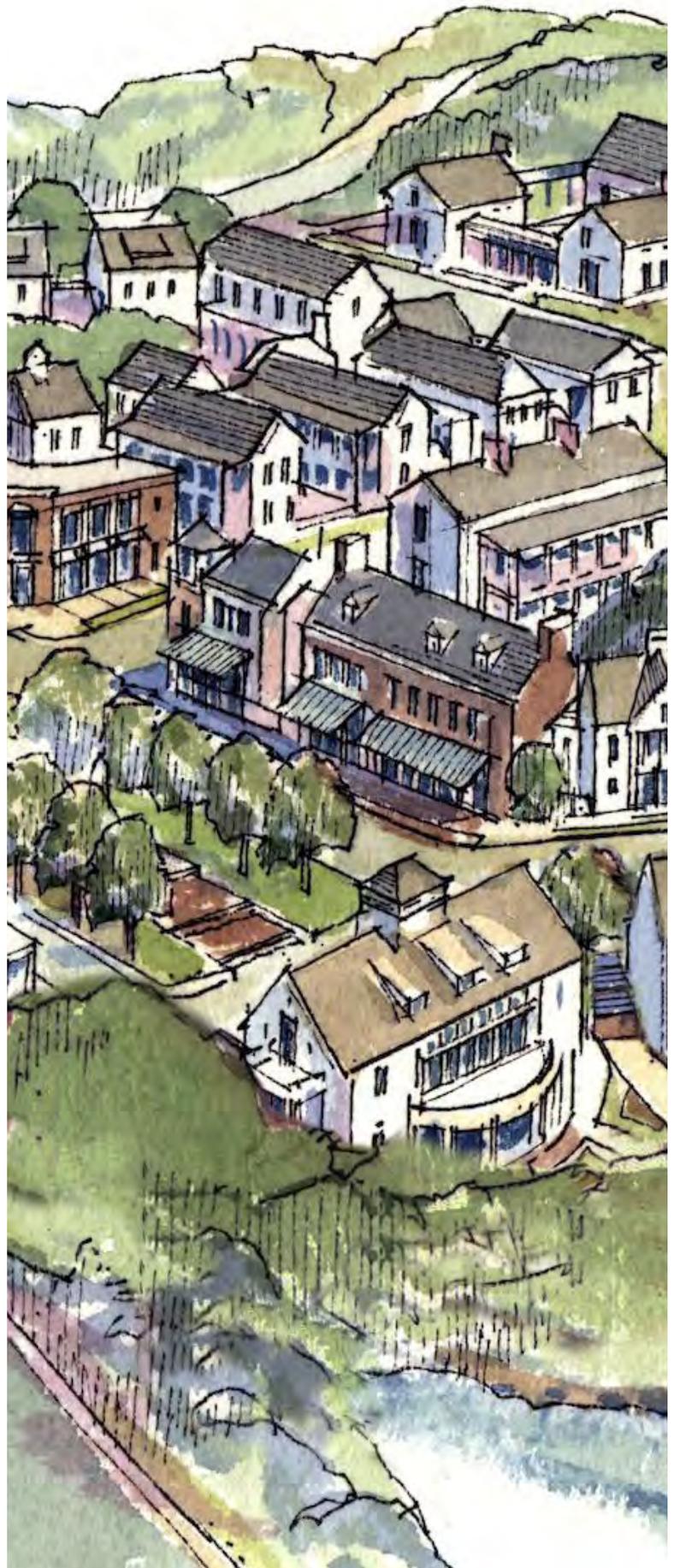
\$\$ - \$250,000 to \$500,000

\$\$\$ - \$500,000 to \$1,000,000

\$\$\$\$ - \$1,000,000+

The following maps and tables begin to prioritize civic infrastructure investments by identifying the expected time frame for each project. The planning, management, and funding responsibilities of these projects are not meant to be undertaken by the city alone. Other partners may include – but are not limited to – Beaufort County, the Beaufort Open Land Trust, the South Carolina Department of Transportation, and other non-profit groups and governmental agencies.

Taken in isolation, the civic infrastructure projects identified in this plan will maintain critical systems for the general health and well being of the City of Beaufort. When viewed in concert with the conceptual development and redevelopment opportunities these projects become the catalysts for a new generation of investment in the city.

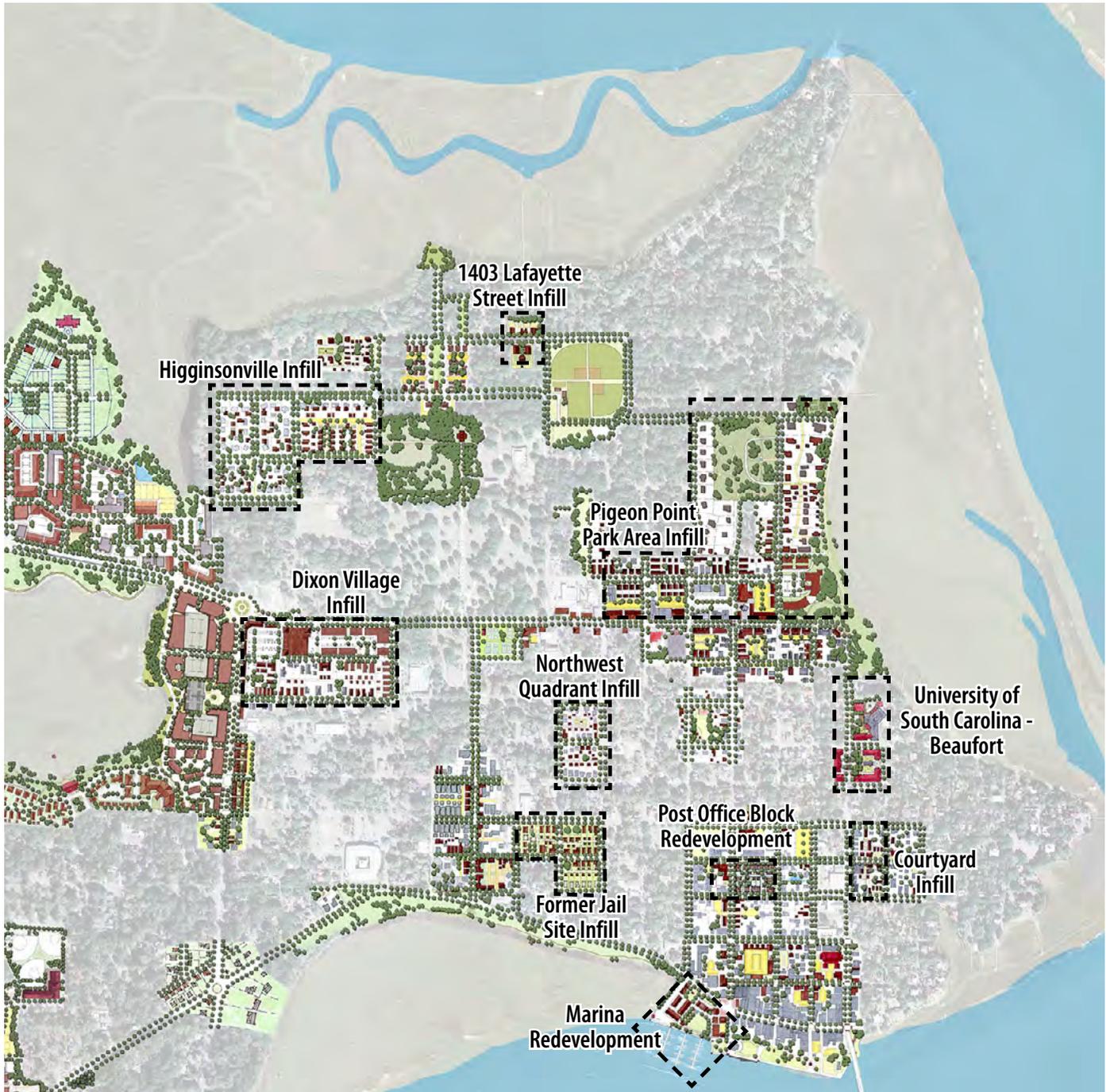


11.1 Sector 1 Projects



▲ SECTOR 1 CIVIC INFRASTRUCTURE PROJECTS

Streetscape Project	Existing Civic Buildings	Major Trailhead	Canoe/Kayak Launch
New Street Connection	Existing Green Infrastructure	Potential Future Trailhead	Air Installation Compatible Use Zone (AICUZ)
On-Street Ped/Bike Route (bike lanes/sharrows)	Proposed Green Infrastructure	Trail Access	
Off-Street Ped/Bike Route (multi-use path)	Spanish Moss Trail	Canoe/Kayak Route	



▲ SECTOR 1 CONCEPTUAL DEVELOPMENT/REDEVELOPMENT OPPORTUNITIES

11: Project Implementation

Civic Master Plan References			Sector 1 Civic Infrastructure Projects		
Diagram Reference	Sector 1 Project Name	Section Reference	Project Schedule	Site Data	Estimated Cost
1-BCE	Bellamy Curve Enhancements	2.6/3.8	Years 6-10 (2019-2023)	Approximately 1.2 acres	\$\$
1-BGP	Basil Green Park Improvements	4.4	Years 1-2 (2014-2016)	Approximately 8.29 acres	\$1,068,240
1-BRP	Bay & Ribaut Park Area	4.7	Years 3-5 (2014-2016)	Approximately 0.3 acres	\$73,860
1-BSB	Bay Street Boardwalk	2.5	Years 6-10 (2019-2023)	Approximately 3,300 LF	\$\$
1-BSE	Boundary Street (East of Ribaut) Road Diet Phase 1	3.8	Years 1-2 (2014-2015)	Approximately 1,540 LF	\$34,269
1-BSE	Boundary Street (East of Ribaut) Road Diet Phase 2	3.8	Years 1-2 (2014-2015)	Approximately 1,360 LF	\$\$
1-DPG	Downtown Parking Garage	2.2	Years 11-20 (2026-2033)	Approximately 2.32 acre redevelopment site	\$\$\$
1-DSS	Duke Street Streetscape -Phase 1 (Bladen to Harrington)	5.6/10.2	Years 1-2 (2014-2015)	Approximately 1,015 LF	\$675,990
1-DSS	Duke Street Streetscape - Phase 2 (Harrington to Charles)	5.6/10.2	Years 11-20 (2026-2033)	Approximately 1,050 LF	\$\$\$\$
1-DSS	Duke Street Streetscape -Phase 3 (Ribaut to Bladen)	5.6/10.2	Years 11-20 (2026-2033)	Approximately 1,950 LF	\$\$\$
1-FMP	Farmer's Market Pavilion	2.2	Years 6-10 (2019-2023)	Part of Marina Redevelopment	\$\$
1-GSS	Greene Street Streetscape	5.6/10.2	Years 1-2 (2014-2015)	Approximately 5,200 LF	\$3,463,200
1-PRF	Port Republic Festival Street Improvements	3.3	Years 6-10 (2019-2023)	640 LF of frontage along Port Republic Street	\$\$\$
1-SYP	Sycamore Park	6.2	Years 3-5 (2014-2016)	Approximately 1.3 acres (2 properties)	\$\$
1-SSS	Sycamore Street Streetscape	5.6/10.2	Years 3-5 (2014-2016)	Approximately 1,780 LF	\$717,900
1-VCE	Visitor Center	2.2	Years 6-10 (2019-2023)	Either as Part of Marina Redevelopment or Downtown Parking Garage Development	\$\$\$
1-WSP	Washington Street Park	4.2	Years 3-5 (2014-2016)	Approximately 1.15 acres	\$\$
–	Bicycle Improvements-Dedicated Bike Facilities	5.3	Years 1-2 (2014-2015)	Approximately 6,000 LF	\$\$
–	Bicycle Sharrows	5.3	Years 1-2 (2014-2015)	Approximately 19,250 LF with Marking every 230 feet (80)	\$1,900 each = \$152,000
–	Downtown Wayfinding Signage	3.9	Years 1-2 (2014-2015)	Approximately N/A	-\$-\$
–	National Historic Landmark District	3.1	On-Going	Stabilization, renovation, and restoration of individual contributing resources	varies
–	Pedestrian Crossings	5.3	Years 1-2 (2014-2015)	21 Crossings	\$1,500 each + \$13.50/LF

\$ - \$0 to \$250,000

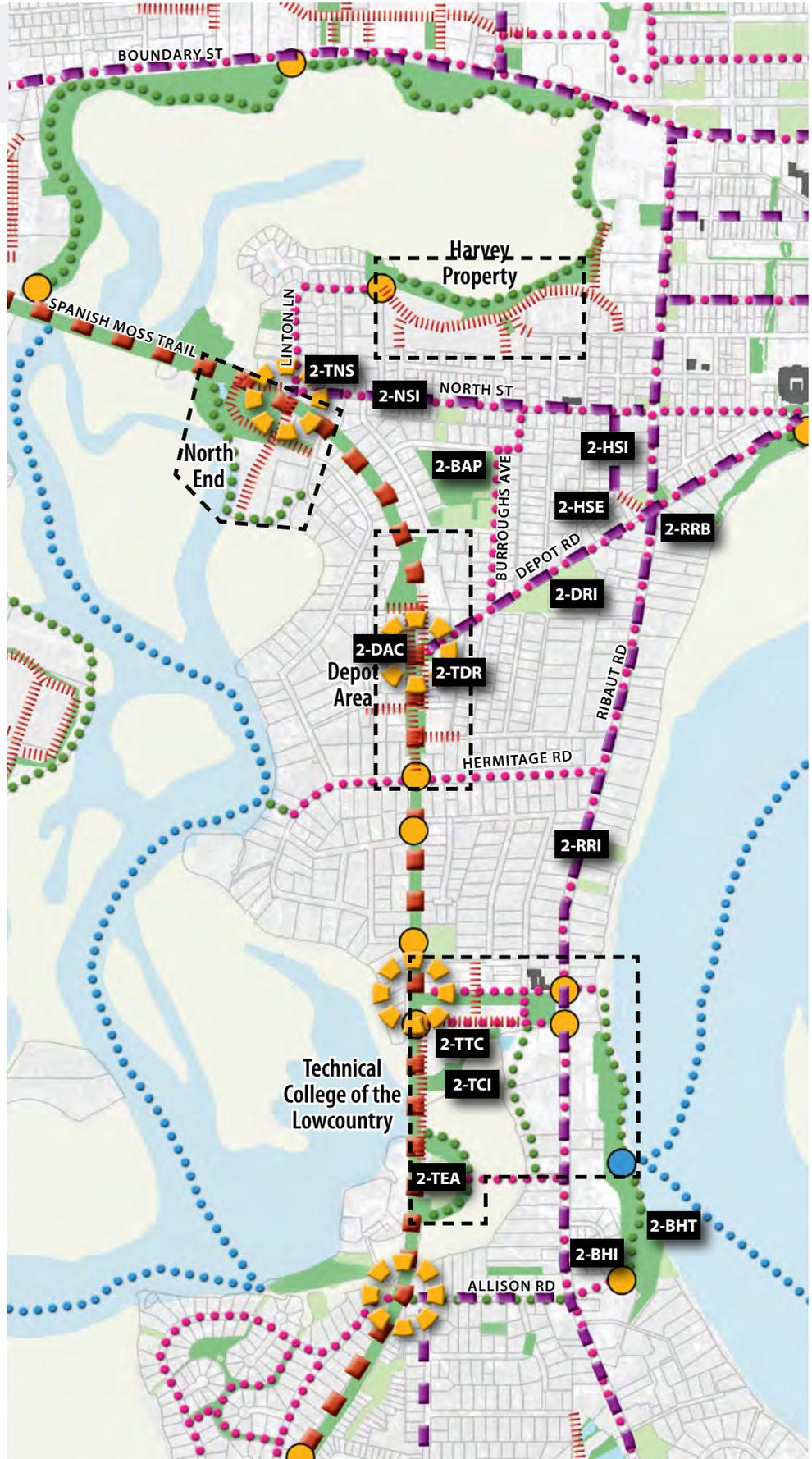
\$\$ - \$250,000 to \$500,000

\$\$\$ - \$500,000 to \$1,000,000

\$\$\$\$ - \$1,000,000+

Civic Master Plan References		Sector 1 Development/Redevelopment Project Information			
Sector 1 Project Name	Section Reference	Civic Investment Required	Approx. Site Acreage	Conceptual Development Opportunity Depicted in Plan (Final Yield Determined by Zoning)	RDC Action
1403 Lafayette Street Infill	6.2	<ul style="list-style-type: none"> City-owned land 	1.75 acres	<ul style="list-style-type: none"> Multi-family housing: 24 units 	Requires RDC Attention
Courtyard Infill	3.5	<ul style="list-style-type: none"> Carteret Street Improvements 	6 acres	<ul style="list-style-type: none"> Commercial Mixed-Use: 16,000 square feet Housing: 6 Units 	Long-term redevelopment
Dixon Village Redevelopment	6.2	TBD	16.5 acres	<ul style="list-style-type: none"> Single-family housing: 24 units Multi-family housing: 32 units Retail: 117,000 square feet Office: 177,000 square feet 	Long-term redevelopment
Former Jail Site Infill	6.2	TBD	5.5 acres	<ul style="list-style-type: none"> Multi-family housing: 82 units Single-family housing: 7 units Retail/Office (jail building reuse) 	Requires RDC Attention
Higginsonville Infill	6.2	TBD	19 acres	<ul style="list-style-type: none"> Single-family housing: 35 units Multi-family housing: 28 units Outdoor pavilion 	Requires RDC Attention
Marina Redevelopment	2.2/2.3	<ul style="list-style-type: none"> Henry C. Chambers Waterfront Park Expansion Downtown Parking Garage 	3 acres	<ul style="list-style-type: none"> Multi-family Housing: 30 units Mixed Use: 30,000 square feet Civic: 5 buildings 	Requires RDC Attention
Northwest Quadrant Infill - Phase 1	6.2	<ul style="list-style-type: none"> Greene Street streetscape improvements Duke Street streetscape improvements 	3.5 acres	<ul style="list-style-type: none"> Duplex: 2 units Single-Family housing: 4 units Accessory dwellings: 2 units 	Requires RDC Attention
Northwest Quadrant Infill - Phase 2	6.2	<ul style="list-style-type: none"> Greene Street streetscape improvements Duke Street streetscape improvements 	3.5 acres	<ul style="list-style-type: none"> Single-family housing: 10 units Accessory Dwellings: 14 units 	Requires RDC Attention
Pigeon Point Park Area Infill	6.2	TBD	36 acres	<ul style="list-style-type: none"> Single-family housing: 48 units Multi-family housing: 108 units Office: 55,000 square feet Retail: 57,000 square feet 	Long-term redevelopment
Post Office Block	6.2	<ul style="list-style-type: none"> Charles Street streetscape improvements North Street streetscape improvements 	3 acres	<ul style="list-style-type: none"> Single Family and Two-Family Housing: 13 units 	Long-term redevelopment
University of South Carolina - Beaufort	7.1	<ul style="list-style-type: none"> Boundary Street Road Diet, Bellamy Curve Improvements 	5+ acres	<ul style="list-style-type: none"> Student housing: 300-500 units Classroom: 105,000 square feet 	Long-term redevelopment

11.2 Sector 2 Projects



- Streetscape Project
- New Street Connection
- On-Street Ped/Bike Route (bike lanes/sharrows)
- Off-Street Ped/Bike Route (multi-use path)
- Existing Civic Buildings
- Existing Green Infrastructure
- Proposed Green Infrastructure
- Spanish Moss Trail
- Major Trailhead
- Potential Future Trailhead
- Trail Access
- Canoe/Kayak Route
- Canoe/Kayak Launch
- Air Installation Compatible Use Zone (AICUZ)

▲ SECTOR 2 CIVIC INFRASTRUCTURE PROJECTS



▲ SECTOR 2 CONCEPTUAL DEVELOPMENT/REDEVELOPMENT OPPORTUNITIES

11: Project Implementation

Civic Master Plan References			Sector 2 Civic Infrastructure Projects		
Diagram Reference	Sector2 Project Name	Section Reference	Project Schedule	Site Data	Estimated Cost
2-BAP	Burroughs Avenue Park	4.8	Years 6-10 (2019-2023)	Approximately 4.42 acres	\$\$
2-BHI	BMHS Campus Expansion/ Improvements	7.3	Years 6-10 (2019-2023)	N/A	\$\$\$\$
2-BHT	Beaufort Memorial Hospital Fitness Trail	7.3	Years 3-5 (2014-2016)	Unknown	\$
2-DAC	Spanish Moss Trail: Depot Area Rail Trail Amenity Center	4.9	Years 1-2 (2014-2015)	N/A	\$
2-DRI	Depot Road Improvements (Sidewalks on South Side, Lighting, Bike Lanes)	9.1	Years 3-5 (2014-2016)	Approximately 2,000 LF	\$\$
2-HSE	Heyward Street Extension (Purchase of ROW for connection to Ribaut)	5.8	Years 11-20 (2026-2033)	Approximately 380 LF	\$\$
2-HSI	Heyward Street Improvements (New Street Section)	5.8	Years 6-10 (2019-2023)	Approximately 1,300 LF	\$\$
2-NSI	North Street Improvements	3.7	Years 11-20 (2026-2033)	Approximately 2,790 LF	\$\$
2-RR1	Ribaut Road Complete Street Improvements-Phase 1 (Boundary to Bay)	8.2	Years 6-10 (2019-2023)	Approximately 3,000 LF	TBD
2-RR1	Ribaut Road Complete Streets Improvement-Phase 2 (Bay St to TCL Campus)	5.8	Years 6-10 (2019-2023)	Approximately 4,000 LF	TBD
2-RR1	Ribaut Road Complete Street improvements-Phase 3 (TCL Campus to Port Royal)	5.8	Years 6-10 (2019-2023)	Approximately 9,630 LF	TBD
2-RRB	Roundabout at Ribaut Road & Bay/ Depot Streets	8.2	Years 6-10 (2019-2023)	N/A	\$\$
2-TCI	TCL Campus Expansion/ Improvements	7.2	Years 6-10 (2019-2023)	Unknown	\$\$\$
2-TDR	Spanish Moss Trail Trailhead: Depot Road	4.7	Years 1-2 (2012-2013)	N/A	\$
2-TEA	TCL/BMHS Marsh Trail (Elliot St to Allison Rd)	2.10/ 5.3/ 7.2	Ongoing	Approximately 2,300 LF	\$\$
2-TNS	Spanish Moss Trail Trailhead: North Street	4.7	Years 1-2 (2012-2013)	N/A	\$
2-TTC	Spanish Moss Trail Trailhead: TCL	5.2	Years 1-2 (2014-2015)	N/A	\$
–	Street Drainage Improvements	N/A	Years 11-20 (2026-2033)	N/A	\$\$\$
–	Waterfront Access: General Access	2.10/2.11	Ongoing	N/A	\$\$

\$ - \$0 to \$250,000

\$\$ - \$250,000 to \$500,000

\$\$\$ - \$500,000 to \$1,000,000

\$\$\$\$ - \$1,000,000+

Civic Master Plan References		Sector 2 Development/Redevelopment Project Information			
Sector 2 Project Name	Section Reference	Civic Investment Required	Approx. Site Acreage	Conceptual Development Opportunity Depicted in Plan (Final Yield Determined by Zoning)	RDC Action
Depot Road Area Infill	9.1	<ul style="list-style-type: none"> Spanish Moss Trail: Trailhead at Depot Road Spanish Moss Trail: Depot Area Rail Trail Amenity Center 	20 acres	<ul style="list-style-type: none"> Civic: 800 square feet Single-family housing: 14 units Mixed-use: 134,900 square feet 	Requires RDC Attention
Harvey Property Development	6.3	<ul style="list-style-type: none"> N/A 	17 acres	<ul style="list-style-type: none"> Single-family housing: 29 units Senior housing: 136 units 	Long-term redevelopment
Salzer / Marshes Tracts	6.3	<ul style="list-style-type: none"> Spanish Moss Trail Phase 2 (Depot Road to Parris Island Gateway - SC 802/US 21) Spanish Moss Trail Trailhead: North Street 	14 acres	<ul style="list-style-type: none"> Civic: 2,000 square feet Single-family housing: 69 units 	Long-term redevelopment
Technical College of the Lowcountry	7.2	<ul style="list-style-type: none"> Ribaut Road Improvements 	35 acres	<ul style="list-style-type: none"> Main campus build-out: 512,000 square feet Polk Island - Civic: 10,000 square feet (Polk Island) Polk Island - Cottage housing: 12 units (potentially rental) 	Long-term redevelopment



▲ SECTOR 3 CONCEPTUAL DEVELOPMENT/REDEVELOPMENT OPPORTUNITIES

11: Project Implementation

Civic Master Plan References			Sector 3 Civic Infrastructure Projects		
Diagram Reference	Sector 3 Project Name	Section Reference	Project Schedule	Site Data	Estimated Cost
3-AHI	Arthur Horne Nature Preserve Improvements	4.11	Years 1-2 (2014-2015)	Approximately 17.5 acres	\$
3-ARI	Allison Road Improvements	7.3	Years 1-2 (2014-2015)	Approximately 1,085 LF	\$480,000
3-BCI	Battery Creek Road Improvements (Sidewalk)	5.6/10.2	Years 6-10 (2019-2023)	Approximately 10,400 LF	\$\$
3-FBI	First Boulevard Improvements (Sharrows/Bike Lanes & Sidewalks on North Side)	8.2	Years 6-10 (2019-2023)	Approximately 2,870 LF	\$\$
3-FSH	New Fire Station Headquarters	7.4	Years 1-2 (2014-2015)	Varies/Multiple Sites	\$\$
3-MOI	Mossy Oaks Road Improvements	5.6	Years 6-10 (2019-2023)	Approximately 4,260 LF	\$\$
3-MOW	Mossy Oaks Waterfront Access	2.8	Years 6-10 (2019-2023)	N/A	\$\$
3-RII	Ribaut Road Complete Street Improvements (Allison Road to Lady's Island Drive)	8.2	Years 11-20 (2026-2033)	N/A	TBD
3-SPI	Southside Park-Step 1 (Final Planning/Engineering & Interim Use as Urban Farm)	4.10	Years 1-2 (2014-2015)	Approximately 8 acres	\$\$
3-SPI	Southside Park -Step 2 (Construction of Phase 1 Improvements)	4.10	Years 3-5 (2014-2016)	Approximately 20 acres	\$\$\$
3-SPI	Southside Park-Step 3 (Construction of Final Phase)	4.10	Years 6-10 (2019-2023)	Approximately 20 acres (40 acres total)	\$\$
3-TAP	Spanish Moss Trail Phase 3 (Allison Road to Port Royal)	5.2	Years 6-10 (2019-2023)	Approximately 12,000 LF	\$\$
3-TAR	Spanish Moss Trail Trailhead: Allison Road	4.7/5.2	Years 1-2 (2014-2015)	N/A	\$
3-TBS	Spanish Moss Trail Trailhead: Broad Street	5.2	Years 6-10 (2019-2023)	N/A	\$
3-WGI	Waddell Gardens Nature Preserve Improvements	4.12	Years 11-20 (2026-2033)	Approximately 7.57 acres	\$\$
3-WRI	Waddell Road Improvements	6.4	Years 6-10 (2019-2023)	Approximately 4,687 LF	\$\$\$

\$ - \$0 to \$250,000

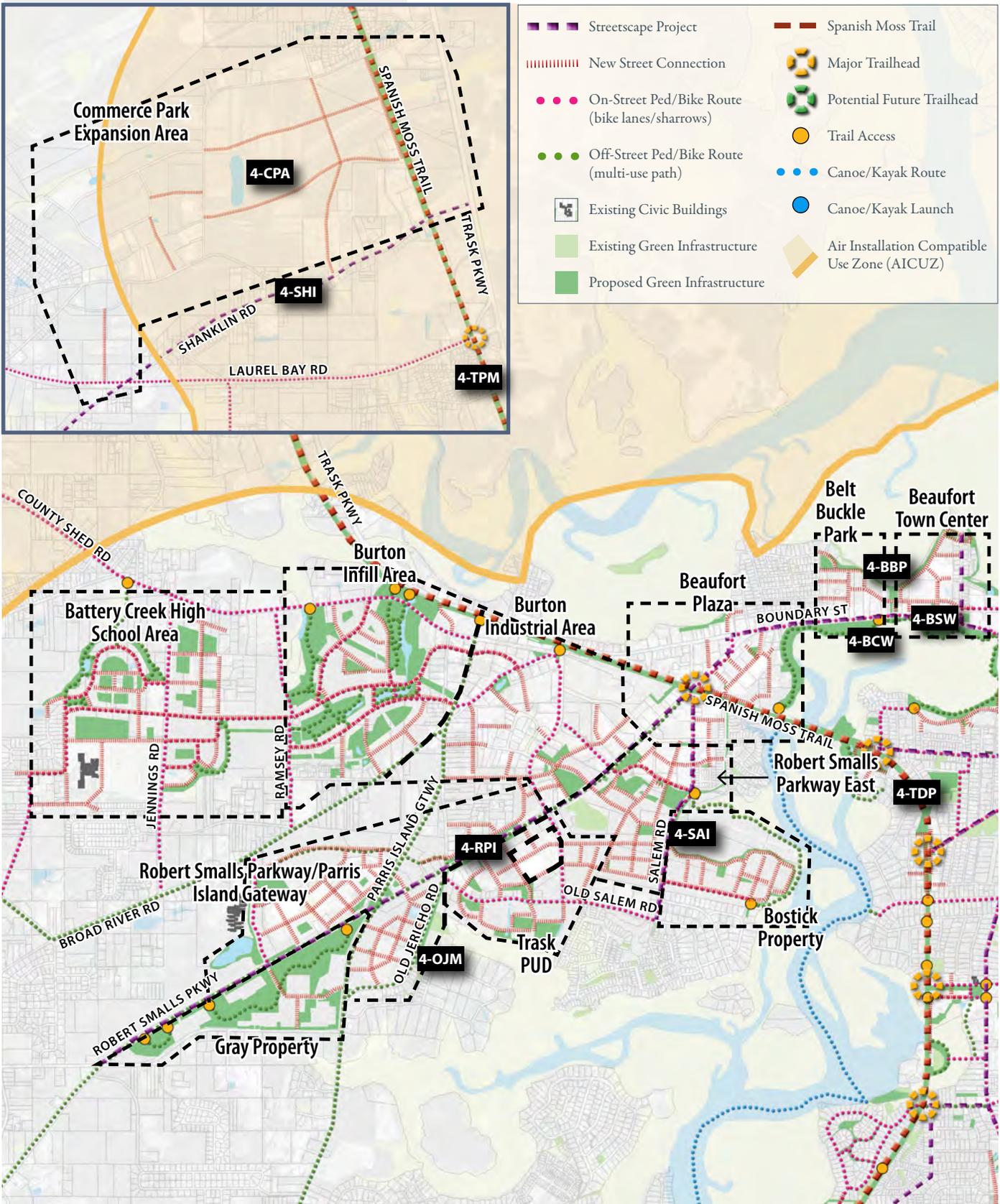
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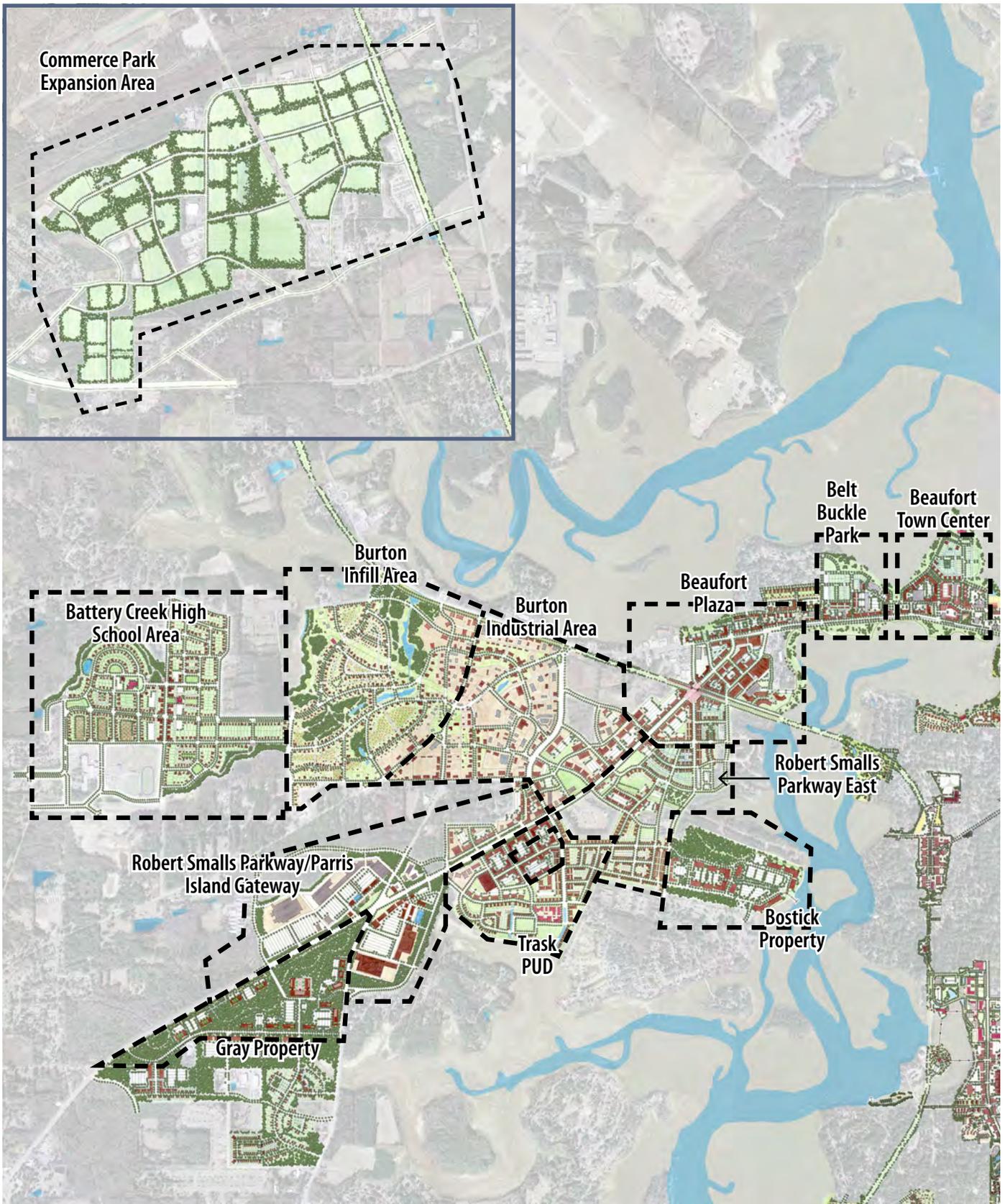
\$\$\$\$ - \$1,000,000+

Civic Master Plan References		Sector 3 Development/Redevelopment Project Information			
Sector 3 Project Name	Section Reference	Civic Investment Required	Approx. Site Acreage	Conceptual Development Opportunity Depicted in Plan (Final Yield Determined by Zoning)	RDC Action
Allison Road Infill/ Redevelopment	6.4	<ul style="list-style-type: none"> Allison Road Improvements Spanish Moss Trail Trailhead: Allison Road 	33 acres	<ul style="list-style-type: none"> Office: 363,000 square feet Retail: 117,000 square feet Single-family housing: 27 units Multi-family/student housing: 205 units Senior housing: 176 units 	Long-term redevelopment
Battery Creek Road Redevelopment	6.4	<ul style="list-style-type: none"> TBD 	15 acres	<ul style="list-style-type: none"> Single-family housing: 35 units 	Long-term redevelopment
Beaufort Memorial Hospital	7.3	<ul style="list-style-type: none"> Ribaut Road Improvements Allison Road Improvements 	17 acres	<ul style="list-style-type: none"> Hospital and Medical Offices: 370,000 square feet (5-story bed tower) 	Long-term redevelopment
Brotherhood Road Redevelopment	6.4	<ul style="list-style-type: none"> Spanish Moss Trail Phase 3 (Allison Road to Port Royal) Mossy Oaks Waterfront Access 	21 acres	<ul style="list-style-type: none"> Single-family housing: 62 units Multi-family housing: 121 units 	Long-term redevelopment
Mystic Drive Infill	6.4	<ul style="list-style-type: none"> Ribaut Road (South of Allison Road) improvements 	27 acres	<ul style="list-style-type: none"> Single-family housing: 65 units (attached) Multi-family housing: 60 units Retail: 67,500 square feet Office: 82,500 square feet 	Long-term redevelopment
Rodgers Drive Redevelopment	6.4	<ul style="list-style-type: none"> Spanish Moss Trail Phase 3 (Allison Road to Port Royal) 	6.5 acres	<ul style="list-style-type: none"> Single-family housing: 35 units Civic: 5,000 square feet 	Long-term redevelopment
South Ribaut Road Neighborhood Infill	8.2	<ul style="list-style-type: none"> Ribaut Road (South of Allison Road) improvements 	56 acres	<ul style="list-style-type: none"> Single-family housing: 27 units Multi-family housing: 451 units Live/Work: 14 units Office/Retail: 148,150 	Long-term redevelopment
Southside Neighborhood Redevelopment	6.4	<ul style="list-style-type: none"> Southside Park - Step 1 (Final Planning/ Engineering & Interim Use as urban farm) Southside Park - Step 2 (Construction of Phase 1 Improvements) Southside Park - Step 3 (Construction of Final Phase) Waddell Road improvements 	143 acres	<ul style="list-style-type: none"> Single-family housing: 125 units Multi-family housing: 473 units Office/Retail: 36,000 square feet Civic: 18,000 square feet 	Requires RDC Attention
Waddell Road Infill	6.4	<ul style="list-style-type: none"> Southside Park - Step 1 (Final Planning/ Engineering & Interim Use as urban farm) Southside Park - Step 2 (Construction of Phase 1 Improvements) Southside Park - Step 3 (Construction of Final Phase) Waddell Road improvements 	18.5 acres	<ul style="list-style-type: none"> Single-family housing: 62 units Civic: 5,000 square feet 	Long-term redevelopment

11.4 Sector 4 Projects



▲ SECTOR 4 CIVIC INFRASTRUCTURE PROJECTS



▲ SECTOR 4 CONCEPTUAL DEVELOPMENT/REDEVELOPMENT OPPORTUNITIES

11: Project Implementation

Civic Master Plan References			Sector 4 Civic Infrastructure Projects		
Diagram Reference	Sector 4 Project Name	Section Reference	Project Schedule	Site Data	Estimated Cost
4-BBP	Belt Buckle Park	2.9/8.1	Years 11-20 (2026-2033)	Approximately 3.25 acres	\$\$\$\$
4-BCW	Battery Creek Marshes Viewshed and Waterfront Access	2.9	Years 3-5 (2014-2016)	Approximately 13 acres	\$\$\$\$
4-BSW	Boundary Street (West of Ribaut Road) Improvements	8.1	Years 1-2 (2014-2015)	Approximately 5,858 LF	\$\$\$\$
4-CPA	Commerce Park	9.2	Years 6-19 (2019-2032)	Approximately 557 acres	\$\$\$\$
4-OJM	Old Jericho Road Multi-Use Path	5.3	Years 11-20 (2026-2033)	Approximately 3,700 LF	\$\$
4-RPI	Robert Smalls Parkway Improvements	8.3	Years 11-20 (2026-2033)	Approximately 15,570 LF	\$\$
4-SAI	Salem Road Improvements	5.6/10.2	Years 11-20 (2026-2033)	Approximately 3,737 LF	\$\$
4-SHI	Shanklin Road Improvements	9.2	Years 11-20 (2026-2033)	Approximately 9,367 LF	\$\$
4-TDP	Spanish Moss Trail Phase 2 (Depot Road to Parris Island Gateways-SC 802/US 21)	5.2	Years 3-5 (2014-2016)	Approximately 22,387 LF	\$\$\$
4-TPM	Spanish Moss Trail Phase 3 (Parris Island Gateway-SC 802/US 21 to MCAS/Clarendon/Shanklin Rd)	5.2	Years 11-20 (2026-2033)	Approximately 10,960 LF	\$\$\$

\$ - \$0 to \$250,000

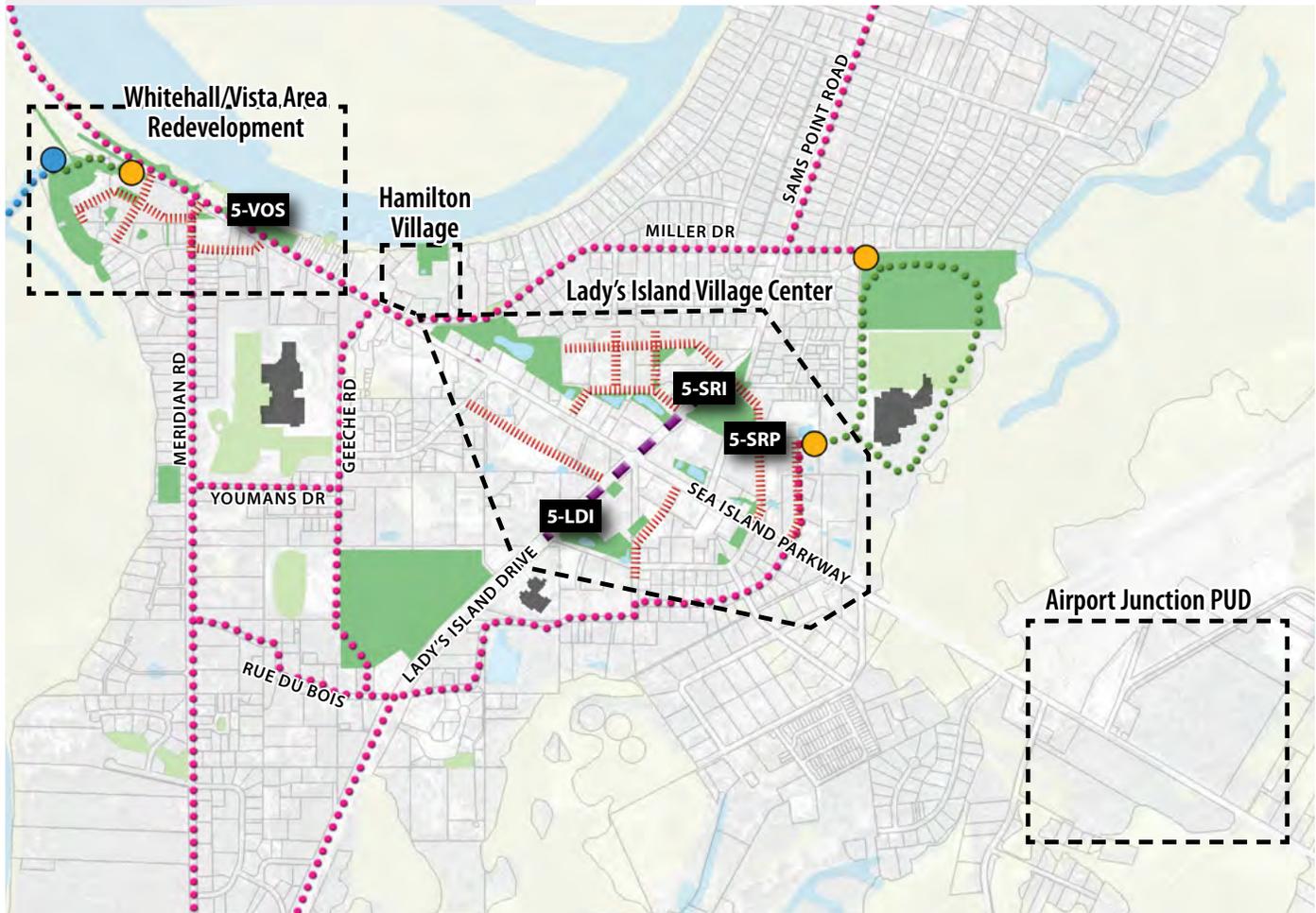
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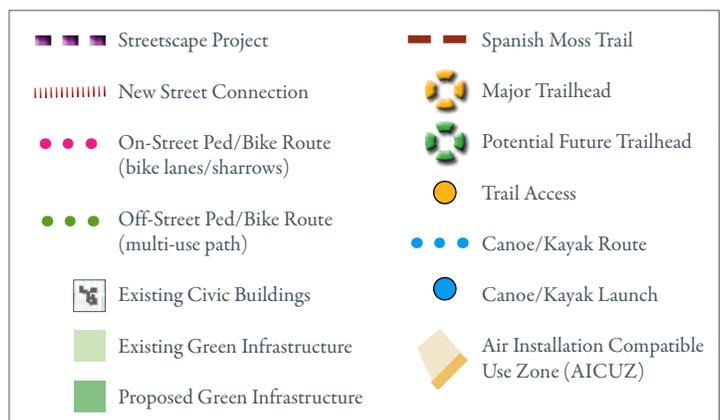
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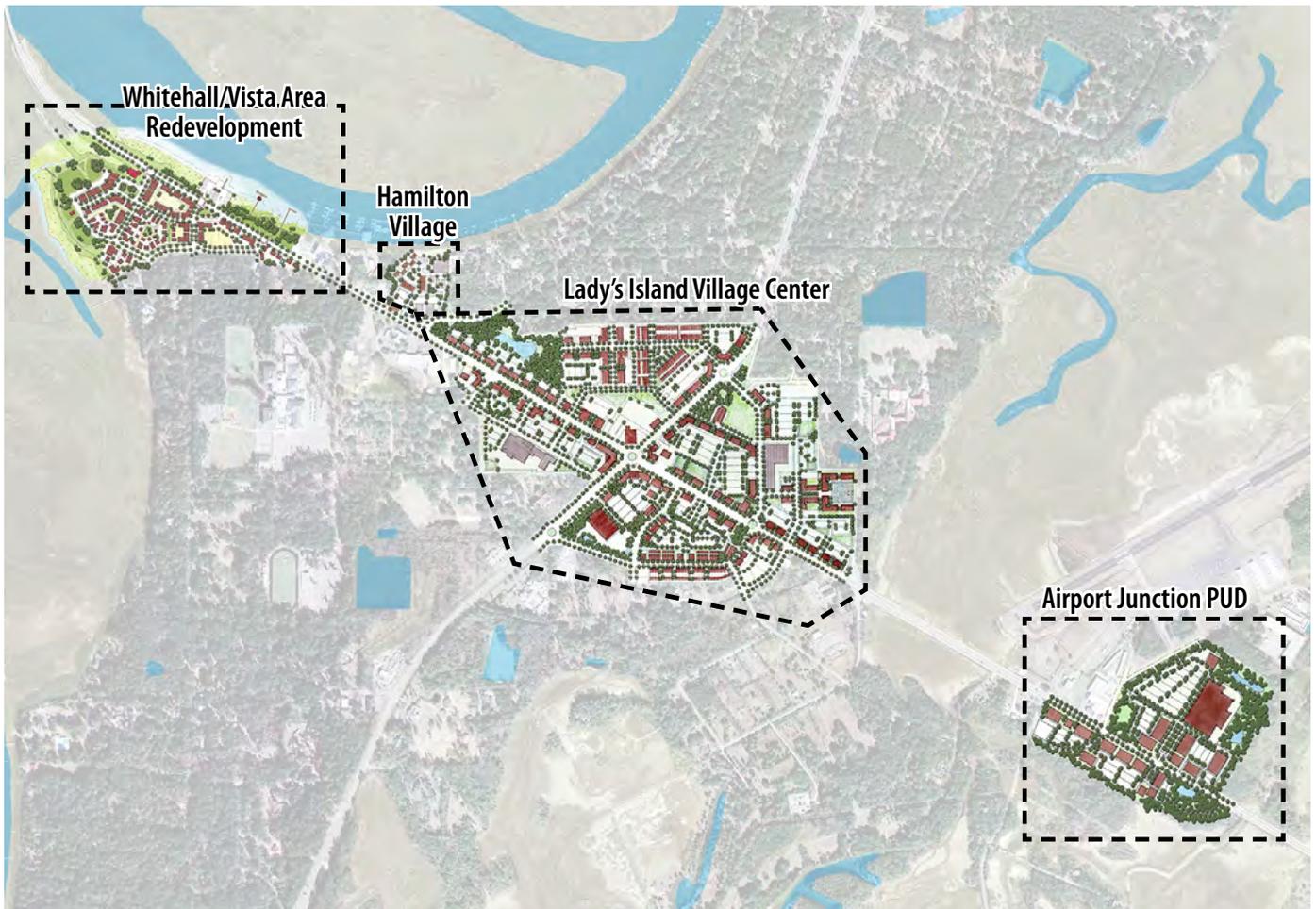
Civic Master Plan References		Sector 4 Development/Redevelopment Project Information			
Sector 4 Project Name	Section Reference	Civic Investment Required	Approx. Site Acreage	Conceptual Development Opportunity Depicted in Plan (Final Yield Determined by Zoning)	RDC Action
Battery Creek High School Area	6.5	TBD	205 acres	<ul style="list-style-type: none"> Civic: 10,000 square feet Single-family housing: 358 units Multi-family housing: 179 units Office/Retail: 26,000 square feet 	Long-term redevelopment
Beaufort Plaza	8.1	<ul style="list-style-type: none"> Spanish Moss Trail Phase 2 (Depot Road to Parris Island Gateway - SC 802/US 21) Spanish Moss Trail Trailhead: Beaufort Plaza Boundary Street (west of Ribaut Road) Improvements 	130 acres	<ul style="list-style-type: none"> Mixed-use/Office: 855,000 square feet Retail: 555,000 square feet Civic: 8,000 square feet Multi-family housing: 300 units 	Long-term redevelopment
Beaufort Town Center Redevelopment	8.1	<ul style="list-style-type: none"> Boundary Street (west of Ribaut Road) Improvements TBD 	64 acres	<ul style="list-style-type: none"> Single-family housing: 53 units Multi-family housing: 104 units Mixed-use: 420,500 square feet Retail: 271,500 square feet 	Approved PUD
Bostick Property (Strategic Opportunity Site)	9.4	<ul style="list-style-type: none"> Salem Road Improvements 	100 acres	<ul style="list-style-type: none"> Office Campus: 760,000 square feet Civic: 3,000 square feet 	Requires RDC Attention
Burton Industrial Area	9.3	<ul style="list-style-type: none"> TBD 	212 acres	<ul style="list-style-type: none"> Multi-family housing: 60 units Office: 470,000 square feet Retail: 70,000 square feet Industrial: 370,000 square feet 	Requires RDC Attention
Burton Infill Area	9.3	<ul style="list-style-type: none"> TBD 	266 acres	<ul style="list-style-type: none"> Single-family housing: 202 units Multi-family housing: 48 units Civic: 6,000 square feet Industrial: 128,000 square feet 	Long-term redevelopment
Commerce Park Expansion Area	9.2	<ul style="list-style-type: none"> Commerce Park Public Infrastructure Improvements Shanklin Road Improvements 	960 acres	<ul style="list-style-type: none"> Expansion to 960 acres from existing 165 acre Commerce Park site 	Requires RDC Attention
Gray Property (Strategic Opportunity Site)	9.4	<ul style="list-style-type: none"> Robert Smalls Parkway Improvements 	100 acre	<ul style="list-style-type: none"> Office Campus: 800,000 square feet 	Requires RDC Attention
Hogarth Street Neighborhood/Belt Buckle Park Area Redevelopment	6.5	<ul style="list-style-type: none"> Boundary Street (west of Ribaut Road) Improvements Belt Buckle Park 	35 acres	<ul style="list-style-type: none"> Single-family housing: 28 units (attached) Multi-family housing: 36 units Civic: 20,000 square feet Retail: 55,000 square feet Office/Mixed-use: 77,000 square feet 	Requires RDC Attention
Robert Smalls Parkway/Parris Island Gateway	8.3/10.2	<ul style="list-style-type: none"> Multi-Use Path Spanish Moss Trail Phase 3 (Parris Island Gateway SC 802/ US 21 to MCAS/ Clarendon) 	148 acres	<ul style="list-style-type: none"> Mixed-Use: 256,000 square feet 	Long-term redevelopment
Robert Smalls Parkway East	8.3/10.2	<ul style="list-style-type: none"> Multi-Use Path Spanish Moss Trail Phase 3 (Parris Island Gateway SC 802/ US 21 to MCAS/ Clarendon) 	96 acres	<ul style="list-style-type: none"> Single-family housing: 43 units (attached) Multi-family housing: 232 units Mixed-use/Office: 185,000 square feet Retail: 20,000 square feet 	Long-term redevelopment
Trask Property PUD	N/A	<ul style="list-style-type: none"> Robert Smalls Parkway Improvements 	115 acres	<ul style="list-style-type: none"> Retail: 228,300 square feet Office/Mixed-Use: 141,900 square feet 	Approved PUD

11.5 Sector 5 Projects



▲ SECTOR 5 CIVIC INFRASTRUCTURE PROJECTS





▲ SECTOR 5 CONCEPTUAL DEVELOPMENT/REDEVELOPMENT OPPORTUNITIES

11: Project Implementation

Civic Master Plan References			Sector 5 Civic Infrastructure Projects		
Diagram Reference	Sector 5 Project Name	Section Reference	Project Schedule	Site Data	Estimated Cost
5-LDI	Lady's Island Drive Improvements	2.7/6.6	Years 11-20 (2026-2033)	Approximately 2,200 LF	\$\$
5-SRI	Sams Point Road Improvements	10.3	Years 11-20 (2026-2033)	Approximately 1,170 LF	\$\$
5-SRP	Sams Point Road Park	4.15	Years 11-20 (2026-2033)	Approximately 1 acre	\$\$

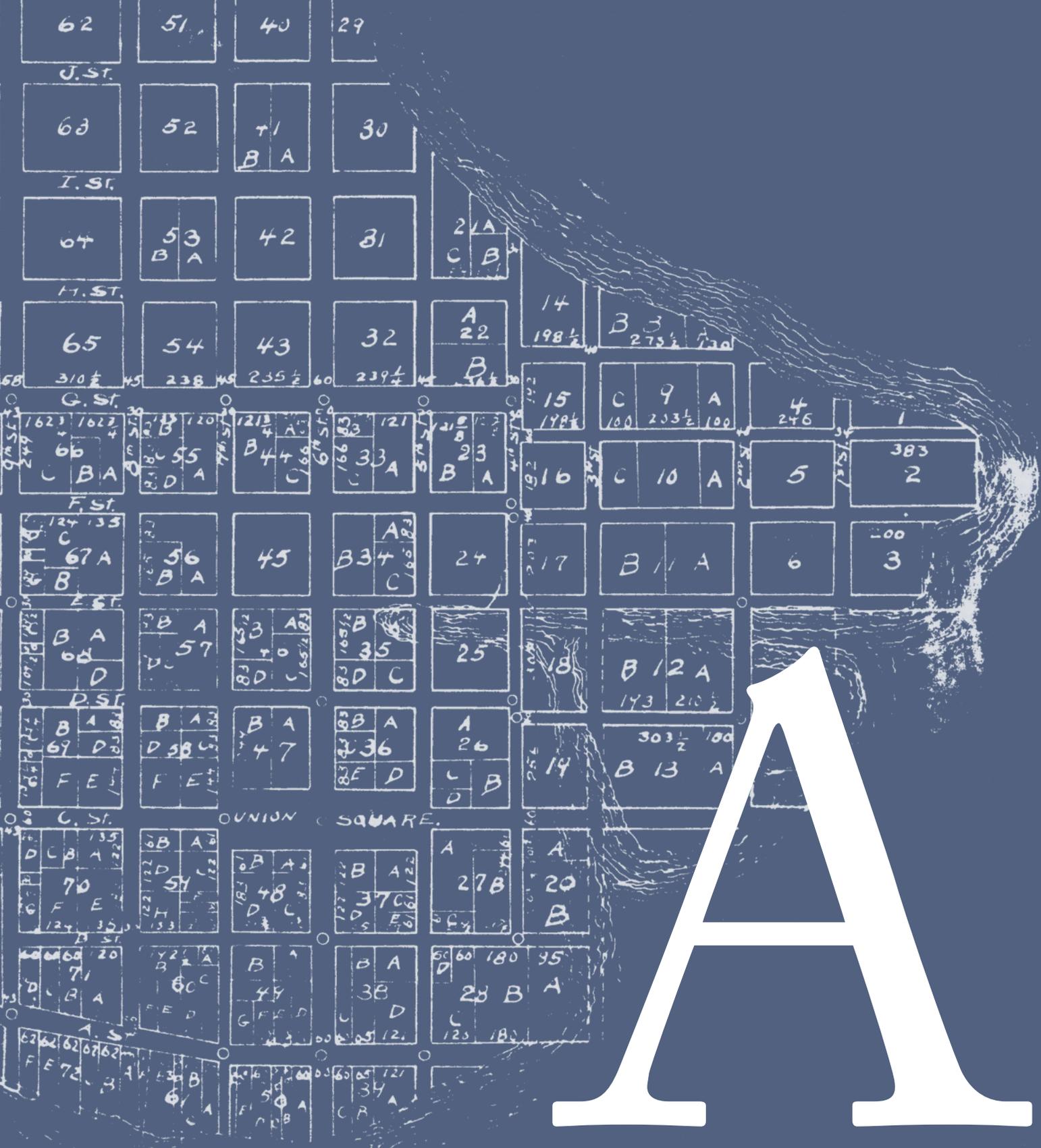
\$ - \$0 to \$250,000

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\$\$\$\$ - \$1,000,000+

Civic Master Plan References		Sector 5 Development/Redevelopment Project Information				
Sector 5 Project Name	Section Reference	Project Type	Civic Investment Required	Approx. Site Acreage	Conceptual Development Opportunity Depicted in Plan (Final Yield Determined by Zoning)	RDC Action
Airport Junction PUD	N/A	Private	<ul style="list-style-type: none"> N/A 	150 acres	<ul style="list-style-type: none"> Office/Mixed Use: 475,000 square feet Retail: 240,000 square feet 	Approved PUD
Hamilton Village Area Infill	6.6	Private	<ul style="list-style-type: none"> N/A 	7 acres	<ul style="list-style-type: none"> Single-family housing: 20 units (attached) Office: 5,000 square feet Retail: 5,000 square feet 	Long-term redevelopment
Lady's Island Village Center	8.4	Private	<ul style="list-style-type: none"> Lady's Island Drive Improvements Sams Point Road Improvements Sams Point Road Park 	128 acres	<ul style="list-style-type: none"> Single-family housing: 100 units Multi-family housing: 324 units Civic: 15,000 square feet Retail: 276,000 square feet Mixed-use: 459,000 square feet Office: 86,000 	Long-term redevelopment
Whitehall/Vista Area Redevelopment	6.6	Private	<ul style="list-style-type: none"> Vista II Civic/Open Space (already acquired by Open Land Trust) 	45 acres	<ul style="list-style-type: none"> Single-family housing: 13 units Multi-family housing: 25 units Civic: 4,000 square feet Mixed-use: 35,000 square feet Retail: 35,000 square feet 	Long-term redevelopment



APPENDIX: MAPS

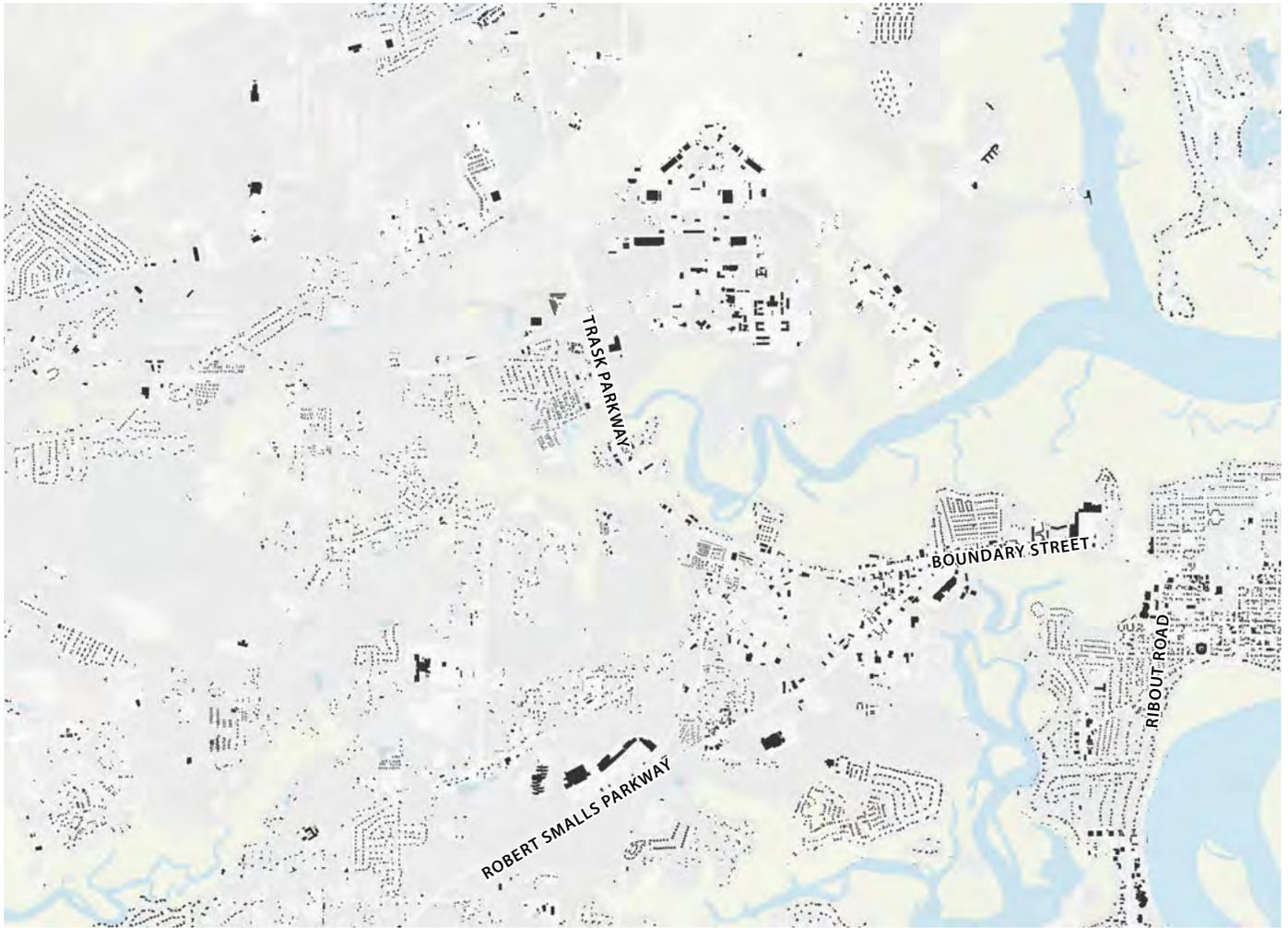


MAPS

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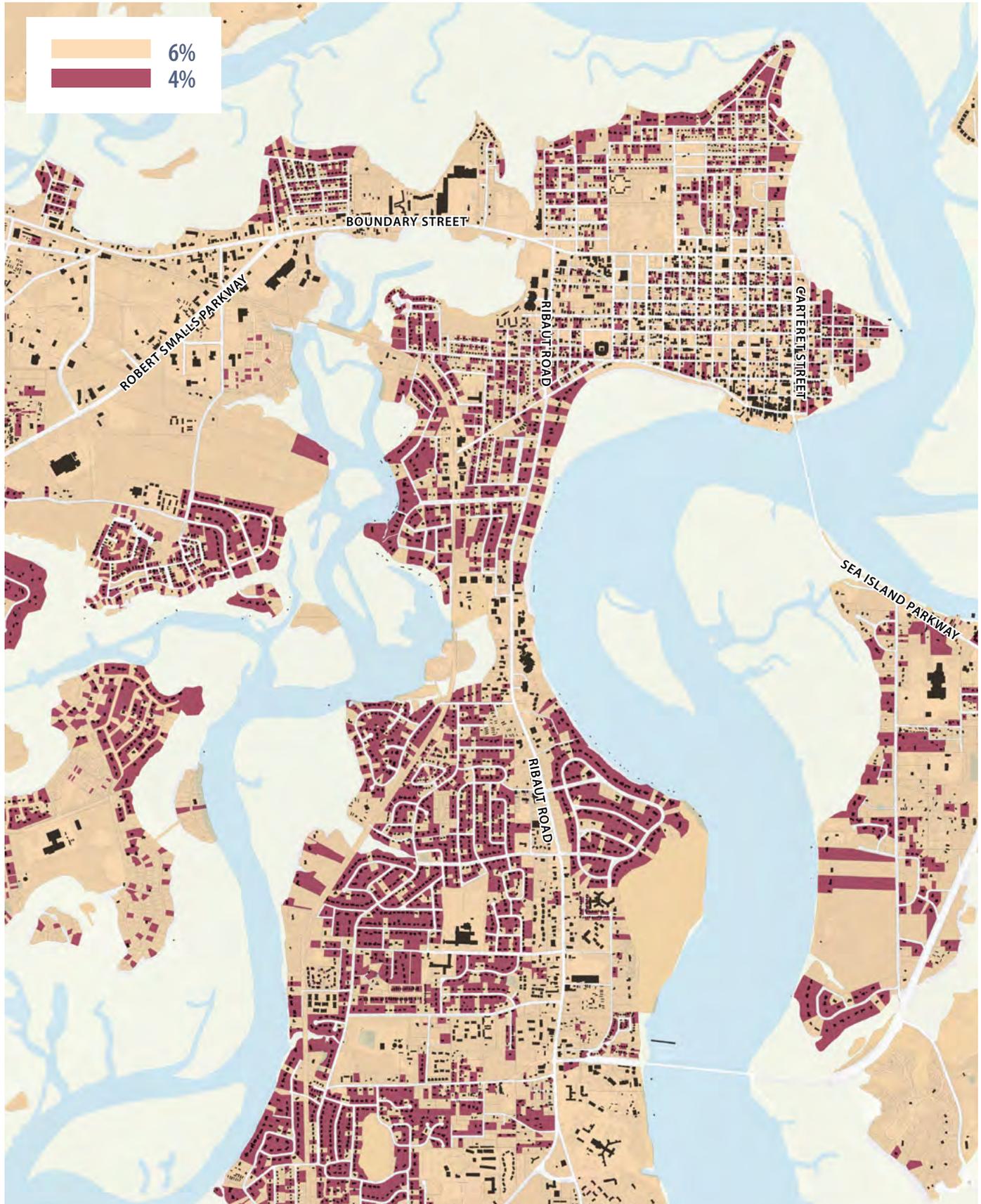
▲ BUILDING FOOTPRINTS SECTORS 1-3



▲ BUILDING FOOTPRINTS SECTOR 4

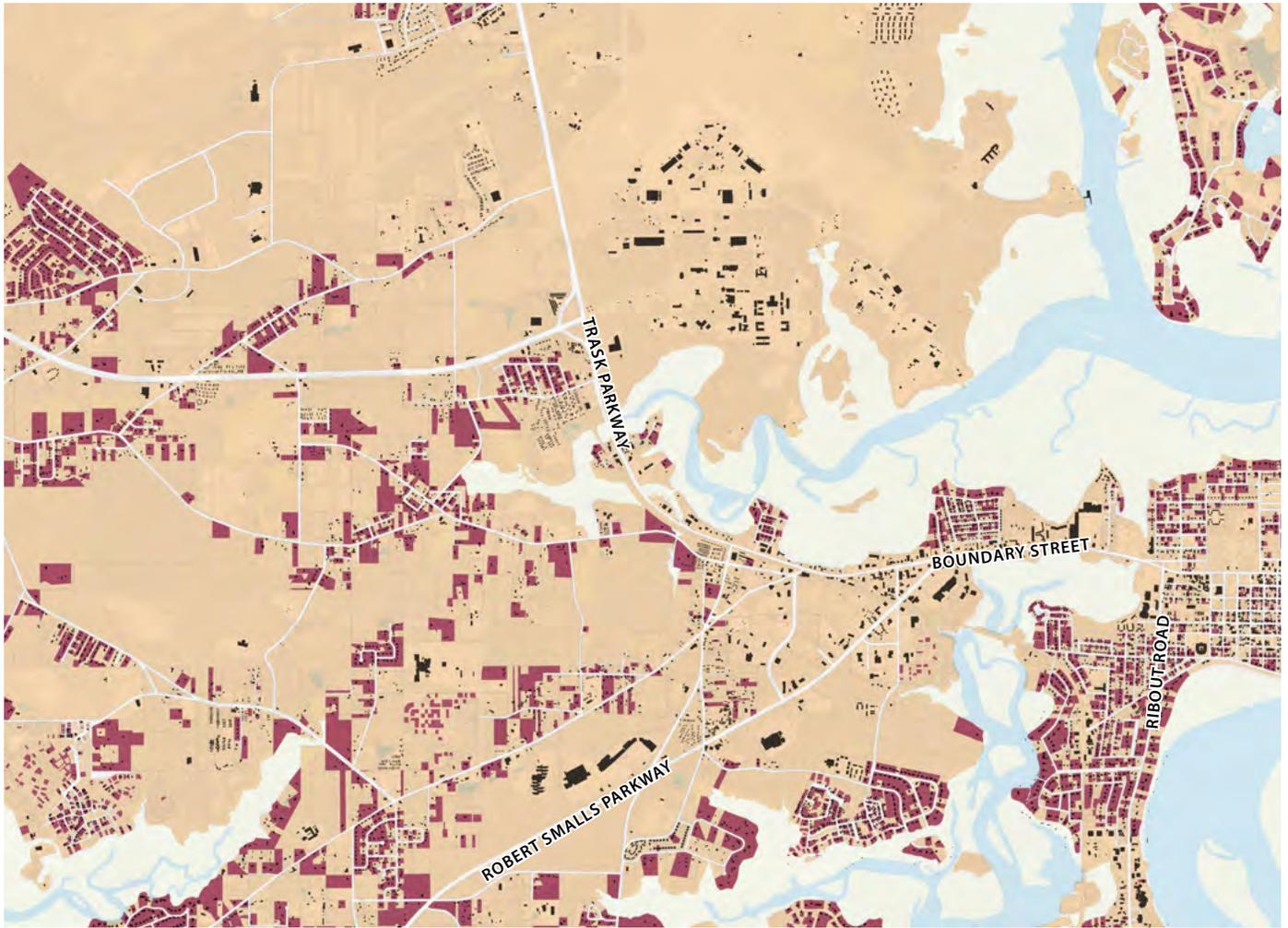


▲ BUILDING FOOTPRINTS SECTOR 5



▲ 4% AND 6% TAX RATES SECTORS 1-3

*As of March 2013



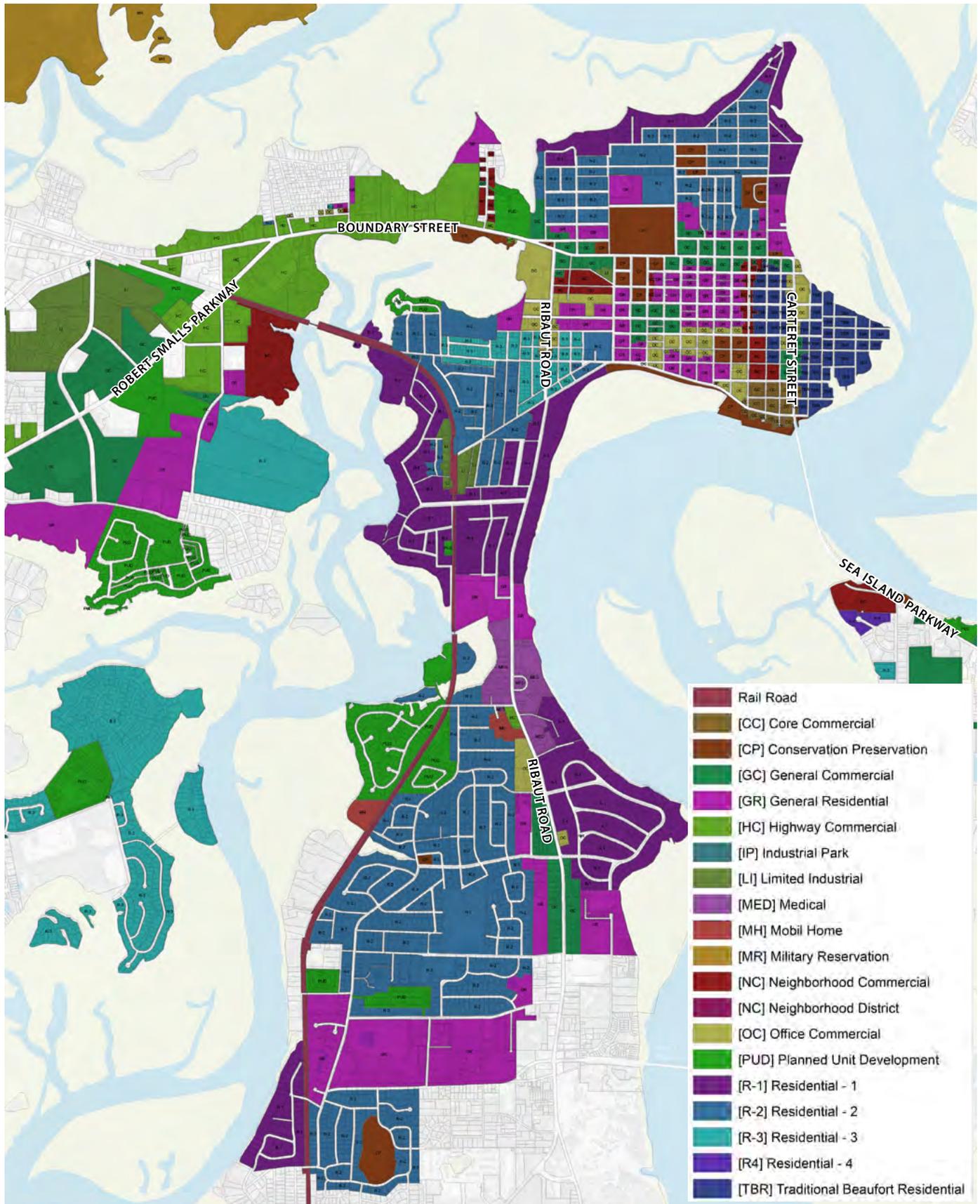
▲ 4% AND 6% TAX RATES SECTOR 4

*As of March 2013

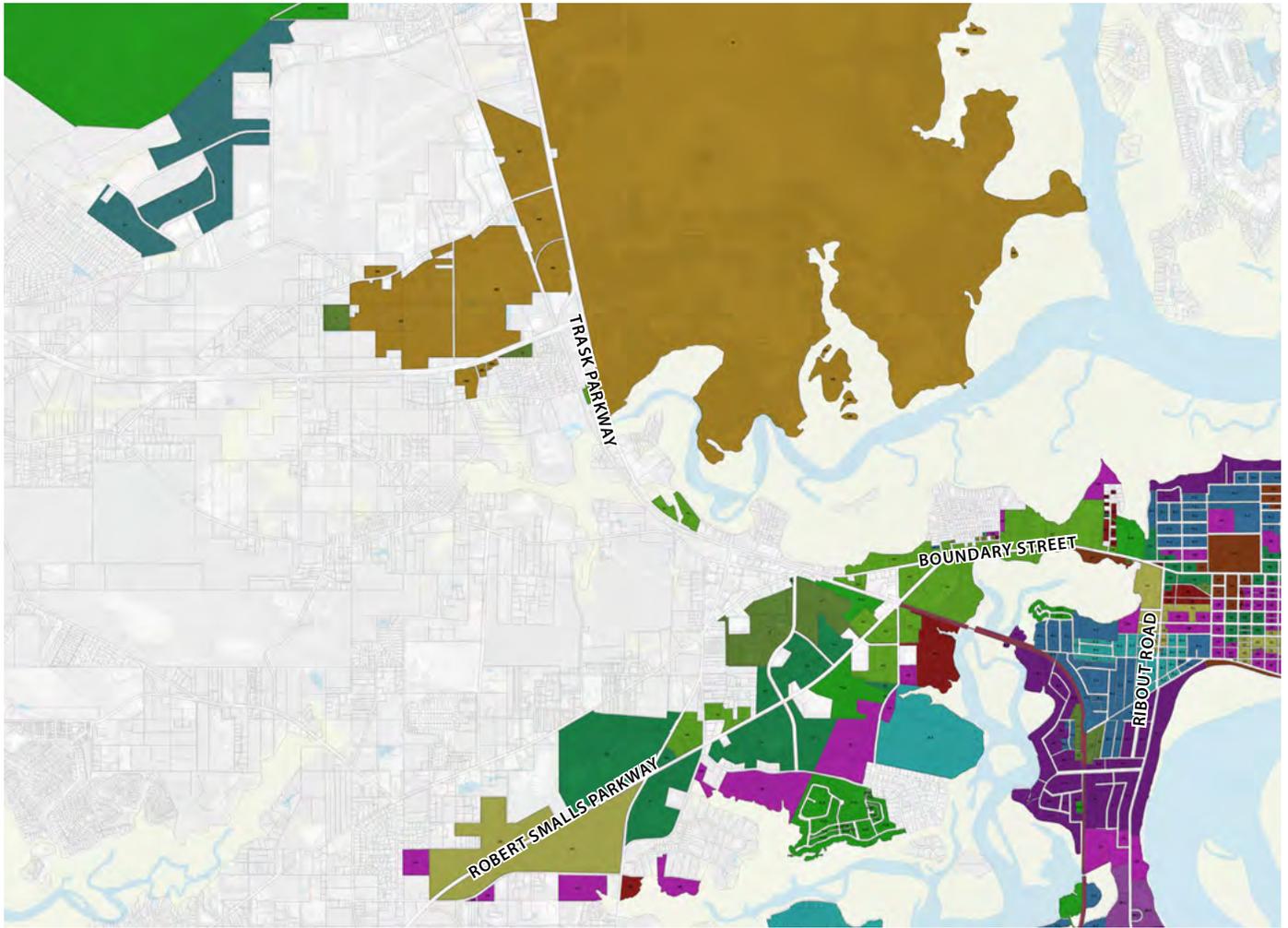


▲ 4% AND 6% TAX RATES SECTOR 5

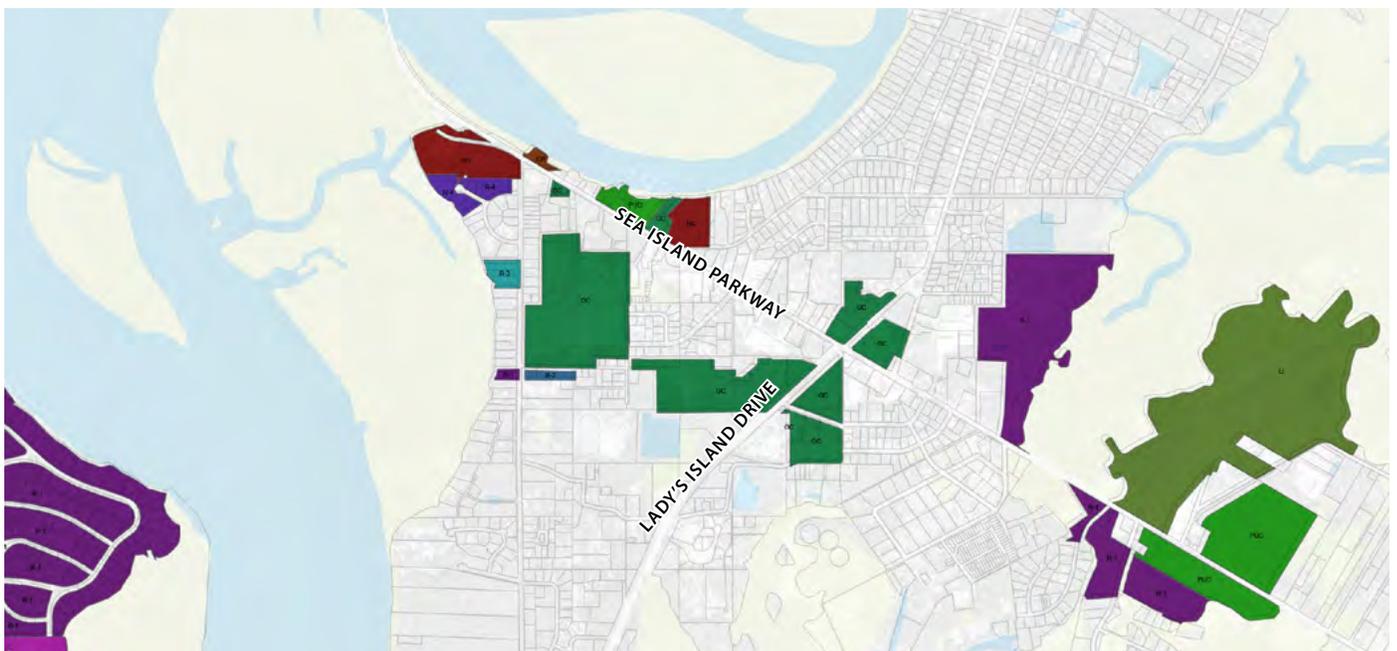
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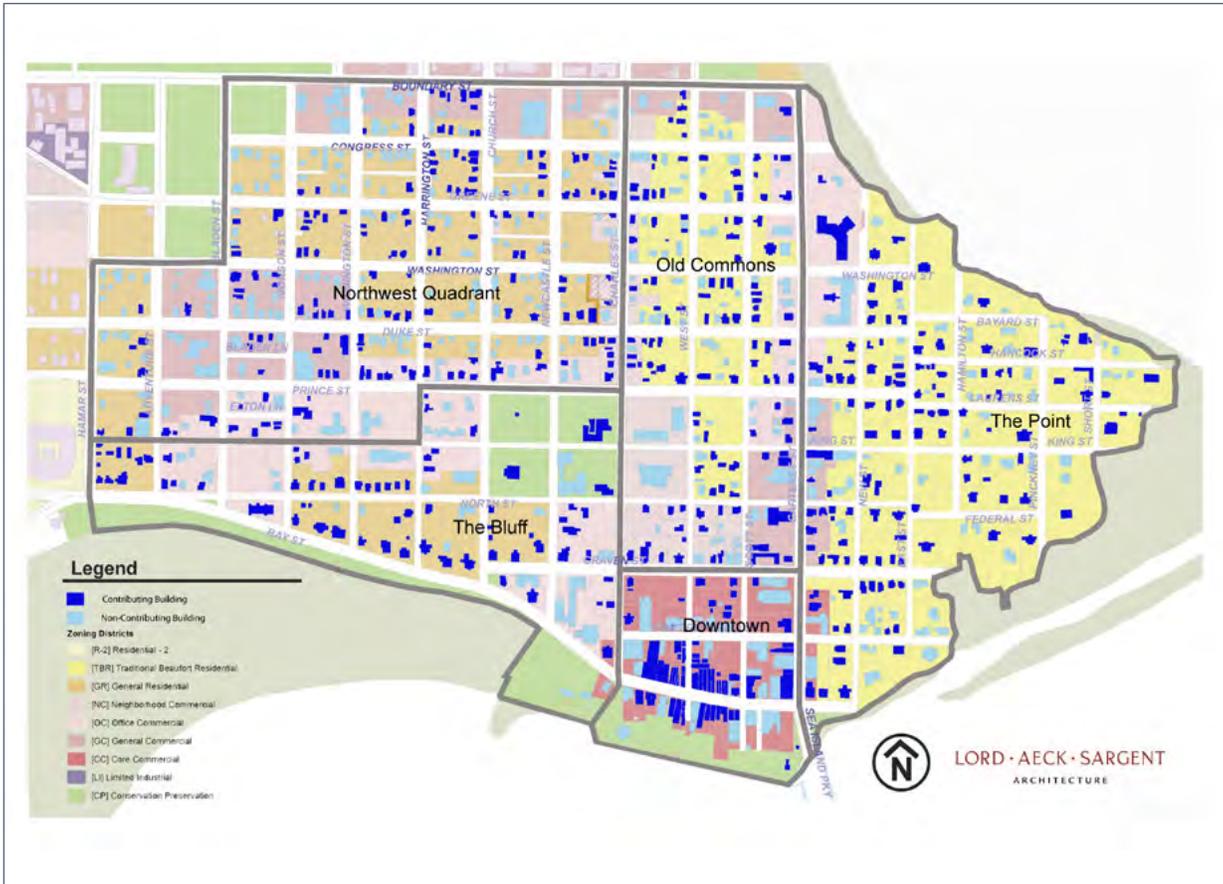
▲ EXISTING ZONING SECTORS 1-3



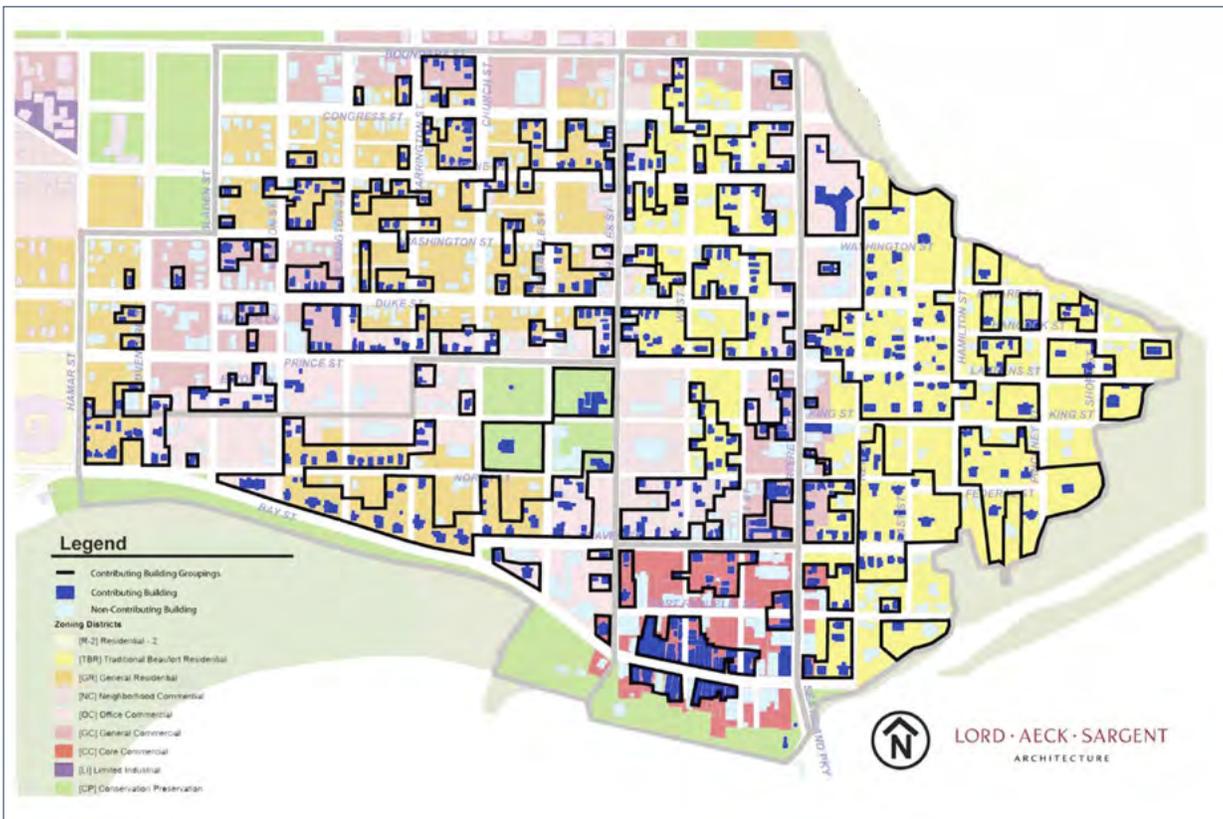
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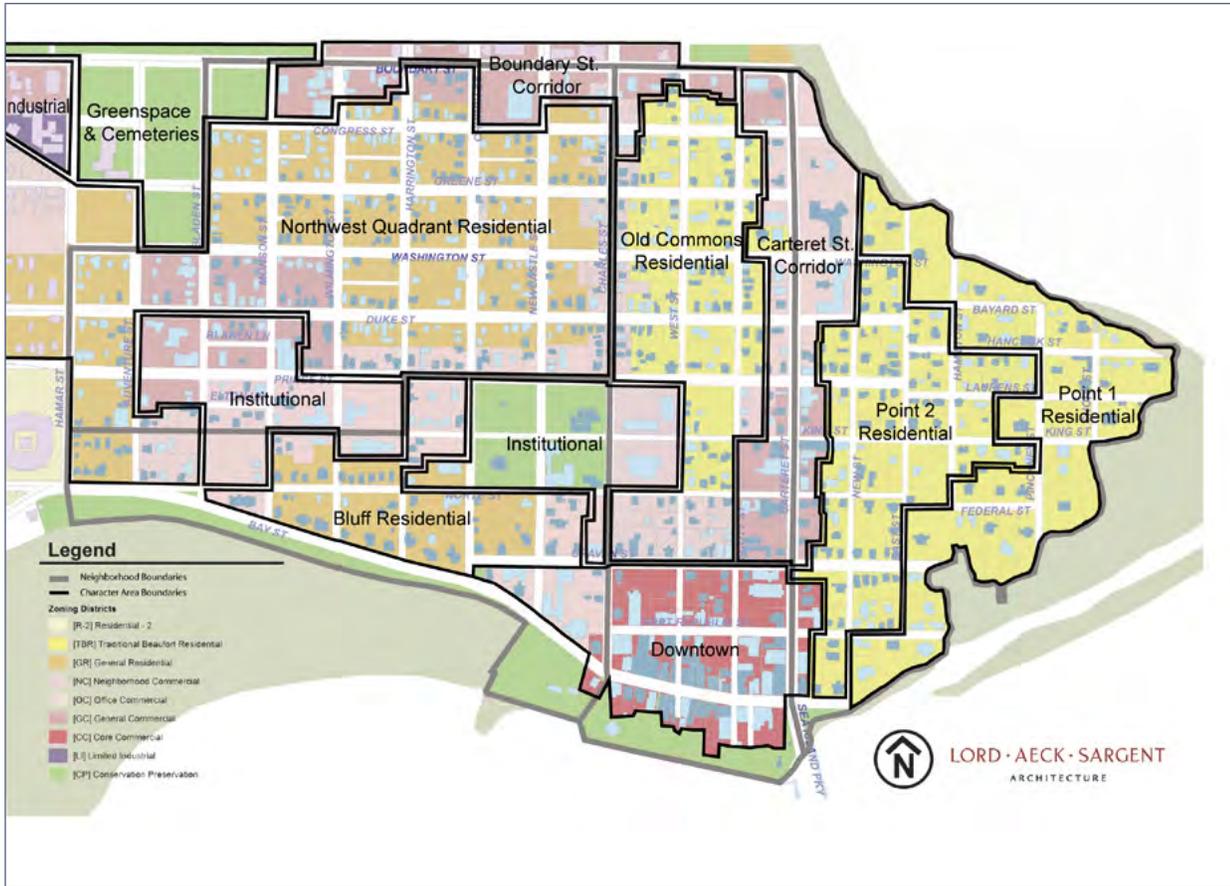
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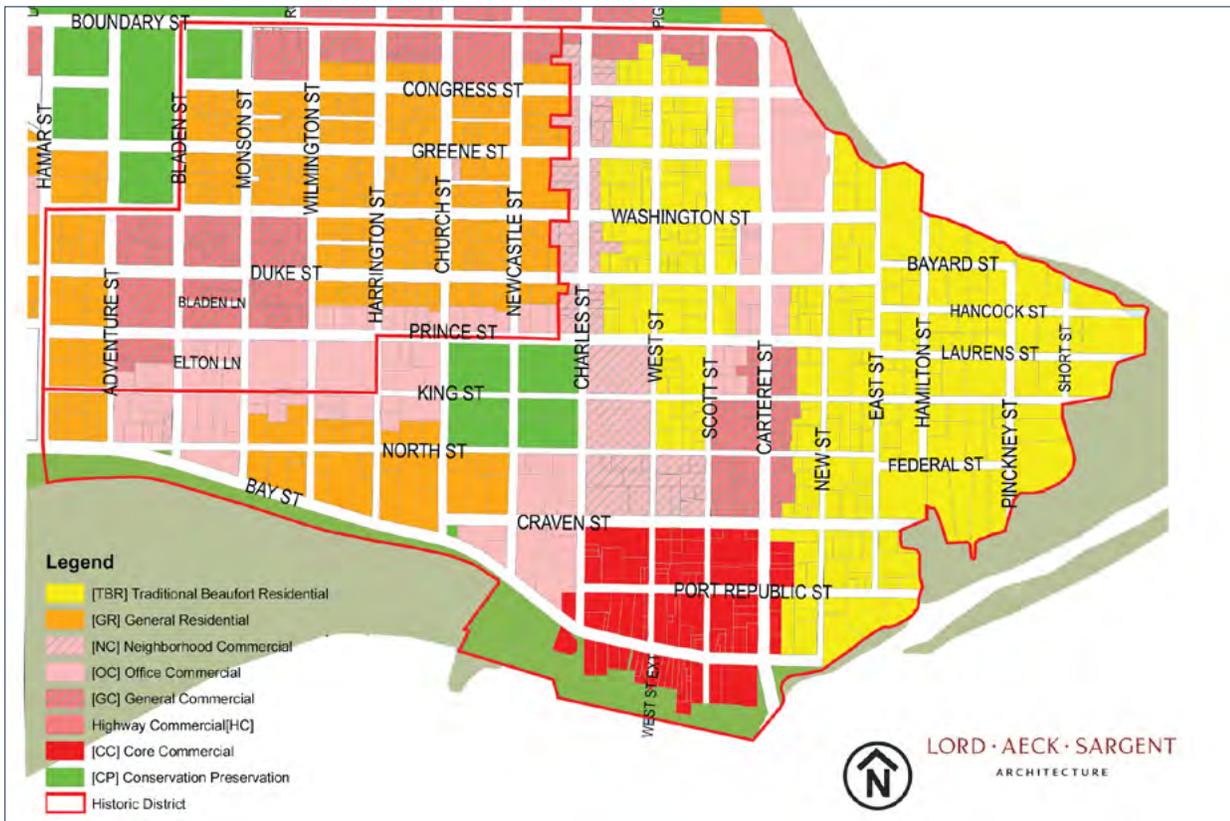
▲ NATIONAL HISTORIC LANDMARK DISTRICT - CONTRIBUTING BUILDINGS (2008)



▲ NATIONAL HISTORIC LANDMARK DISTRICT - CONTRIBUTING BUILDING CLUSTERS (2008)



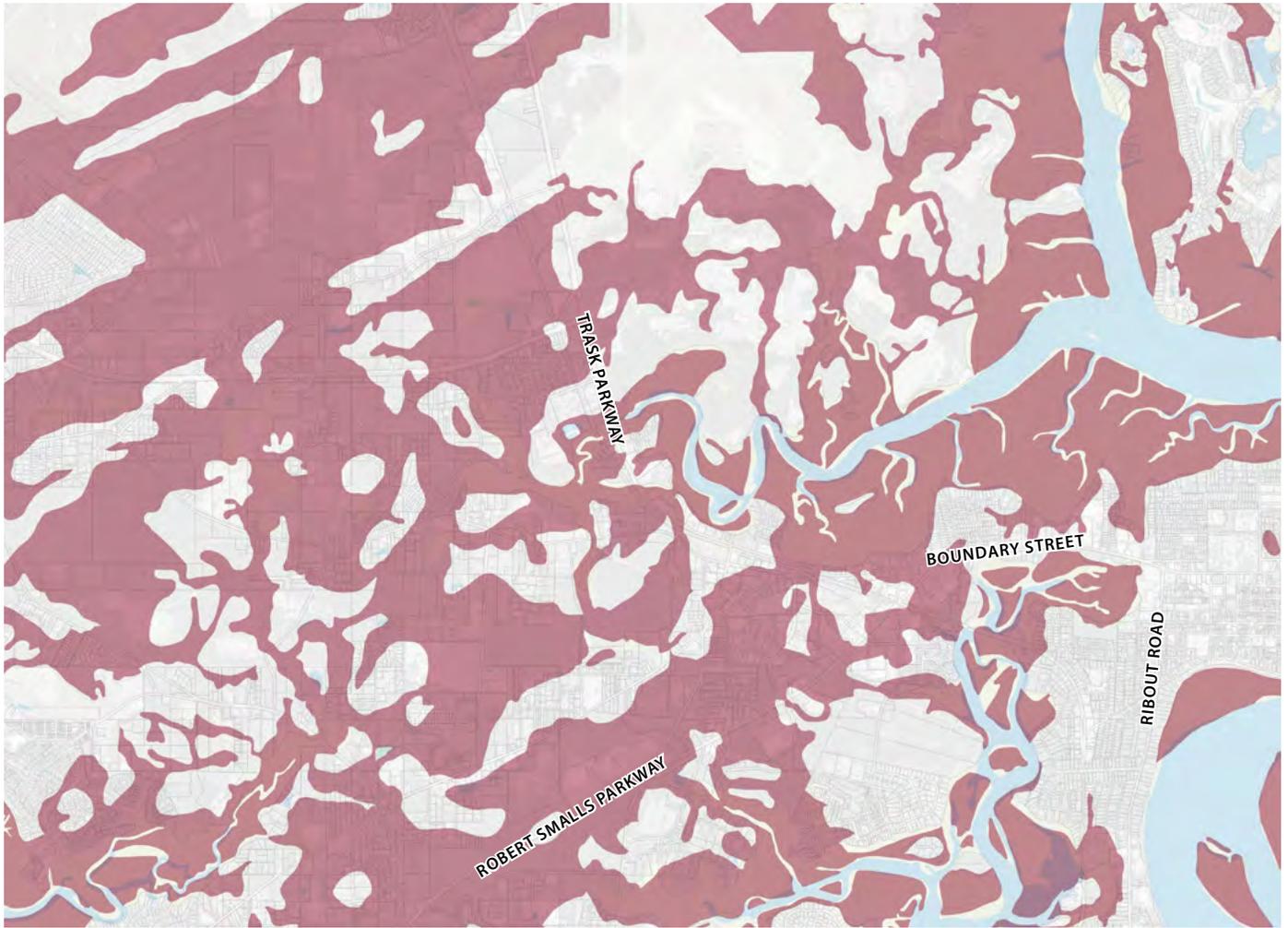
▲ NATIONAL HISTORIC LANDMARK DISTRICT - CHARACTER AREAS (2008)



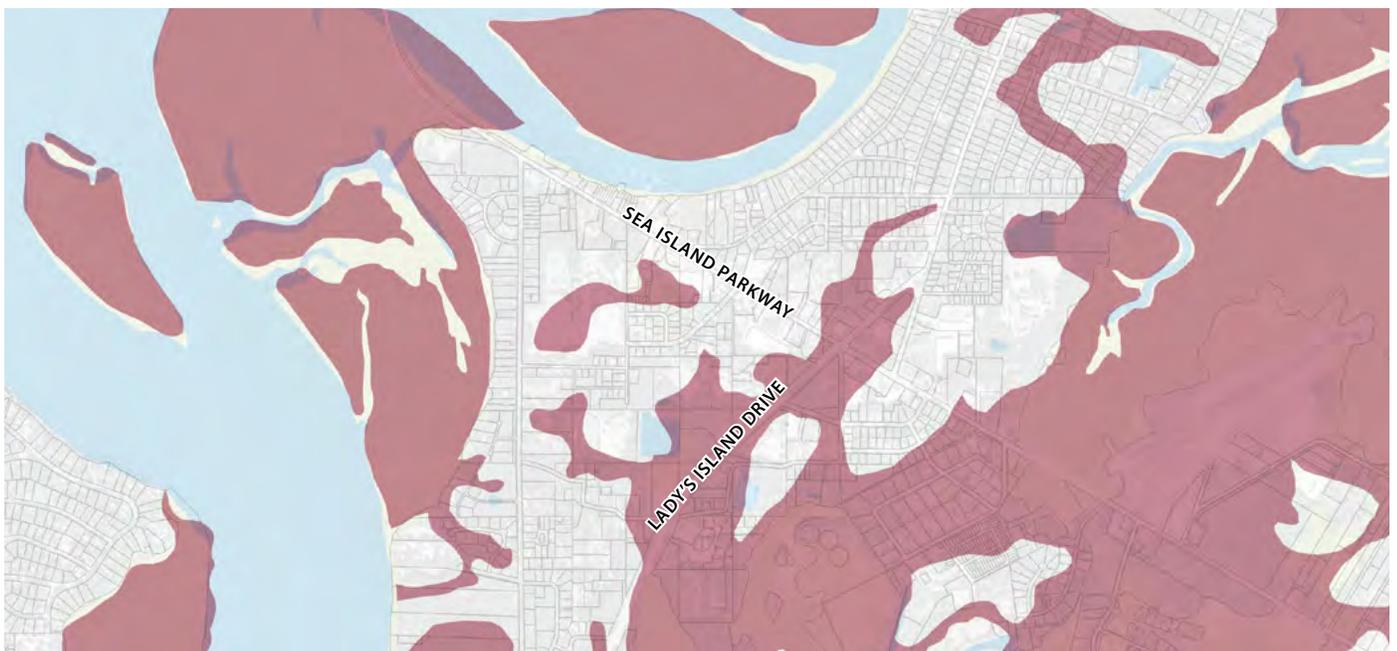
▲ NATIONAL HISTORIC LANDMARK DISTRICT - EXISTING ZONING (2008)



▲ SOIL CONDITIONS SECTOR 1-3



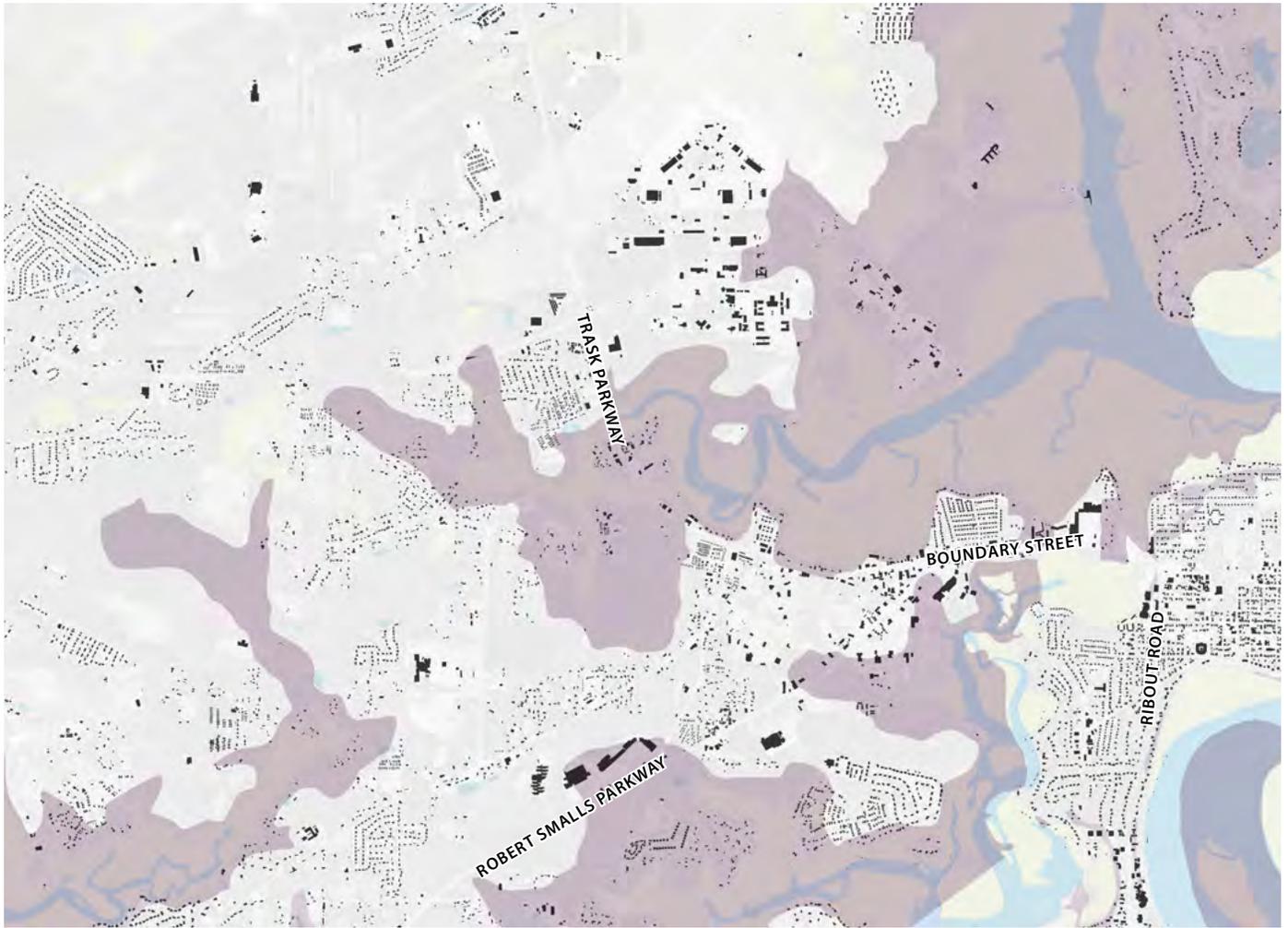
▲ SOIL CONDITIONS SECTOR 4



▲ SOIL CONDITIONS SECTOR 5



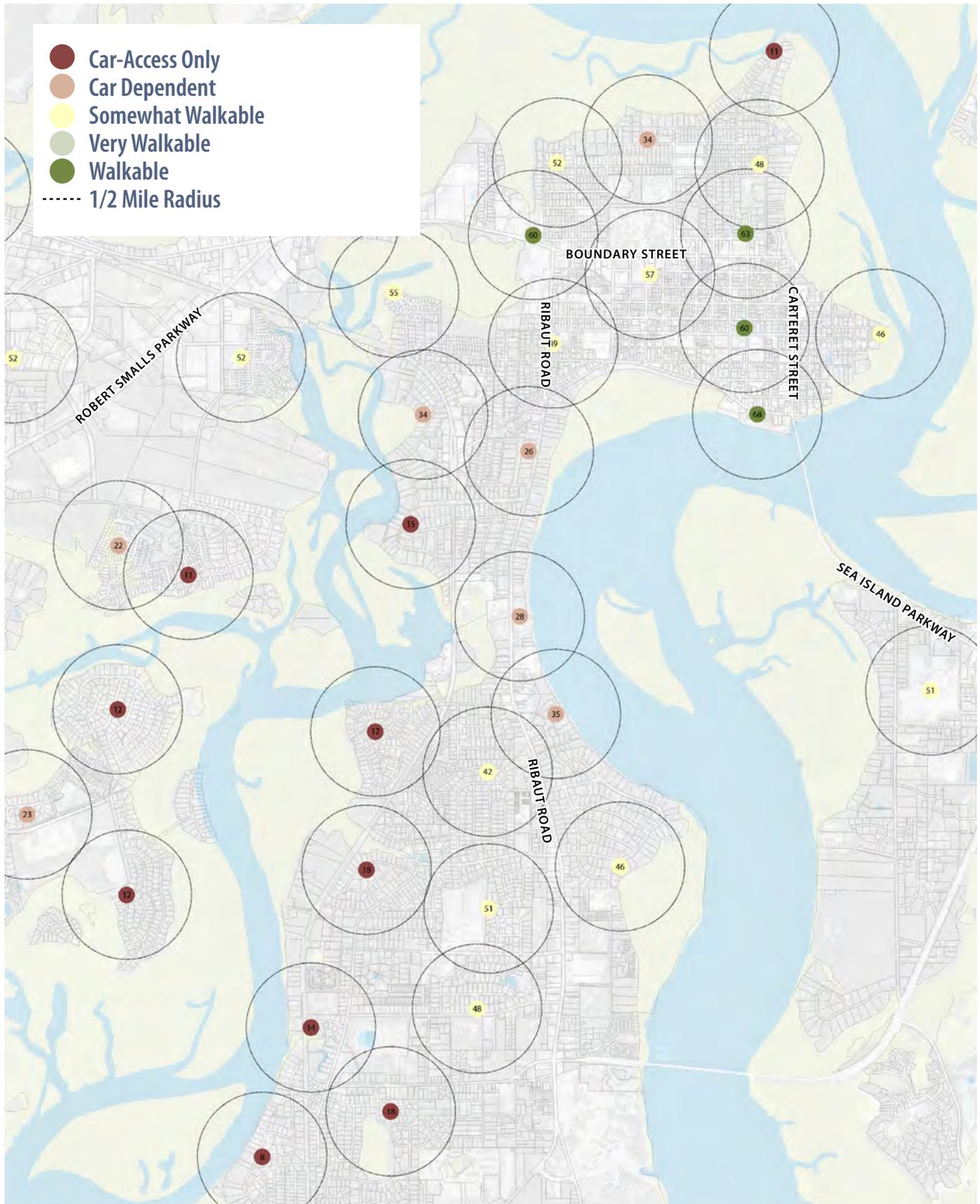
▲ FLOODPLAIN SECTOR 1-3



▲ FLOODPLAIN SECTOR 4

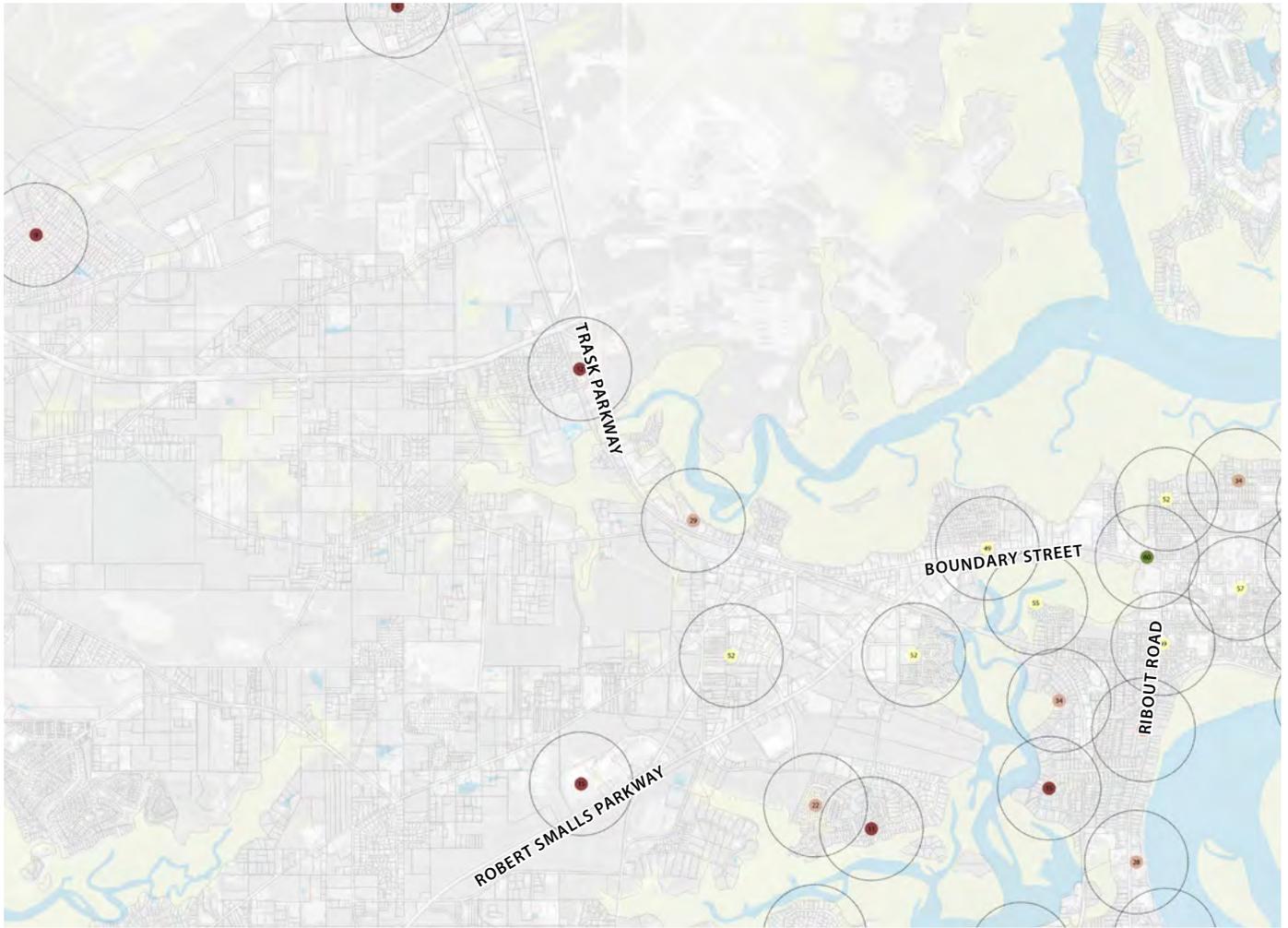


▲ FLOODPLAIN SECTOR 5



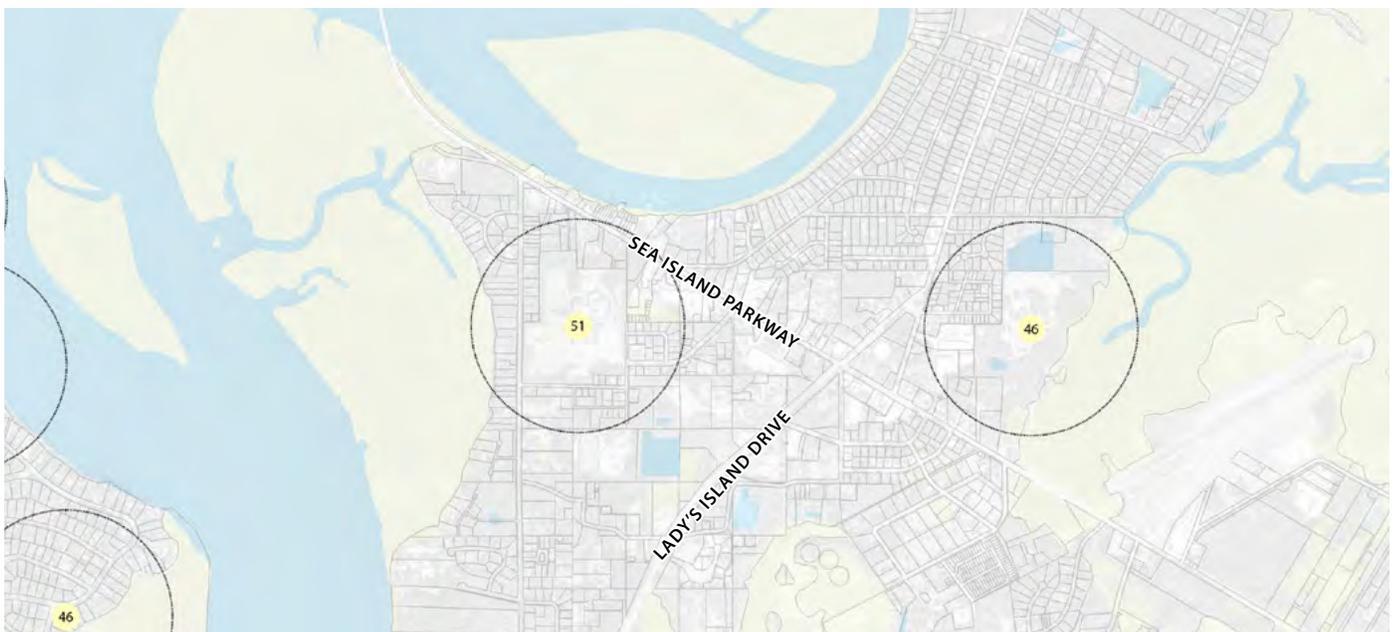
▲ EXISTING WALK SCORE SECTORS 1-3

*Based on data collected for the 2009 Comprehensive Plan



▲ EXISTING WALK SCORE SECTOR 4

*Based on data collected for the 2009 Comprehensive Plan



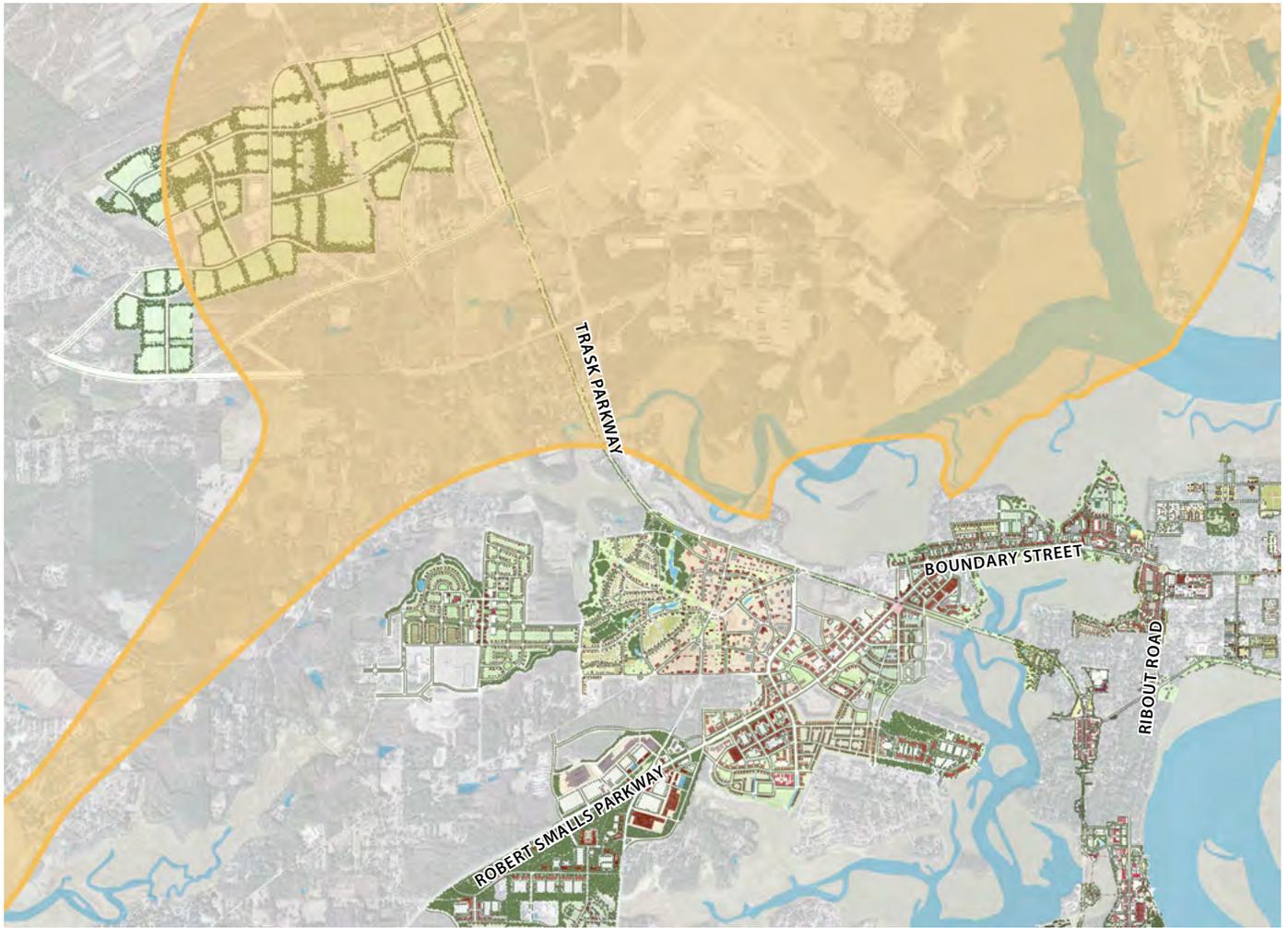
▲ EXISTING WALK SCORE SECTOR 5

*Based on data collected for the 2009 Comprehensive Plan



▲ AICUZ* SECTOR 1-3

*Note: AICUZ-Air Installation Compatible Use Zone. The AICUZ boundary shown above is expected to change during the 2013 calendar year. See Section 7.7 for more information about the AICUZ and its role in this plan.

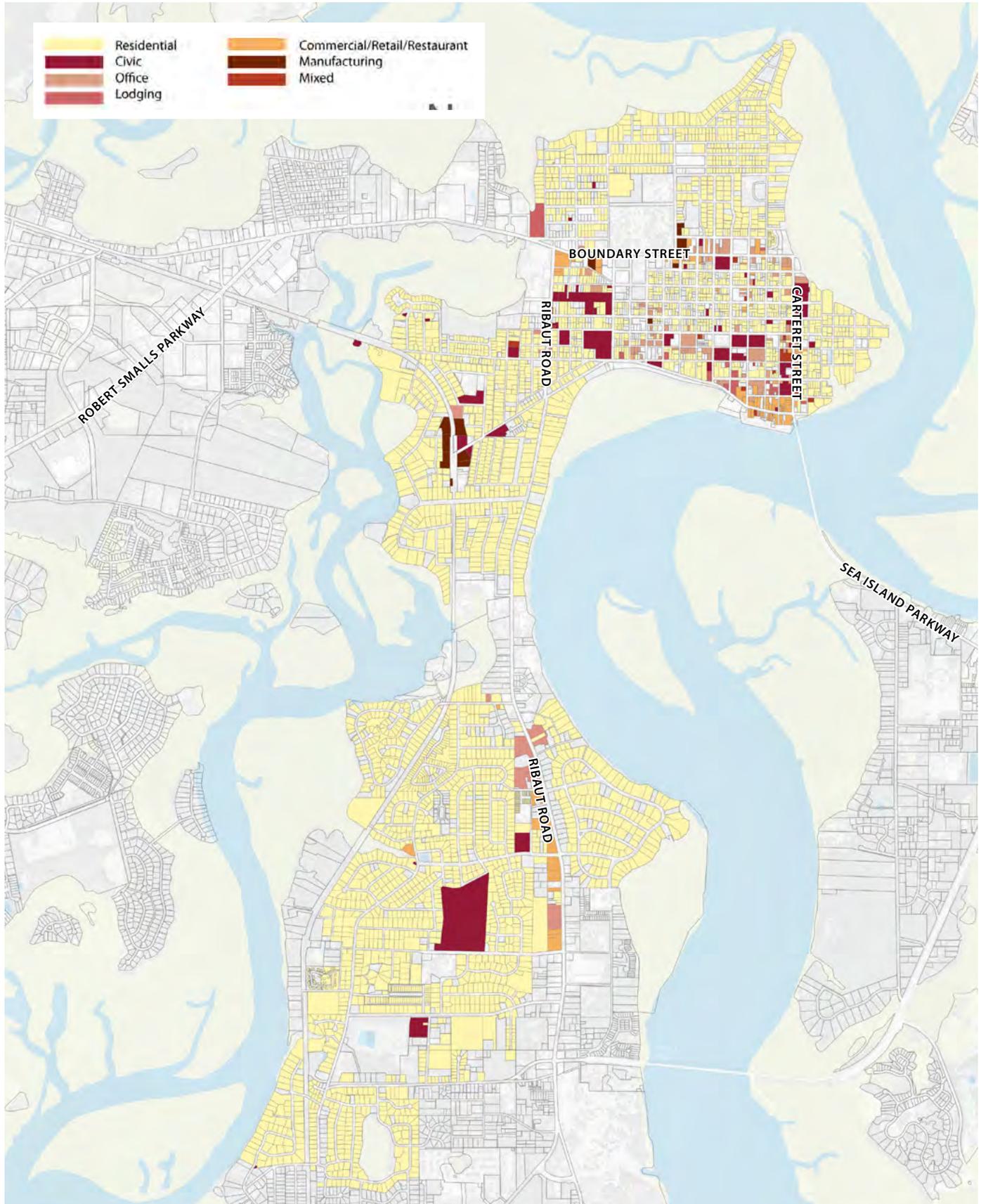


▲ AICUZ* SECTOR 4

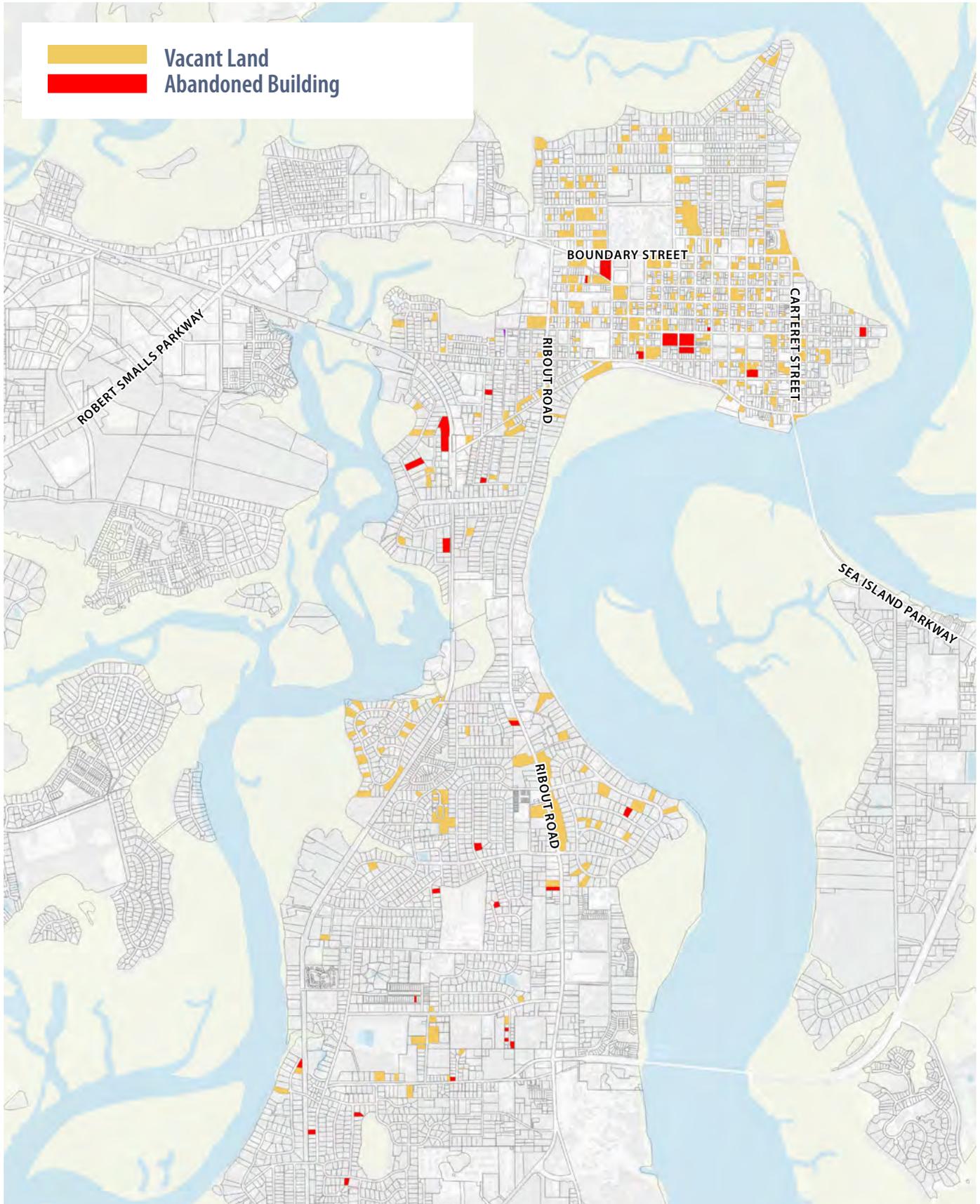


▲ AICUZ* SECTOR 5

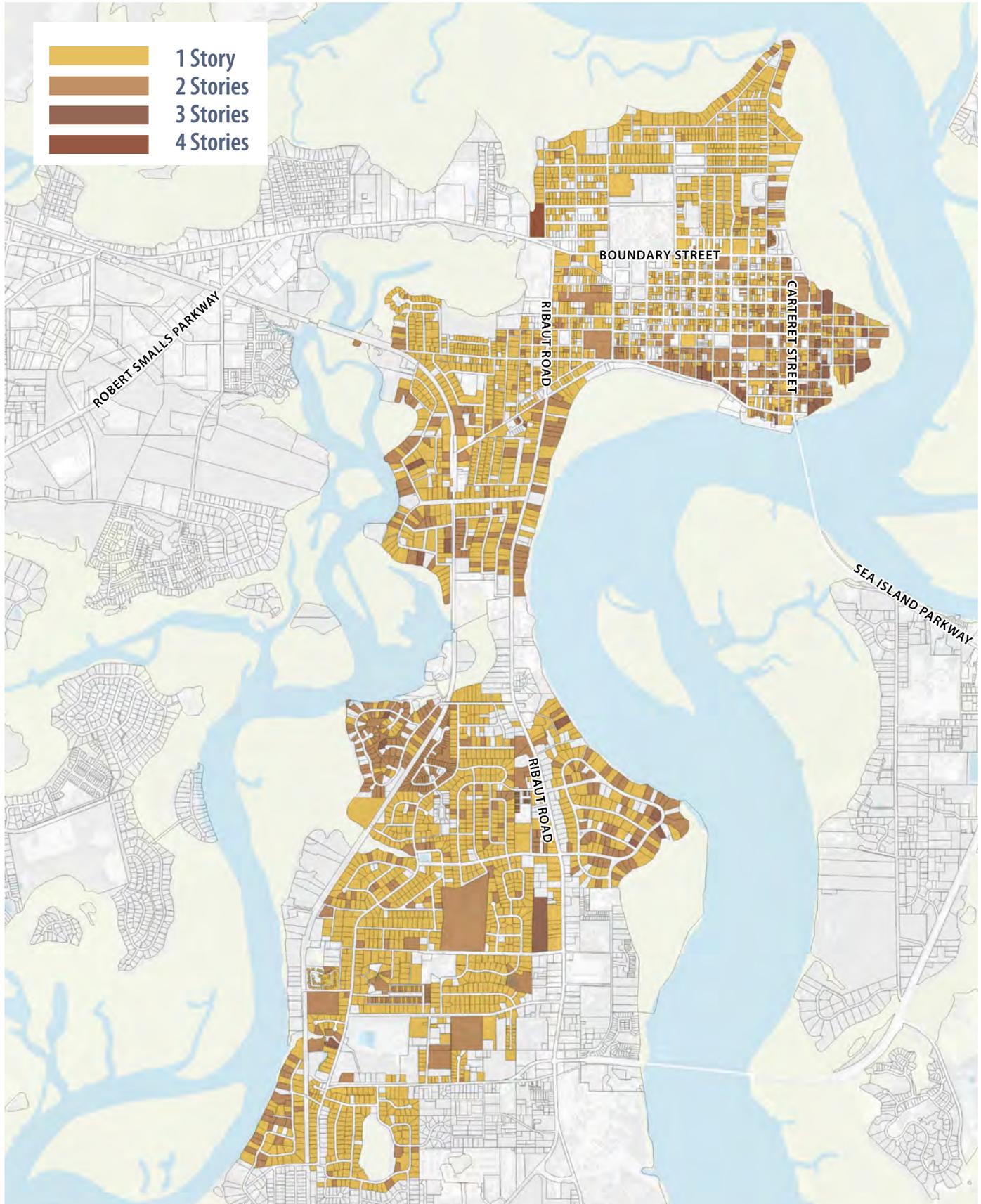
*Note: AICUZ-Air Installation Compatible Use Zone. The AICUZ boundary shown above is expected to change during the 2013 calendar year. See Section 7.7 for more information about the AICUZ and its role in this plan.



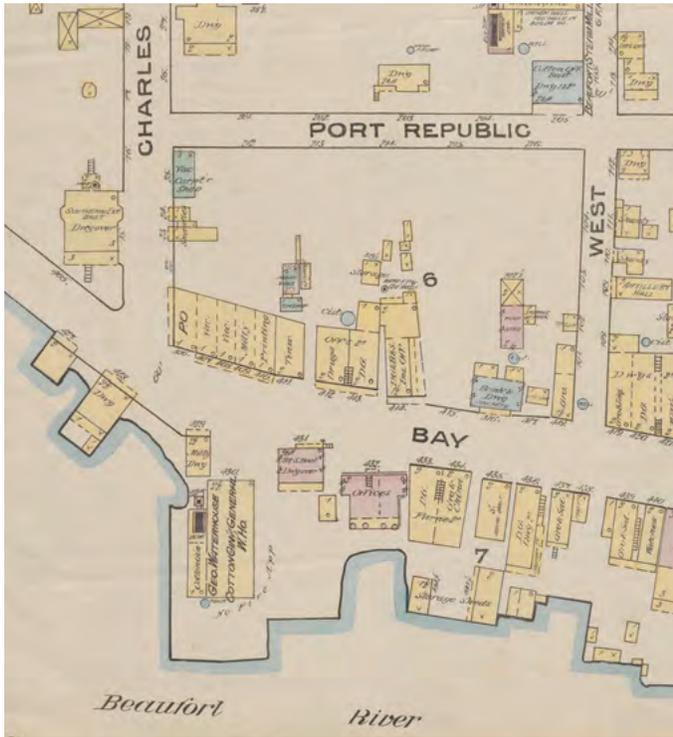
▲ EXISTING BUILDING USE SECTORS 1-3 AS OF 2011



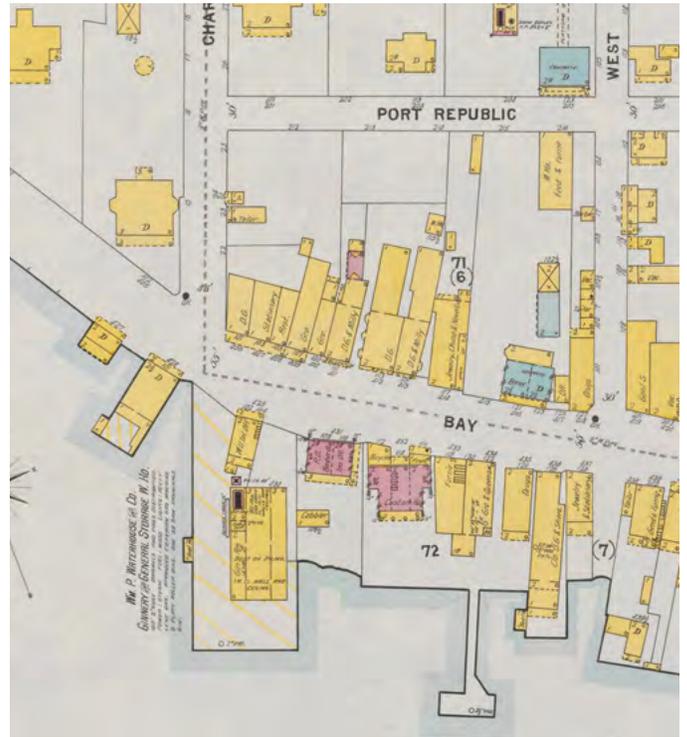
▲ EXISTING VACANT LAND AND ABANDONED BUILDINGS SECTORS 1-3 AS OF 2011



▲ EXISTING BUILDING HEIGHTS SECTORS 1-3 AS OF 2011



▲ 1884 SANBORN MAP OF MARINA AREA



▲ 1899 SANBORN MAP OF MARINA AREA



▲ 1924 SANBORN MAP OF BAY STREET FROM MARINA PARKING AREA TO CARTERET



▲ HISTORIC DISTRICT STREETSCAPE EXISTING CONDITIONS SURVEY (OCTOBER, 2013)



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PLAT OF THE TOWN OF BEAUFORT

1st. that the map of this map or part of the town of Beaufort in the Period of Sr. Helms Carolina was adopted by the United States Direct Tax Commissioners for South Carolina Nov. 3rd 1862. as appears from the record of the same and Book "A" of the Direct Tax Commission's office and said map conformable to H. said town in its water bounds. extension and all water

